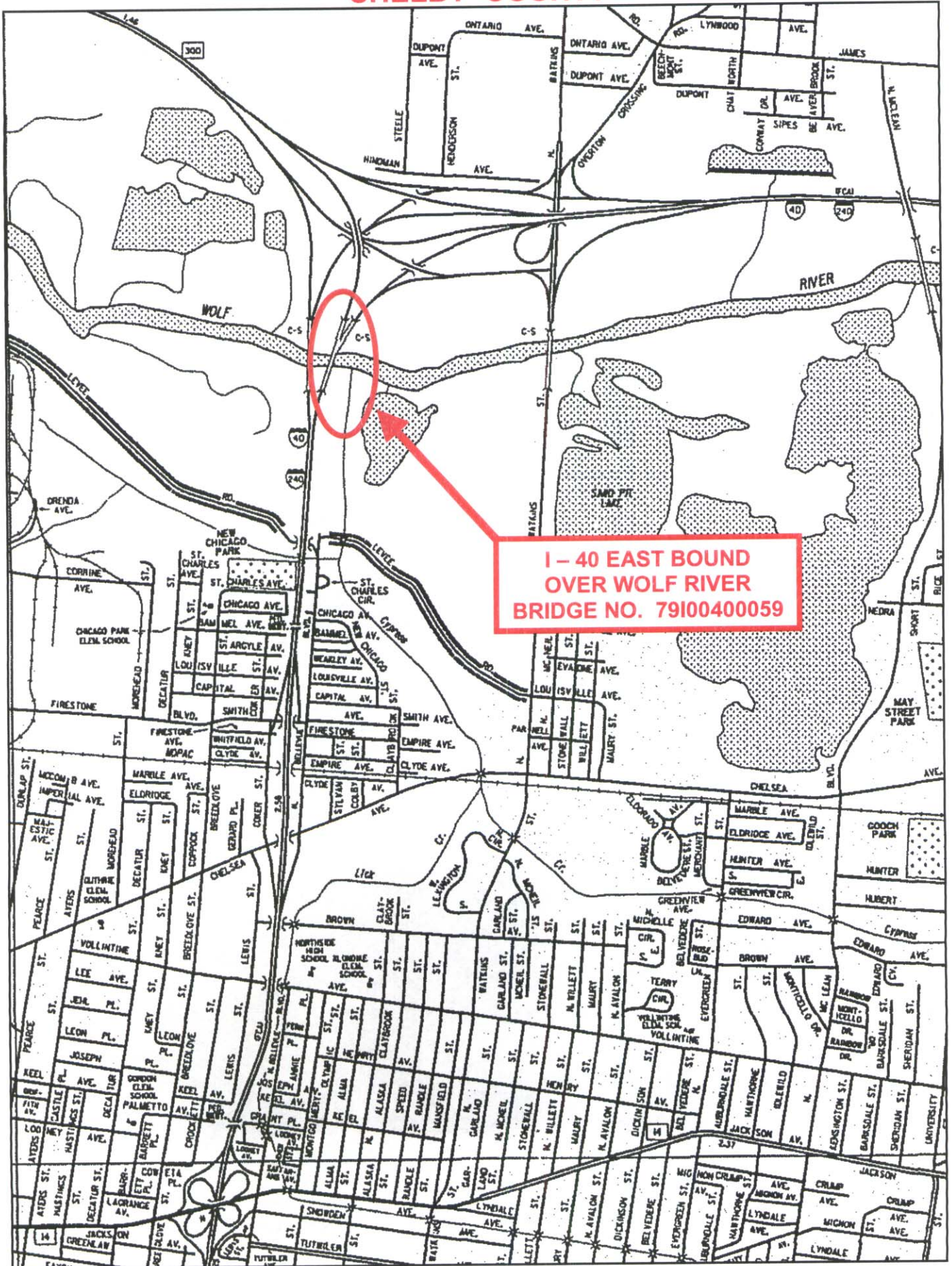


SHELBY COUNTY



Phillip Shraybman - Br. No. 79-I40-5.09R

From: Phillip Shraybman
To: Akin, Jim
Date: 7/26/2005 4:16 PM
Subject: Br. No. 79-I40-5.09R
CC: Aram, Amin; Leatherwood, Terry.D; Simpson, Ken

Dear Jim,

We've received your report for the above mentioned bridge. We appreciate the thorough inspection and, especially, the detailed documentation of channel migration toward Bent 2. Your information includes measurements and sketches, illustrating the current condition and all the changes that have occurred since the previous inspection. Based on the provided information we have performed our analysis and have concluded the following:

- a) migration of the channel toward Bent 2 is taking place and may be considered as an established fact;
- b) there is a potential for its further progress toward Bent 2 and further undermining of the embankment around the bent;
- c) in the worst case scenario of b), even if the embankment around Bent 2 has been completely washed away, the exposure may reach the top of the footing;
- d) the footing of Bent 2 is sitting on 15' to 20' long concrete piles, and its stability will not be threatened even if the top of the footing is exposed;

Considering all of the above we've concluded that migration of the channel does not presently create a dangerous condition for the bridge, and therefore it's somewhat overstretching to call the overall condition of the bridge "poor" and code Item 60 ("Condition rating of Substructure") as a 4.

We've changed it to a 6, which sufficiently reflects the situation with the channel migration at this time. However, all of us have to keep in mind how treacherous Wolf River can be, especially after some serious flooding events, and how unpredictably an overall condition can change. Therefore, we count on you to keep doing your best in monitoring any further development in channel migration.

Sincerely,
Phillip Shraybman



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE INSPECTION AND REPAIR OFFICE
NASHVILLE, TENNESSEE 37243-0338
January 5, 1998

Mr. Jim Moore
Program Scheduling Section
Suite 600, James K. Polk Bldg.
Nashville, TN. 37243

RE: Contract Maintenance - In House
Construction No. 79959-4152-04
Bridge No. 79-140-9.50 (R & L lane) / Wolf River
Bridge No. 79-140-8.25 (R & L lane) / I.C.R.R.
Bridge No. 79-140-7.60 (R & L lane) / Hollywood
St.
Bridge No. 79-140-5.09 (R & L lane) / Wolf River
Bridge No. 79-4186-2.11 / I40
Bridge No. 79-2819-4.93 / I40
Shelby County

Dear Mr. Moore

We request scheduling the above Contract Maintenance Bridge Repair project for the May 1, 1998, letting.

PROPOSED REPAIRS:

- 1) Place seismic restraints on bridges.

If we can be of any further assistance, please contact us.

Sincerely,

BRIAN EGLE

(for)
Hollis I. Tackitt,
Civil Engineering Manager II
Bridge Inspection and Repair

BKE:bke

cc: Mr. Ed Wasserman
Mr. Paul Sharp ✓
Mr. Wayne Seger
File

Also, enclosed are two (2) copies of "Description of Work" sheets, "Layout of Bridge" sheets, and "Bridge Scour Repair Detail" sheets for NO PLANS SCOUR CONTRACT No. 8634 for the following bridges:

<u>COUNTY</u>	<u>BRIDGE NO.</u>	<u>DESCRIPTION</u>
Shelby	79-I40-5.09 (Right)	I-40 over Wolf River (Eastbound)
Shelby	79-I40-5.09 (Left)	I-40 over Wolf River (Westbound)

If we can be of any further assistance, please contact us.

Yours very truly,

Wayne McInturf
(For)
Larry E. Hinds
Civil Engineering Manager 2

RBG:gvi

cc: Mr. Joe Scott
File

*Item 113 = ?
Revise on TRIMS
by Fri Oct 23
I can pull
tape next week*



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE INSPECTION AND REPAIR OFFICE
NASHVILLE, TENNESSEE 37243-0338

March 14, 1991

Mr. William L. Moore, Jr.
Regional Engineering Director
P. O. Box 429, 120 State Street
Jackson, TN 38301

RE: Contract Maintenance
Bridge No. 79-I40-5.09, L and R Lanes/Wolf River
Shelby County

Dear Mr. Moore:

We wish to inform you of our plans to schedule the following scour bridge repair project for the May 10, 1991, letting. We have listed the tentative items of repair that we plan for the bridge. Please review these and if your personnel can add any input to our proposed repairs, we will certainly appreciate it.

PROPOSED REPAIRS

- 1) Excavation and placement of rip-rap.

If we can be of any further assistance, please contact us.

Yours very truly,

Mile Jawn

(for)

Larry E. Hinds
Civil Engineering Manager 2
Bridge Inspection and Repair Office

RBG:gap

cc: Mr. Richard Gentry

Mr. Jim Akin

Mr. Charlie Hunter

Mr. Ray Brisson



NOV 12 1986 3

EXP J+ REPAIR
ADDITIONAL INFO REQ'DSTATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
P. O. BOX 429
JACKSON, TENNESSEE 38301

November 6, 1986

Mr. Richard Omohundro
Director of Construction
Suite 700 James K. Polk Bldg.
Nashville, Tennessee 37219

COMPLETION NOTICE

Contract No: 5815
Project No: 79007-4159-04
County: Shelby

Dear Sir:

Please be advised that the above project was completed 11-3-86 and the final records will be submitted on or before 2-6-87.

Termini: I-40; from 1800' E
Hollywood Street to 1650' NW
AvenueType: Joint seal & bridge deck
repair

Length: 5.738 miles

Contract Amount: \$382,163.36

Proposal Received: 8-9-85

Contract Time: 60 working days

Formal Notice of Award: 9-10-85

Time Began: 10-4-85

Contract Executed: 9-13-85

Work Began: 10-16-85

Contract Accepted: 9-20-85

Work Completed: 11-3-86

Contract Effective: 10-4-85

Time Consumed: 60 working days

Contractor: Kenneth R. Hartman, Inc.
Surety: Insurance Company of North America
Engineer: Mr. William B. Baker

You are requested to advertise this project for final settlement.

Yours very truly,

WLM/JAB/dh

William L. Moore, Jr.
Regional Engineering Directorcc: Cnstr.Contrs.Audr.
Mtl's&Tests Engr.
Admr.Prog.Opers.
Comm.of Labor
Admr.Prog.Dev&Sched.
Admr.Map & Stats.
Mgr.Utilities
Maint.Engr.Dir.Hyd.&Stru.Design
Dept.Empl.Security
Surety
Contractor
District Engineer
Project Engineer
EEO Officer
Reg.Cnstr.OfficeReg.Final Rcds.
Reg.Traffic Engr.
Reg. Maint.Engr.
Reg. Mtl's&Test Engr.
Reg. Utl's Engr.
Reg. Safety Coor.
Reg. Bridge Insp.

BRIDGE MAINTENANCE RECOMMENDATIONS

Tennessee Department
of Transportation

COUNTY: SHELBY

LOCATION: 79-I0040-05.09-R

CO. SEQ.: 1 SPEC. CASE: 0

CROSSING: I40-RL / WOLF RIVER

FED. BRIDGE NO.: 79I00400059

MAINT. DIST.: 79

REPAIR LIST NO.: 3

DATE ADDED: 11/10/2003

REVISED:

FACILITY CARRIED:	I40-RL	NUMBER OF MAIN SPANS:	10
HIGHWAY SYSTEM:	02-INTERSTATE URBAN	NUMBER OF APPROACH SPANS:	0
BRIDGE WIDTH (CURB TO CURB):	71 FT 10 IN	BRIDGE LENGTH (FT):	826
BRIDGE WIDTH (OUT TO OUT):	74 FT 1 IN	MAXIMUM SPAN LENGTH (FT):	81
APPROACH ROADWAY (W/SHOULDERS):	71 FT 10 IN	SKEW ANGLE (DEGREES):	90
MAINTAINED BY: STATE HIGHWAY AGENCY			
MAIN SPAN MATERIAL: PRESTRESSED CONCRETE CONTINUOUS			
MAIN SPAN DESIGN TYPE: STRINGER/MULTI-BEAM OR GIRDER			
APPROACH SPAN MATERIAL: OTHER OR NOT APPLICABLE			
APPROACH SPAN DESIGN TYPE: OTHER OR NOT APPLICABLE			
INSPECTION DATE:	07/15/2003	GENERAL CONDITION:	FAIR
EVALUATION DATE:	09/09/2003	STRUCTURALLY DEFICIENT:	NO
PROPOSED REPLACEMENT:		SUFFICIENCY RATING:	92.1
H TRUCK RATING @ INV.:	20 TONS		

No.	RECOMMENDATIONS	REPAIR DATE	REPAIRED BY
1.	INSTALL SCOUR PROTECTION AT BENT NO. 2		
2.	CLEAR DRIFT		
3.	CLEAR APPROACH DRAINS.		
4.	APPROACH GUARDRAILS ARE SUBSTANDARD		
5.	REPAIR CONCRETE PARAPET IN SPAN NO. 11B		
6.	REPAIR BROKEN OR LOOSE SEISMIC DEVICES AT ABUTMENT NO. 1		

COMMENTS:

CHANNEL MIGRATION TOWARD BENT NO. 2 HAS BEEN REPORTED. CHANGE OF PILE EXPOSURE AT BENT 3 FROM 34.8' TO 50.7'. CHANGE OF PILE EXPOSURE AT BENT 4 FROM 45.4' TO 50'. CHANGE OF PILE EXPOSURE AT BENT 10A FROM 17.3' TO 25.4'.

Bridge Maintenance Recommendations

Page No. _____

Page 1 of 1

Bridge Location No.: **79 - I0040 - 0509 R**

Co. Route Log Mile

Under/Over Pass No.: - -

Crossing: **WOLF RIVER**

Bridge Number: **79I00400059**

Road Name:

Region: 04

Road Name #2:

District: 45 Spec. Case: 0

Bridge Rating: **FAIR**

Maint. Resp.: 01 Co. Seq: 01

Inspection Cycle: 15

County: **Shelby**

Inspection Date: **8/3/01**

City:

@ ' x '
Barrels Length Width

Comments: **EARTHQUAKE DEVICES @ ABUTMENT #1 BROKEN**

Maintenance Recommendations:

Maintenance Completed
by/date

009	CLEAN DRAINS AT APPROACH NO. _1 & 2	
204	CUT VEGETATION	
201	CLEAN DRIFT	
228	APPROACH GUARDRAILS ARE SUBSTANDARD	
226	GUARDRAIL TERMINALS AT APPROACH NO. 1 & 2 ARE SUBSTANDARD	
007	CLEAN AND SEAL JOINT AT APPROACH NO. _1 & 2	
001	LEVEL APPROACH NO. _1 & 2	

COMPLETION NOTIFICATION: RETURN WITHIN 6 MONTHS OF INSPECTION DATE.

INITIAL AND DATE RECOMMENDATIONS WHEN COMPLETED.

MAINTENANCE ACTIVITIES ARE COMPLETED (DATE) _____ BY _____

MAINTENANCE ACTIVITIES ARE PARTIALLY COMPLETED (DATE) _____ BY _____

MAINTENANCE ACTIVITIES ARE INCOMPLETE, SCHEDULED FOR (DATE) _____

EXPLANATIONS AND COMMENTS:



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

Bridge Condition Coding Form

County: 79

Route: 10040

Special Case: 0

County Sequence: 01

Log Mile: 5.09

Bridge Number: 791004000591
(Includes Item 5A)

Feature Intersected: I40-RL / WOLF RIVER

CODE ONLY THOSE VALUES WHICH HAVE CHANGED

ITEM #	DESCRIPTION	VALUE	CONDITION CODING GUIDELINES (Values for Coding Items 58, 59, 60 and 62)
90	INSPECTION DATE	09/16/1999 <i>8/31/2001</i>	N NOT APPLICABLE
10	MINIMUM V.C. OVER DECK (ROADWAY + SHOULDERS)	99 FT. 99 IN.	9 EXCELLENT CONDITION
520	MINIMUM V.C. OVER DECK (EXCLUDES SHOULDERS)	99 FT. 99 IN.	8 VERY GOOD CONDITION - NO PROBLEMS NOTED.
36	TRAFFIC SAFETY FEATURES		7 GOOD CONDITION - SOME MINOR PROBLEMS.
	Br. Rail Trans. Appr. Rail Appr. Rail Ends		6 SATISFACTORY CONDITION - MINOR DETERIORATION OF STRUCTURAL ELEMENTS.
	1 0 <i>0</i> 0		5 FAIR CONDITION - ALL PRIMARY STRUCTURAL ELEMENTS ARE SOUND BUT MAY HAVE MINOR SECTION LOSS, CRACKING, SPALLING OR SCOUR.
41	STRC OPEN/CLOSED/POSTED	A	4 POOR CONDITION - ADVANCED SECTION LOSS, DETERIORATION, SPALLING OR SCOUR.
	A K P		3 SERIOUS CONDITION - LOSS OF SECTION, DETERIORATION, SPALLING OR SCOUR HAVE SERIOUSLY AFFECTED PRIMARY STRUCTURAL COMPONENTS. LOCAL FAILURES ARE POSSIBLE. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT.
58	DECK	7	2 CRITICAL CONDITION - ADVANCED DETERIORATION OF PRIMARY STRUCTURAL ELEMENTS. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT OR SCOUR MAY HAVE REMOVED SUBSTRUCTURE SUPPORT. UNLESS CLOSELY MONITORED IT MAY BE NECESSARY TO CLOSE THE BRIDGE UNTIL CORRECTIVE ACTION IS TAKEN.
59	SUPERSTRUCTURE	7	1 "IMMINENT" FAILURE CONDITION - MAJOR DETERIORATION OR SECTION LOSS PRESENT IN CRITICAL STRUCTURAL COMPONENTS OR OBVIOUS VERTICAL OR HORIZONTAL MOVEMENT AFFECTING STRUCTURAL STABILITY. BRIDGE IS CLOSED TO TRAFFIC BUT CORRECTIVE ACTION MAY PUT BACK IN LIGHT SERVICE.
60	SUBSTRUCTURE	7	0 FAILED CONDITION - OUT OF SERVICE AND BEYOND CORRECTIVE ACTION.
61	CHANL/CHANL PROTECTION	6	
62	CULVERT AND RETAIN WALL	N	
71	WATERWAY ADEQUACY	<i>8</i> <i>7</i>	
72	APPROACH RDWY ALIGNMENT (USE VALUES OF 3, 6, OR 8)	8	
521	OVERALL CONDITION (Circle One)		
	GOOD <u>FAIR</u> POOR CRITICAL		

TEAM LEADER SIGNATURE

REVIEW DATE

8/31/2001



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

Bridge Condition Coding Form

County:

Route:

Special Case:

County Sequence:

Log Mile:

Bridge Number:
(Includes Item 5A)

Feature Intersected:

CODE ONLY THOSE NUMBERS WHICH HAVE CHANGED

ITEM #	DESCRIPTION	VALUE	COMMENTS
90	INSPECTION DATE	<input type="text" value="12/1/97"/>	RATINGS FOR CODING ITEMS 58, 59, 60 AND 62
		<u>9116199</u>	N NOT APPLICABLE
10	MINIMUM V.C. OVER DECK (ROADWAY + SHOULDERS)	99 FT. 99 IN.	9 EXCELLENT CONDITION
		____ FT. ____ IN.	8 VERY GOOD CONDITION - NO PROBLEMS NOTED.
520	MINIMUM V.C. OVER DECK (EXCLUDES SHOULDERS)	99 FT. 99 IN.	7 GOOD CONDITION - SOME MINOR PROBLEMS.
		____ FT. ____ IN.	6 SATISFACTORY CONDITION - MINOR DETERIORATION OF STRUCTURAL ELEMENTS.
54	MINIMUM VERTICAL UNDERCLEARANCE (EXCLUDES SHOULDERS)	<input type="text" value="N"/> 0 FT. 0 IN.	5 FAIR CONDITION - ALL PRIMARY STRUCTURAL ELEMENTS ARE SOUND BUT MAY HAVE MINOR SECTION LOSS, CRACKING, SPALLING OR SCOUR.
	Circle One: H R N	____ FT. ____ IN.	4 POOR CONDITION - ADVANCED SECTION LOSS, DETERIORATION, SPALLING OR SCOUR.
36	TRAFFIC SAFETY FEATURES		3 SERIOUS CONDITION - LOSS OF SECTION, DETERIORATION, SPALLING OR SCOUR HAVE SERIOUSLY AFFECTED PRIMARY STRUCTURAL COMPONENTS. LOCAL FAILURES ARE POSSIBLE. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT.
	Br. Rail Trans. Appr. Rail Appr. Rail Ends		2 CRITICAL CONDITION - ADVANCED DETERIORATION OF PRIMARY STRUCTURAL ELEMENTS. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT OR SCOUR MAY HAVE REMOVED SUBSTRUCTURE SUPPORT. UNLESS CLOSELY MONITORED IT MAY BE NECESSARY TO CLOSE THE BRIDGE UNTIL CORRECTIVE ACTION IS TAKEN.
	1 1 1 1		1 "IMMINENT" FAILURE CONDITION - MAJOR DETERIORATION OR SECTION LOSS PRESENT IN CRITICAL STRUCTURAL COMPONENTS OR OBVIOUS VERTICAL OR HORIZONTAL MOVEMENT AFFECTING STRUCTURAL STABILITY. BRIDGE IS CLOSED TO TRAFFIC BUT CORRECTIVE ACTION MAY PUT BACK IN LIGHT SERVICE.
	0 0		0 FAILED CONDITION - OUT OF SERVICE AND BEYOND CORRECTIVE ACTION.
41	STRC OPEN/CLOSED/POSTED	A	
	A K P		
58	DECK	7	
59	SUPERSTRUCTURE	7	
60	SUBSTRUCTURE	7	
61	CHANL/CHANL PROTECTION	6	
62	CULVERT AND RETAIN WALL	N	
72	APPROACH RDWY ALIGNMENT (USE VALUES OF 3, 6, OR 8)	8	
	OVERALL CONDITION (Circle One)		
	GOOD <input checked="" type="radio"/> FAIR POOR CRITICAL		

SIGNATURE

DATE

9116199



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

Bridge Condition Coding Form

County:

Route:

Special Case:

County Sequence:

Log Mile:

Bridge Number:
(Includes Item 5A)

Feature Intersected:

CODE ONLY THOSE NUMBERS WHICH HAVE CHANGED

ITEM #	DESCRIPTION	VALUE	COMMENTS
90	INSPECTION DATE	<input type="text" value="12/1/97"/> <u>9/16/99</u>	RATINGS FOR CODING ITEMS 58, 59, 60 AND 62 N NOT APPLICABLE
10	MINIMUM V.C. OVER DECK (ROADWAY + SHOULDERS)	99 FT. 99 IN. ____ FT. ____ IN.	9 EXCELLENT CONDITION
520	MINIMUM V.C. OVER DECK (EXCLUDES SHOULDERS)	99 FT. 99 IN. ____ FT. ____ IN.	8 VERY GOOD CONDITION - NO PROBLEMS NOTED.
54	MINIMUM VERTICAL UNDERCLEARANCE (EXCLUDES SHOULDERS)	<input type="text" value="N"/> 0 FT. 0 IN.	7 GOOD CONDITION - SOME MINOR PROBLEMS.
	Circle One: H R N	____ FT. ____ IN.	6 SATISFACTORY CONDITION - MINOR DETERIORATION OF STRUCTURAL ELEMENTS.
36	TRAFFIC SAFETY FEATURES		5 FAIR CONDITION - ALL PRIMARY STRUCTURAL ELEMENTS ARE SOUND BUT MAY HAVE MINOR SECTION LOSS, CRACKING, SPALLING OR SCOUR.
	Br. Rail Trans. Appr. Rail Appr. Rail Ends		4 POOR CONDITION - ADVANCED SECTION LOSS, DETERIORATION, SPALLING OR SCOUR.
	1 1 1 1		3 SERIOUS CONDITION - LOSS OF SECTION, DETERIORATION, SPALLING OR SCOUR HAVE SERIOUSLY AFFECTED PRIMARY STRUCTURAL COMPONENTS. LOCAL FAILURES ARE POSSIBLE. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT.
41	STRC OPEN/CLOSED/POSTED	A	2 CRITICAL CONDITION - ADVANCED DETERIORATION OF PRIMARY STRUCTURAL ELEMENTS. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT OR SCOUR MAY HAVE REMOVED SUBSTRUCTURE SUPPORT. UNLESS CLOSELY MONITORED IT MAY BE NECESSARY TO CLOSE THE BRIDGE UNTIL CORRECTIVE ACTION IS TAKEN.
	A K P		1 "IMMINENT" FAILURE CONDITION - MAJOR DETERIORATION OR SECTION LOSS PRESENT IN CRITICAL STRUCTURAL COMPONENTS OR OBVIOUS VERTICAL OR HORIZONTAL MOVEMENT AFFECTING STRUCTURAL STABILITY. BRIDGE IS CLOSED TO TRAFFIC BUT CORRECTIVE ACTION MAY PUT BACK IN LIGHT SERVICE.
58	DECK	7	0 FAILED CONDITION - OUT OF SERVICE AND BEYOND CORRECTIVE ACTION.
59	SUPERSTRUCTURE	7	
60	SUBSTRUCTURE	7	
61	CHANL/CHANL PROTECTION	6	
62	CULVERT AND RETAIN WALL	N	
72	APPROACH RDWY ALIGNMENT (USE VALUES OF 3, 6, OR 8)	8	
	OVERALL CONDITION (Circle One)		
	GOOD <input checked="" type="radio"/> FAIR <input type="radio"/> POOR <input type="radio"/> CRITICAL <input type="radio"/>		

SIGNATURE

DATE

9/16/99

TENNESSEE BRIDGE INSPECTION PROGRAM

SUMMARY OF EVALUATION

REV. 03-05-2003

BRIDGE ID NO: 79I00400059

LOCATION NO: 79 - I0040 - 5.01 R

(6A) CROSSING: I-40 E.B. / WOLF RIVER

(505) METHOD OF ANALYSIS: LOAD RESISTANCE
FACTOR METHOD - RF

(548) RATING BASED ON: AASHTOWare Bridge Rating
(6" Asphalt)

LOAD RATINGS IN TONS

INVENTORY (503) H 25	(518B) HS 23
OPERATING (504) H 37	(519) HS 31

REQ. POSTING:

(549) EVALUATOR: DCD

(522) EVAL. DATE: 11/19/2018

LAST UPDATED BY: LINER

(29) ADT: 119,200 (30) ADT YR: 2019

(100) STRAHNET ROUTE: YES

(19) DETOUR LENGTH: 1 KM

(520) VC OVER RDWY: 99.99 METERS

CONDITION RATINGS

APPRAISAL RATINGS

CODE VALUES

(58) DECK RATING: 7	(67) STRUCTURAL EVALUATION: 5	N - NOT APPLICABLE
(59) SUPERSTRUCTURE RATING: 7	(68) DECK GEOMETRY: 7	9 - EXCELLENT CONDITION
(60) SUBSTRUCTURE RATING: 6	(69) UNDER CLEARANCE: N	8 - VERY GOOD CONDITION
(61) CHANNEL PROTECTION: 6	(70) BRIDGE POSTING: 5	7 - GOOD CONDITION
(62) CULVERT RATING: N	(71) WATERWAY ADEQUACY: 8	6 - SATISFACTORY
(113A) NBIS SCOUR CODE: 4	(72) APPROACH RDWY ALIGNMENT: 8	5 - FAIR CONDITION
(113B) TDOT SCOUR CODE: C		4 - POOR CONDITION
		3 - SERIOUS CONDITION
		2 - CRITICAL CONDITION
		1 - FAILURE IS IMMINENT
		0 - FAILED CONDITION

OTHER RATING ITEMS

(521) OVERALL CONDITION: F	(36) TRAFFIC SAFETY
(513) TEXTURE COAT RATING: F 10	FEATURES: 1 1 1 1
(514) PAINT CONDITION RATING: N	(525) REPAIR LIST NO: 3
(41) WEIGHT POSTING CODE: A	

COMMENTS

IN 1998 AN EXTENSIVE AMOUNT OF RIP-RAP WAS PLACED (BY THE REGION) ON THE NORTH BANK AND AN OLD COFFER DAM WAS REMOVED FROM UPSTREAM. IN FOLLOW UP INSPECTIONS THROUGH 2013 IT APPEARS THAT THE CHANNEL IS MOVING TOWARD THE SOUTH BANK (BENT NO. 2). IN JULY 2013 COLUMNS "C" AND "D" OF BENT NO. 2 WERE LOCATED IN THE CHANNEL. IN DECEMBER 2016, COLUMN B WAS LOCATED IN THE CHANNEL. CONTINUE TO MONITOR THIS CONDITION CLOSELY.

ALL FOOTINGS ARE ON CONCRETE PILES AND NO FOOTING IS EXPOSED AT THIS TIME. KEEP MONITORING CHANNEL MIGRATION. (PJS 01-29-09)

Bridge Name: I-40 EB over Wolf River
NBI Structure ID: 79100400059
Bridge ID: 79100400059

Analyzed By: bridgeware
Analyze Date: Wednesday, June 17, 2020 14:59:22
Analysis Engine: AASHTO LRFR Engine Version 6.8.1.3001
Analysis Preference Setting: None

Report By: bridgeware
Report Date: Wednesday, June 17, 2020 19:26:53

Structure Definition Name: Girder Lines
Member Name: Ramp Exterior
Member Alternative Name: Ramp Exterior

Load and Resistance Factor Rating Summary

		Girder Summary							
		Rating		Capacity	Location				
Live Load		Factor	Controls	(Ton)	Span	(ft)	Percent	Impact	Lane
Annual Permit 1	Permit	1.347	STRENGTH-II Concrete Flexure	111.13	1	78.94	100.0	As Requested	As Requested
Annual Permit 2	Permit	1.434	STRENGTH-II Concrete Flexure	118.28	1	78.94	100.0	As Requested	As Requested
EV2	Legal	2.921	STRENGTH-I Concrete Flexure	83.97	11	31.20	60.0	As Requested	As Requested
EV3	Legal	1.886	STRENGTH-I Concrete Flexure	81.10	11	31.20	60.0	As Requested	As Requested
Gravel Truck	Legal	2.051	STRENGTH-I Concrete Flexure	75.90	11	31.20	60.0	As Requested	As Requested
Gravel Truck + Lane Load	Legal	1.434	STRENGTH-I Concrete Flexure	79.56	1	78.94	100.0	As Requested	As Requested
H 15-44	Inventory	2.373	STRENGTH-I Concrete Flexure	35.59	1	78.94	100.0	As Requested	As Requested
H 15-44	Operating	3.076	STRENGTH-I Concrete Flexure	46.13	1	78.94	100.0	As Requested	As Requested
HL-93 (US)	Inventory	0.843	STRENGTH-I Concrete Flexure	30.37	1	78.94	100.0	As Requested	As Requested
HL-93 (US)	Operating	1.093	STRENGTH-I Concrete Flexure	39.36	1	78.94	100.0	As Requested	As Requested
HS 20-44	Inventory	1.193	SERVICE-III PS Tensile Stress	42.95	1	39.16	49.6	As Requested	As Requested
HS 20-44	Operating	2.307	STRENGTH-I Concrete Flexure	83.04	1	78.94	100.0	As Requested	As Requested
Lane-Type Legal Load	Legal	1.618	STRENGTH-I Concrete Flexure	64.72	1	78.94	100.0	As Requested	As Requested
Overweight Permit	Permit	1.049	STRENGTH-II Concrete Flexure	133.69	1	78.94	100.0	As Requested	As Requested
SU7	Legal	2.017	STRENGTH-I Concrete Flexure	78.16	1	78.94	100.0	As Requested	As Requested
School Bus - Standard	Legal	5.026	STRENGTH-I Concrete Flexure	72.87	11	31.20	60.0	As Requested	As Requested
Type 3S2	Legal	2.445	STRENGTH-I Concrete Flexure	88.04	1	78.94	100.0	As Requested	As Requested

Note:
"N/A" indicates not applicable
"***" indicates not available

Bridge Name: I-40 EB over Wolf River
NBI Structure ID: 79100400059
Bridge ID: 79100400059

Analyzed By: bridgeware
Analyze Date: Wednesday, June 17, 2020 14:59:22
Analysis Engine: AASHTO LRFR Engine Version 6.8.1.3001
Analysis Preference Setting: None

Report By: bridgeware
Report Date: Wednesday, June 17, 2020 19:26:58

Structure Definition Name: Girder Lines
Member Name: Ramp Interior
Member Alternative Name: Ramp Interior

Load and Resistance Factor Rating Summary

		Girder Summary							
		Rating		Capacity	Location				
Live Load		Factor	Controls	(Ton)	Span	(ft)	Percent	Impact	Lane
Annual Permit 1	Permit	1.206	STRENGTH-II Concrete Flexure	99.51	1	78.94	100.0	As Requested	As Requested
Annual Permit 2	Permit	1.285	STRENGTH-II Concrete Flexure	106.04	1	78.94	100.0	As Requested	As Requested
EV2	Legal	2.131	STRENGTH-I Concrete Flexure	61.28	11	31.20	60.0	As Requested	As Requested
EV3	Legal	1.376	STRENGTH-I Concrete Flexure	59.19	11	31.20	60.0	As Requested	As Requested
Gravel Truck	Legal	1.497	STRENGTH-I Concrete Flexure	55.39	11	31.20	60.0	As Requested	As Requested
Gravel Truck + Lane Load	Legal	1.283	STRENGTH-I Concrete Flexure	71.21	1	78.94	100.0	As Requested	As Requested
H 15-44	Inventory	1.904	SERVICE-III PS Tensile Stress	28.56	4	39.30	50.0	As Requested	As Requested
H 15-44	Operating	2.750	STRENGTH-I Concrete Flexure	41.25	1	78.94	100.0	As Requested	As Requested
HL-93 (US)	Inventory	0.720	SERVICE-III PS Tensile Stress	25.91	4	39.30	50.0	As Requested	As Requested
HL-93 (US)	Operating	0.978	STRENGTH-I Concrete Flexure	35.20	1	78.94	100.0	As Requested	As Requested
HS 20-44	Inventory	0.967	SERVICE-III PS Tensile Stress	34.82	4	39.30	50.0	As Requested	As Requested
HS 20-44	Operating	1.821	STRENGTH-I Concrete Flexure	65.57	11	31.20	60.0	As Requested	As Requested
Lane-Type Legal Load	Legal	1.447	STRENGTH-I Concrete Flexure	57.87	1	78.94	100.0	As Requested	As Requested
Overweight Permit	Permit	1.275	STRENGTH-II Concrete Flexure	162.53	1	78.94	100.0	As Requested	As Requested
SU7	Legal	1.529	STRENGTH-I Concrete Flexure	59.24	11	31.20	60.0	As Requested	As Requested
School Bus - Standard	Legal	3.667	STRENGTH-I Concrete Flexure	53.18	11	31.20	60.0	As Requested	As Requested
Type 3S2	Legal	2.192	STRENGTH-I Concrete Flexure	78.93	1	78.94	100.0	As Requested	As Requested

Note:
"N/A" indicates not applicable
"***" indicates not available

Bridge Name: I-40 EB over Wolf River
NBI Structure ID: 79100400059
Bridge ID: 79100400059

Analyzed By: bridgeware
Analyze Date: Wednesday, June 17, 2020 14:59:22
Analysis Engine: AASHTO LRFR Engine Version 6.8.1.3001
Analysis Preference Setting: None

Report By: bridgeware
Report Date: Wednesday, June 17, 2020 19:27:03

Structure Definition Name: Girder Lines
Member Name: Mainline Interior
Member Alternative Name: Mainline Interior

Load and Resistance Factor Rating Summary

		Girder Summary							
Live Load		Rating	Controls	Capacity	Span	Location	Percent	Impact	Lane
		Factor		(Ton)		(ft)			
Annual Permit 1	Permit	1.349	STRENGTH-II Concrete Flexure	111.28	1	78.94	100.0	As Requested	As Requested
Annual Permit 2	Permit	1.440	STRENGTH-II Concrete Flexure	118.83	1	78.94	100.0	As Requested	As Requested
EV2	Legal	2.208	STRENGTH-I Concrete Flexure	63.49	11	35.34	60.0	As Requested	As Requested
EV3	Legal	1.432	STRENGTH-I Concrete Flexure	61.55	11	35.34	60.0	As Requested	As Requested
Gravel Truck	Legal	1.547	STRENGTH-I Concrete Flexure	57.25	11	35.34	60.0	As Requested	As Requested
Gravel Truck + Lane Load	Legal	1.434	STRENGTH-I Concrete Flexure	79.57	1	78.94	100.0	As Requested	As Requested
H 15-44	Inventory	2.024	SERVICE-III PS Tensile Stress	30.36	8	40.61	50.0	As Requested	As Requested
H 15-44	Operating	3.067	STRENGTH-I Concrete Flexure	46.00	1	78.94	100.0	As Requested	As Requested
HL-93 (US)	Inventory	0.761	SERVICE-III PS Tensile Stress	27.40	6	40.61	50.0	As Requested	As Requested
HL-93 (US)	Operating	1.091	STRENGTH-I Concrete Flexure	39.28	1	78.94	100.0	As Requested	As Requested
HS 20-44	Inventory	1.024	SERVICE-III PS Tensile Stress	36.88	8	40.61	50.0	As Requested	As Requested
HS 20-44	Operating	1.858	STRENGTH-I Concrete Flexure	66.88	11	35.34	60.0	As Requested	As Requested
Lane-Type Legal Load	Legal	1.614	STRENGTH-I Concrete Flexure	64.57	1	78.94	100.0	As Requested	As Requested
Overweight Permit	Permit	1.422	STRENGTH-II Concrete Flexure	181.35	1	78.94	100.0	As Requested	As Requested
SU7	Legal	1.566	STRENGTH-I Concrete Flexure	60.68	11	35.34	60.0	As Requested	As Requested
School Bus - Standard	Legal	3.821	STRENGTH-I Concrete Flexure	55.40	11	35.34	60.0	As Requested	As Requested
Type 3S2	Legal	2.286	STRENGTH-I Concrete Flexure	82.30	11	35.34	60.0	As Requested	As Requested

Note:
"N/A" indicates not applicable
"***" indicates not available

Bridge Name: I-40 EB over Wolf River
NBI Structure ID: 79I00400059
Bridge ID: 79I00400059

THIS MEMBER CONTROLS LOAD RATING

Analyzed By: bridgeware
Analyze Date: Wednesday, June 17, 2020 14:59:22
Analysis Engine: AASHTO LRFR Engine Version 6.8.1.3001
Analysis Preference Setting: None

Report By: bridgeware
Report Date: Wednesday, June 17, 2020 19:27:06

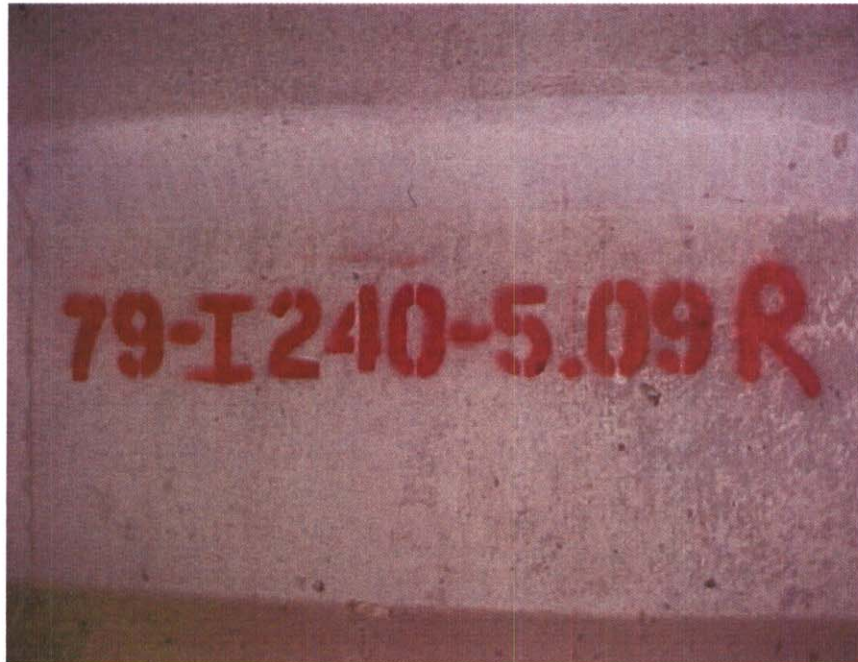
Structure Definition Name: Girder Lines
Member Name: Mainline Exterior
Member Alternative Name: Mainline Exterior

Load and Resistance Factor Rating Summary

		Girder Summary							
		Rating		Capacity	Location				
Live Load		Factor	Controls	(Ton)	Span	(ft)	Percent	Impact	Lane
Annual Permit 1	Permit	1.075	STRENGTH-II Concrete Flexure	88.65	1	78.94	100.0	As Requested	As Requested
Annual Permit 2	Permit	1.147	STRENGTH-II Concrete Flexure	94.67	1	78.94	100.0	As Requested	As Requested
EV2	Legal	2.110	STRENGTH-I Concrete Flexure	60.67	11	35.34	60.0	As Requested	As Requested
EV3	Legal	1.368	STRENGTH-I Concrete Flexure	58.83	11	35.34	60.0	As Requested	As Requested
Gravel Truck	Legal	1.479	STRENGTH-I Concrete Flexure	54.71	11	35.34	60.0	As Requested	As Requested
Gravel Truck + Lane Load	Legal	1.142	STRENGTH-I Concrete Flexure	63.39	1	78.94	100.0	As Requested	As Requested
H 15-44	Inventory	1.683	SERVICE-III PS Tensile Stress	25.24	8	40.71	50.0	As Requested	As Requested
H 15-44	Operating	2.443	STRENGTH-I Concrete Flexure	36.65	1	78.94	100.0	As Requested	As Requested
HL-93 (US)	Inventory	0.636	SERVICE-III PS Tensile Stress	22.91	8	40.71	50.0	As Requested	As Requested
HL-93 (US)	Operating	0.869	STRENGTH-I Concrete Flexure	31.30	1	78.94	100.0	As Requested	As Requested
HS 20-44	Inventory	0.851	SERVICE-III PS Tensile Stress	30.65	8	40.71	50.0	As Requested	As Requested
HS 20-44	Operating	1.775	STRENGTH-I Concrete Flexure	63.92	11	35.34	60.0	As Requested	As Requested
Lane-Type Legal Load	Legal	1.286	STRENGTH-I Concrete Flexure	51.44	1	78.94	100.0	As Requested	As Requested
Overweight Permit	Permit	1.045	STRENGTH-II Concrete Flexure	133.19	1	78.94	100.0	As Requested	As Requested
SU7	Legal	1.497	STRENGTH-I Concrete Flexure	57.99	11	35.34	60.0	As Requested	As Requested
School Bus - Standard	Legal	3.652	STRENGTH-I Concrete Flexure	52.95	11	35.34	60.0	As Requested	As Requested
Type 3S2	Legal	1.957	STRENGTH-I Concrete Flexure	70.46	1	78.94	100.0	As Requested	As Requested

Note:
 "N/A" indicates not applicable
 "***" indicates not available

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



BRIDGE NO.



SPAN #8

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



LOOKING AHEAD ON ROUTE



VIEW ACROSS TOP OF DECK

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



LOOKING UPSTREAM FROM TOP OF BRIDGE



LOOKING BACK ON ROUTE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



APPROACH #2 ASPHALT SPALLING



LOOKING BACK ON ROUTE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



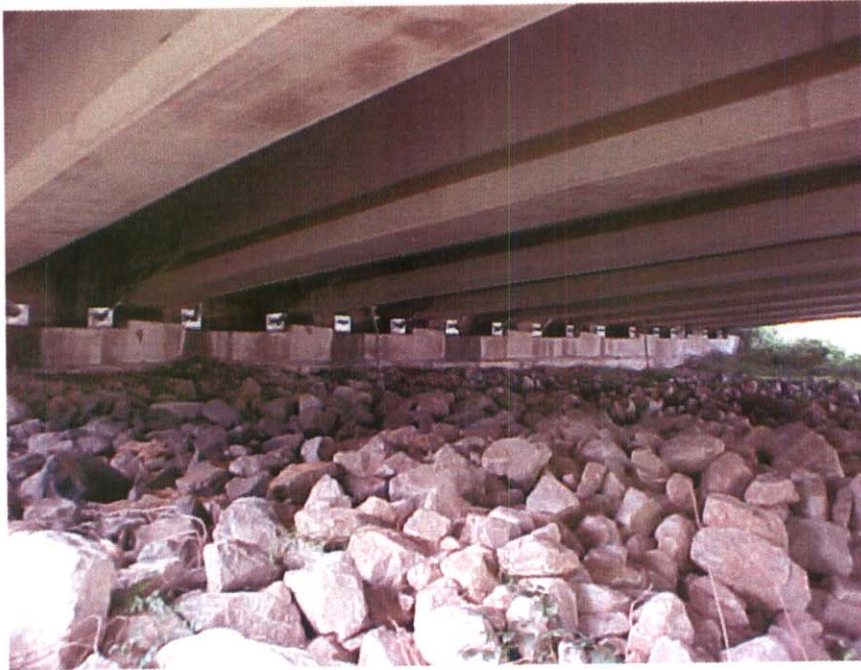
ABUTMENT #2 "A"



LOOKING DOWNSTREAM FROM TOP OF BRIDGE

Bridge Loc. No: 79 - I0040 - 05.09 - R

Date: 08-03-01

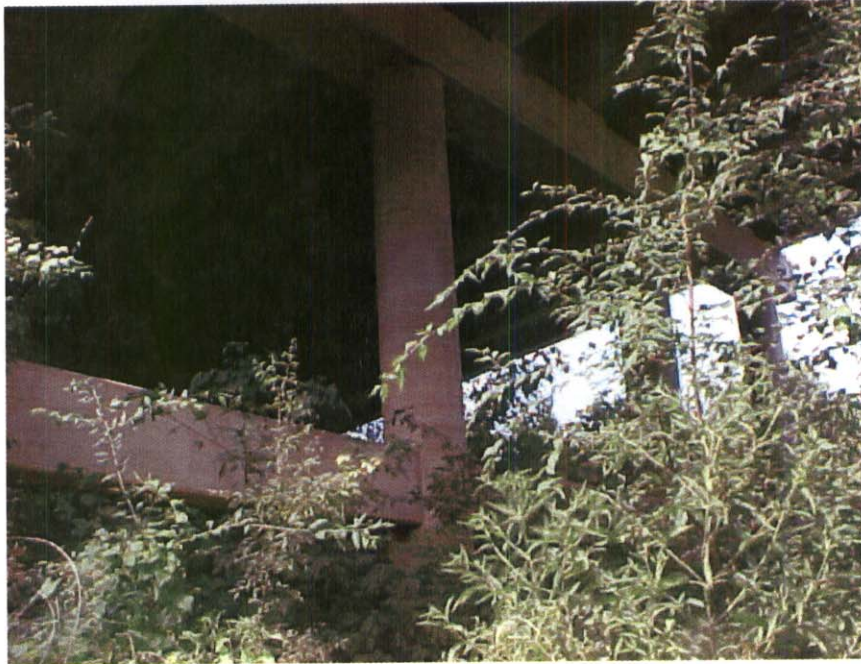


ABUTMENT #1



SPAN #1, BOTTOM OF DECK

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



BENT #1



BENT #4 & DRIFT

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



ELEVATION LEFT SIDE



LEFT VIEW OF BENTS #5 & #6

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



**APPROACH #1 HAS JOINT MATERIAL MISSING,
TYPICAL OF APPROACH #2**



ASPHALT SPALLING AT APPROACH #1

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01

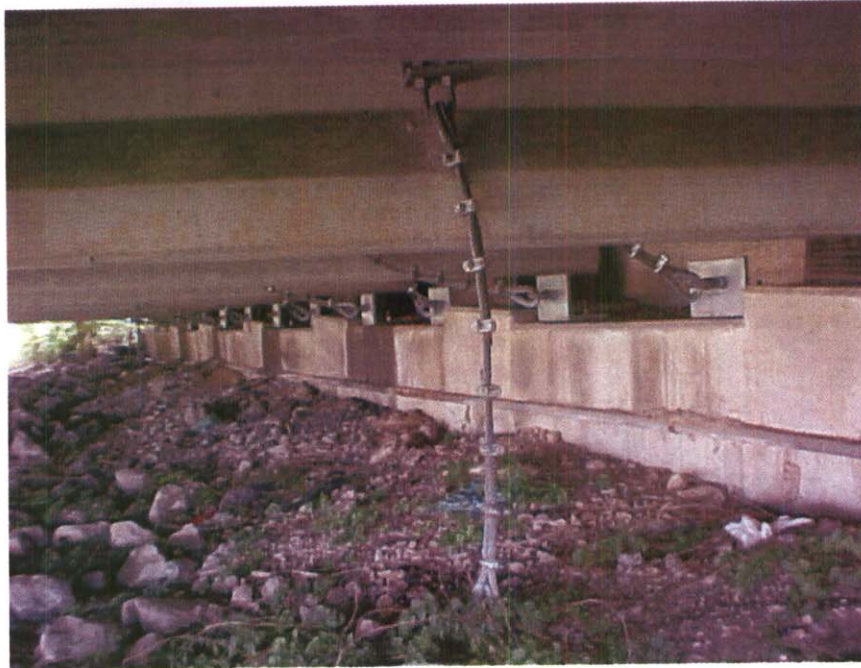


BENT #10



BENT #8, REAR SIDE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



SPAN #1 EARTHQUAKE DEVICE IS BROKEN



SPAN #1 EARTHQUAKE DEVICE IS BROKEN

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



SPAN #1 EARTHQUAKE DEVICE IS BROKEN



SPAN #1 EARTHQUAKE DEVICE IS BROKEN

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 08-03-01



SPAN #1 EARTHQUAKE DEVICE IS BROKEN



SPAN #1 EARTHQUAKE DEVICE IS BROKEN

BRIDGE INSPECTION REPORT

AUG 02 2001

Form BIR 3.0
(Rev. 9-22-98)
DT-0069

Field Report No. 15 Date 8-2-01
Previous Report No. 14 Date 9-16-99
Plans: YES (X) NO ()

Bridge No. 79100400059 Bridge Location No. 79 - 10040 - 5.09 R
Eleven Digit No. Co. Route Log Mile OVER/UNDER PASS

Road Name _____ over WOLF RIVER
Crossing _____
Year Constructed _____ County Shelby Structure Name (If Named) _____
Year Widened _____ Year Rehabilitated _____ Maintenance District 45

FEATURES

Wearing Surface Concrete () Timber () Asphalt (X) Depth _____ (in.)
Flared Width Yes (X) No () Median Width Open (X) None () Closed ()
Navigational Control Yes () No (X) Bridge Skew 60° LT (X) RT ()
Structure Type (Main Span) CONC. I. BEAM
Structure Type (Appr. Spans) _____
No. Main Spans 11 No. Approach Spans _____
Maximum Span Length 81.7' (**. ft.)
Total Length 830.0 (**. ft.)

INSPECTORS

1. COLLINS
2. LOUG
3. ADAMS
4. BYRD
5. REBLES
6. _____
7. _____
8. _____

WIDTHS (ft.)

Deck Out-to-Out 74/75
Roadway Curb/Curb 72/106
Roadway Rail/Rail _____
Sidewalk Rt. _____ Lt. _____
*Approach Roadway 60
*(Does Not Include Shoulders)
Approach Shoulder Rt. 12
Lt. 12

CLEARANCES

Min. Vertical Clearance over Deck 5 (ft.-in.)
Min. Vertical Under Clearance 5 (ft.-in.)
Min. Lateral Under Clearance Rt. 5 (ft.)
Min. Lateral Under Clearance Lt. 5 (ft.)

FRACTURE CRITICAL: N/A
(If Yes, Include BIR 3.9)

NBIS Bridge Length (<25 ft.) N/A (ft.-in.)

UNDERWATER INSPECTION

To Be Performed By: _____

Date _____

DOT FIELD TEAM () CONTRACT DIVERS () NONE REQUIRED ()

Change in Structural Condition: Yes () No (X)

COMMENTS:

Major Repairs Made: Yes () No (X)

BRIDGE RATING: () (4) () ()

GOOD FAIR POOR CRITICAL

Supervising Bridge Inspector: [Signature]

AUG 11 2001

Date _____

PERFORMANCE EVALUATION

Time of Day Inspected 10:00 Weather Conditions _____
Vehicles Observed ALL TYPES

LIVE LOAD BEHAVIOR

Substructure	YES	NO	Comments
Horiz./ Vert. Defl.	()	(X)	
Vibration	()	(X)	
Superstructure			
Horiz./ Vert. Defl.	()	(X)	
Vibration	()	(X)	

APPROACH

	Rating	Comments
Alignment	(G) F P C	
Slab	G F P C	W/IV COVERED WITH P.C.
Joints	G F (P) C	APP#1 & #2 MAT MISSING (007)
Pavement	G F (P) C	APP#1 & #2 UNEVEN 1" (001)
Embankment	(G) F P C	
Drains	G F (P) C	APP#1 & #2 RT (009) OPENING BLOCKED

TRAFFIC SAFETY FEATURES

	Rating	STANDARD/ SUB-STANDARD	Comments
Bridgerailing	G (F) P C	(X) ()	
Transitions	(G) F P C	() (X)	
Guardrail	(G) F P C	() (X)	
Guardrail Terminal	(G) F P C	() (X)	

SIGNING

	YES	NO	NEEDED	Weight Limit Posted
Paddleboards	()	(X)	()	YES () NO (X)
Vertical Clearance (<14'-6")	()	(X)	()	Gross..... Tons
NARROW ()	()	(X)	()	2 Axle..... Tons
ONE LANE BRIDGE ()	()	(X)	()	3 or more Axles.. Tons

Other Signs or Plaques: OVERHEAD TRIANGLES CENTER BRIDGES

Comments Regarding any
Problems with Signing: _____

Form BIR 3.2
(Rev. 9-22-98)
DT-0081

Bridge Location No. 79 - 10040 - 5.09 R
Co. Route Log Mile

AUG 6, 1998
Date _____

DECK

	Rating	Comments
Wearing Surface	G (F) P C	
Deck - Structural Condition	(G) F P C	
Curbs	G F P C	
Median	G F P C	
Sidewalks	G F P C	
Parapet	G (F) P C	
Railing	G F P C	
Paint	G F P C	
Drains	(G) F P C	
Lighting Standards	G (F) P C	
Utilities	G F P C	
Joint Leakage	(G) F P C	
Expansion Joints	G F P C	CONT DECK

SUPERSTRUCTURE

Bearing Devices	G F P C	
Beams <i>CIB</i>	(G) F P C	
Girders	G F P C	
P C C S	G F P C	
BOLTS (PCCS)	G F P C	
Floor Beams	G F P C	
Stringers	G F P C	
Diaphragms	(G) F P C	
Bracing <i>EQ DEVICES</i>	G (F) P C	ABUT #1
Trusses - General	G F P C	
Portals	G F P C	
Bracing	G F P C	
Paint	G F P C	
Alignment of Members	(G) F P C	

TEXTURE COAT

Condition Rating	G (F) P C
Overall Appearance	G (F) P C
Staining Rating	G (F) P C

Fading	G (F) P C
Needs Spot Painting	YES () NO (X)
Needs Repainting	YES () NO (X)

Comments _____

RECOMMENDATIONS: _____

Scaling Rating	G (F) P C
CLEAN SEAL JOINTS ()	
CLEAN DRAINS ()	

Form BIR 3.3
(Rev. 9-22-98)
DT-0082

Bridge Location No. 79 - 10040 - 5.09 R
Co. Route Log Mile

AUG 05 2001

Date _____

SUBSTRUCTURE

PILES TO BE
REPLACED

ABUTMENTS

	Rating	Comments	PILE(S)	ABUTMENT
Caps	G <u>F</u> P C			
Breastwall	G F P C			
Wings	<u>G</u> F P C			
Backwall	G <u>F</u> P C			
Plumb	<u>G</u> F P C			
Footings	G F P C			
Piles	G F P C	<i>ND</i>		
Embankment	<u>G</u> F P C			
Bearing	G <u>F</u> P C			
Slope Paving	G F P C			
Rip Rap	<u>G</u> F P C			
Earthquake Devices	G F <u>P</u> C	<i>ABUTMENT 1 4 LOOSE U BOLTS BROKE</i>		

PIERS

	Rating	Comments	PILE(S)	PIER
Caps	G F P C			
Columns	G F P C			
Plumb	G F P C			
Footings	G F P C	<i>ND</i>		
Piles	G F P C			
Bearing	G F P C			
Web	G F P C			
Earthquake Devices	G F P C			

BENTS

	Rating	Comments	PILE(S)	BENT
Caps	G <u>F</u> P C			
Columns	G <u>F</u> P C			
Plumb	<u>G</u> F P C			
Footings	G F P C			
Piles <i>DRIFT</i>	G F <u>P</u> C	<i>BENT #4 AROUND BENT (201) E #5</i>		
Bearing	<u>G</u> F P C			
<i>STRUTS</i>	<u>G</u> F P C			
Earthquake Devices	G F P C	<i>NONE</i>		

Piles Need Replacement: NO (X) YES ()

CUT VEGETATION NO () YES (X) (204)

CLEAR DRIFT NO () YES (X) (201)

RECOMMENDATIONS:

STREAM CHANNEL DATA AND CONDITIONS

Stream Crossing: WOLF RIVER

- I. 1. Type of bed material? SAND
2. Has channel shifted? YES () NO () NOT APPARENT (☒)
3. Condition of rip-rap? G (☒) P C Est. % failed _____ % N/A ()
4. Overall condition of channel? G (☒) P C
5. Item 61 - Code values 0 thru 9 according to the recording
and coding guide currently in effect: 6
6. Underwater diver inspection recommended? YES (☒) NO ()
If yes, why? _____
- II. Channel and bank stability conditions: (check if applicable)
1. Steep bank conditions: - Failures upstream () Failures downstream ()
2. Moderate bank erosion (☒)
3. Bank vegetation: a. low growth (☒) b. large timber (☒) c. clear banks (☒)
d. dead trees upstream (☒) e. dead trees downstream (☒)
4. Sediment or gravel accumulation: YES () NO () UNKNOWN (☒)
5. Channel altered or straightened: YES () NO () UNKNOWN (☒)
6. Stable conditions: a. live growth (☒) b. bedrock ()
c. boulders () d. flat slopes ($\leq 2:1$) ()
- III. Waterway adequacy and debris characteristics: (check if applicable)
1. Bridge deck elevations:
a. level with approach roadway. (☒)
b. higher than approach roadway. ()
c. roadway approach ≥ 2 ft. above natural ground line. (☒)
2. Abutment encroaches into channel. ()
3. Large scour (blowhole) under bridge. ()
4. Indications that flood waters overtop bridge:
NO (☒) YES () OCASSIONALLY () FREQUENTLY () UNKNOWN ()
5. Debris characteristics:
a. debris/drift present YES (☒) NO ()
b. debris/drift likely to accumulate YES (☒) NO ()
c. dead trees upstream (☒) dead trees downstream (☒)
- IV. Comments: THERE IS A DRIFT ALL THE WAY ACROSS
BENT #4 AND ON THE RT. SIDE OF BENT #5.

SPECIAL INSPECTION DATA - FOR REASONS OTHER THAN FC OR SCOUR

- I. Does this bridge need a special inspection? YES () NO (☒)
II. Reason for special inspection: _____

Inspection Team's Summary

Bridge Location No. 79 - 10040 - 5.09 R

Inspection Date 8-2-01

Bridge Rating FAIR

AUG 03 2001

THIS ELEVEN SPAN C.I.B BRIDGE WITH
CONCRETE SUBSTRUCTURE IS IN FAIR CONDITION.
ALL TRAFFIC SAFETY FEATURES ARE PRESENT.
APP. # 1 & 2 PAVEMENT IS CRACKING & UNEVEN.
APP. # 1 & 2 JOINTS HAVE MATERIAL MISSING.
DRAINS ARE 100% FILLED APP. # 1 & 2 RT SIDES.
ABUT # 1 HAS FOUR EARTHQUAKE DEVICES
BROKE. THERE IS A DRIFT AROUND BENT # 4.
& # 5 UNDER BRIDGES. SCOUR FAIR

Carolyn Adams

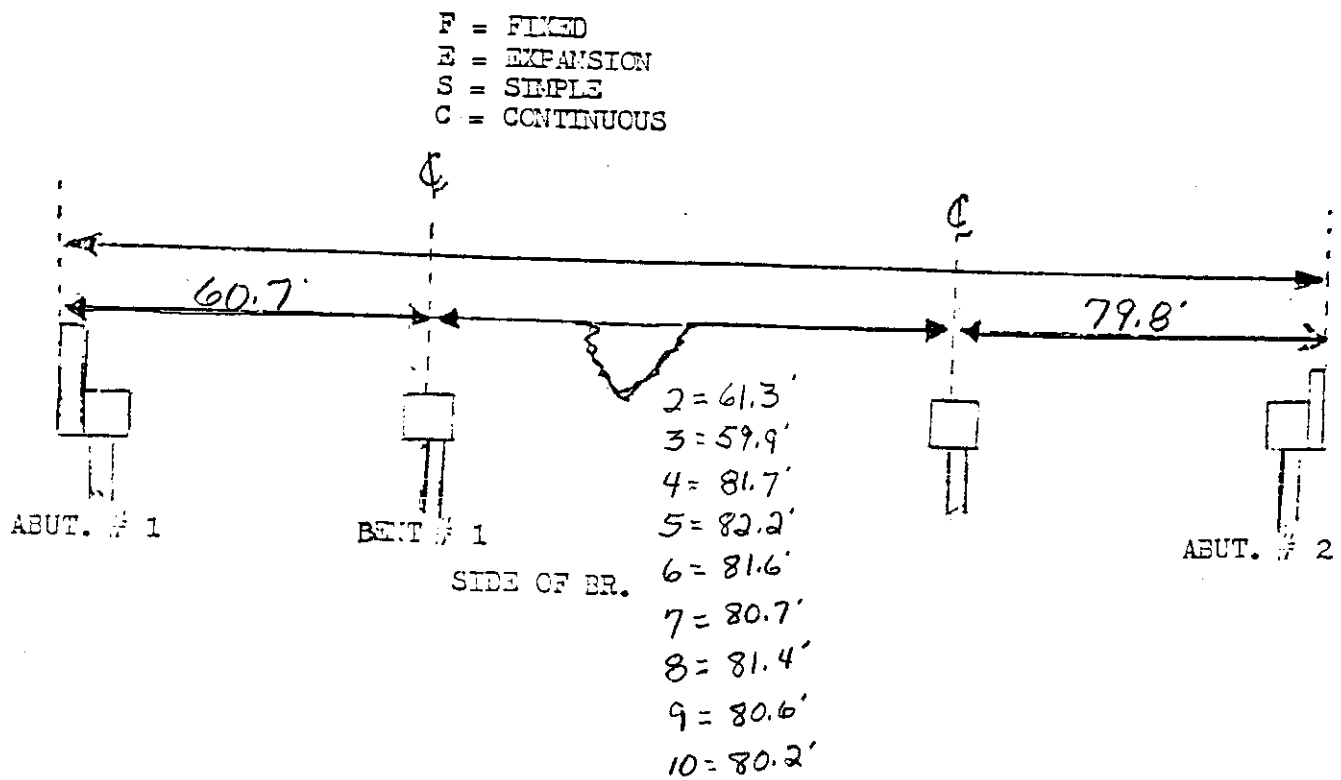
Cross Section: yes () no (X)
N/A

Pontis: yes () no (X)

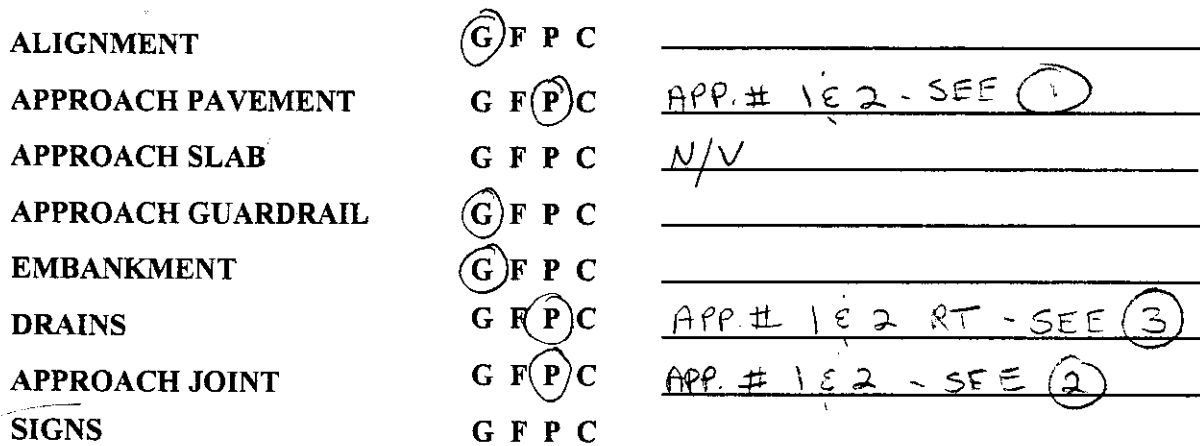
BR. NO. 79-I-40-5.09R

AUG 04 2000

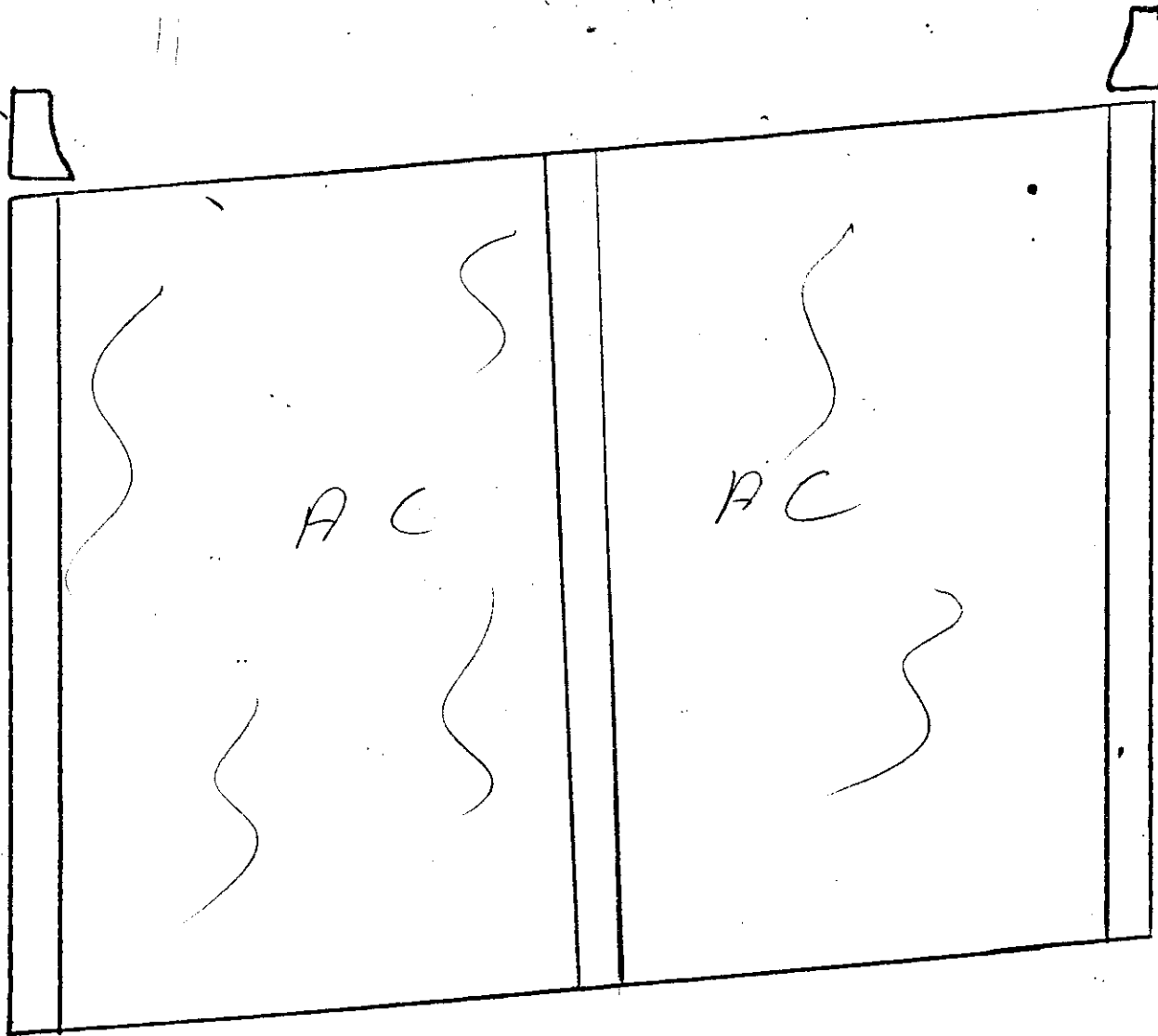
DIR. OF ROUTE →



CO.	ROUTE	LOG MILE	L/R	SKEW
01	1	0.00	R	0.00
01	1	0.01	R	0.00
01	1	0.02	R	0.00
01	1	0.03	R	0.00
01	1	0.04	R	0.00
01	1	0.05	R	0.00
01	1	0.06	R	0.00
01	1	0.07	R	0.00
01	1	0.08	R	0.00
01	1	0.09	R	0.00
01	1	0.10	R	0.00
01	1	0.11	R	0.00
01	1	0.12	R	0.00
01	1	0.13	R	0.00
01	1	0.14	R	0.00
01	1	0.15	R	0.00
01	1	0.16	R	0.00
01	1	0.17	R	0.00
01	1	0.18	R	0.00
01	1	0.19	R	0.00
01	1	0.20	R	0.00
01	1	0.21	R	0.00
01	1	0.22	R	0.00
01	1	0.23	R	0.00
01	1	0.24	R	0.00
01	1	0.25	R	0.00
01	1	0.26	R	0.00
01	1	0.27	R	0.00
01	1	0.28	R	0.00
01	1	0.29	R	0.00
01	1	0.30	R	0.00
01	1	0.31	R	0.00
01	1	0.32	R	0.00
01	1	0.33	R	0.00
01	1	0.34	R	0.00
01	1	0.35	R	0.00
01	1	0.36	R	0.00
01	1	0.37	R	0.00
01	1	0.38	R	0.00
01	1	0.39	R	0.00
01	1	0.40	R	0.00
01	1	0.41	R	0.00
01	1	0.42	R	0.00
01	1	0.43	R	0.00
01	1	0.44	R	0.00
01	1	0.45	R	0.00
01	1	0.46	R	0.00
01	1	0.47	R	0.00
01	1	0.48	R	0.00
01	1	0.49	R	0.00
01	1	0.50	R	0.00
01	1	0.51	R	0.00
01	1	0.52	R	0.00
01	1	0.53	R	0.00
01	1	0.54	R	0.00
01	1	0.55	R	0.00
01	1	0.56	R	0.00
01	1	0.57	R	0.00
01	1	0.58	R	0.00
01	1	0.59	R	0.00
01	1	0.60	R	0.00
01	1	0.61	R	0.00
01	1	0.62	R	0.00
01	1	0.63	R	0.00
01	1	0.64	R	0.00
01	1	0.65	R	0.00
01	1	0.66	R	0.00
01	1	0.67	R	0.00
01	1	0.68	R	0.00
01	1	0.69	R	0.00
01	1	0.70	R	0.00
01	1	0.71	R	0.00
01	1	0.72	R	0.00
01	1	0.73	R	0.00
01	1	0.74	R	0.00
01	1	0.75	R	0.00
01	1	0.76	R	0.00
01	1	0.77	R	0.00
01	1	0.78	R	0.00
01	1	0.79	R	0.00
01	1	0.80	R	0.00
01	1	0.81	R	0.00
01	1	0.82	R	0.00
01	1	0.83	R	0.00



BRIDGE NO. 79 T-10 509 SK. 60 RT. SPAN NO. 1



DECK	G	(F)	P	C
APET	G	(F)	P	C
INS	G	F	P	C
NT	G	F	P	C
ARD	G	F	P	C
IL				

$\frac{1}{4}$ " CRACKS
 FINE CRACKS
 N/A

1 AUG 03 2007

BRIDGE NO. 79 T-40 5.09

BENT NO. _____ SPAN NO. 1 ABT. NO. _____ PIER NO. _____

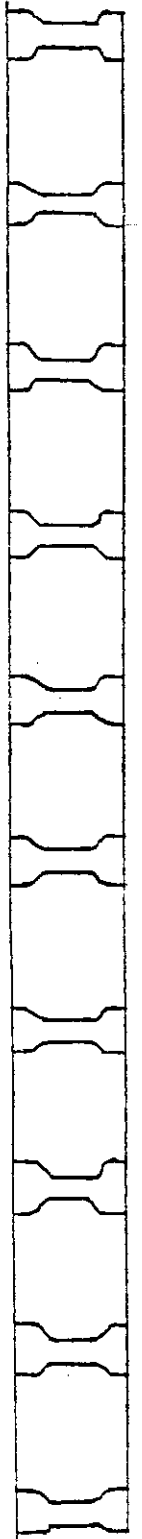
[illegible]

BR. NO. 79 T-40 3.09 SK. RT

AUG

SPAN NO. 1

	A	
	B	
	C	
	D	
	E	
	F	
	G	
	H	
	I	
	J	



BRIDGE NO.

791205.03SK. 60

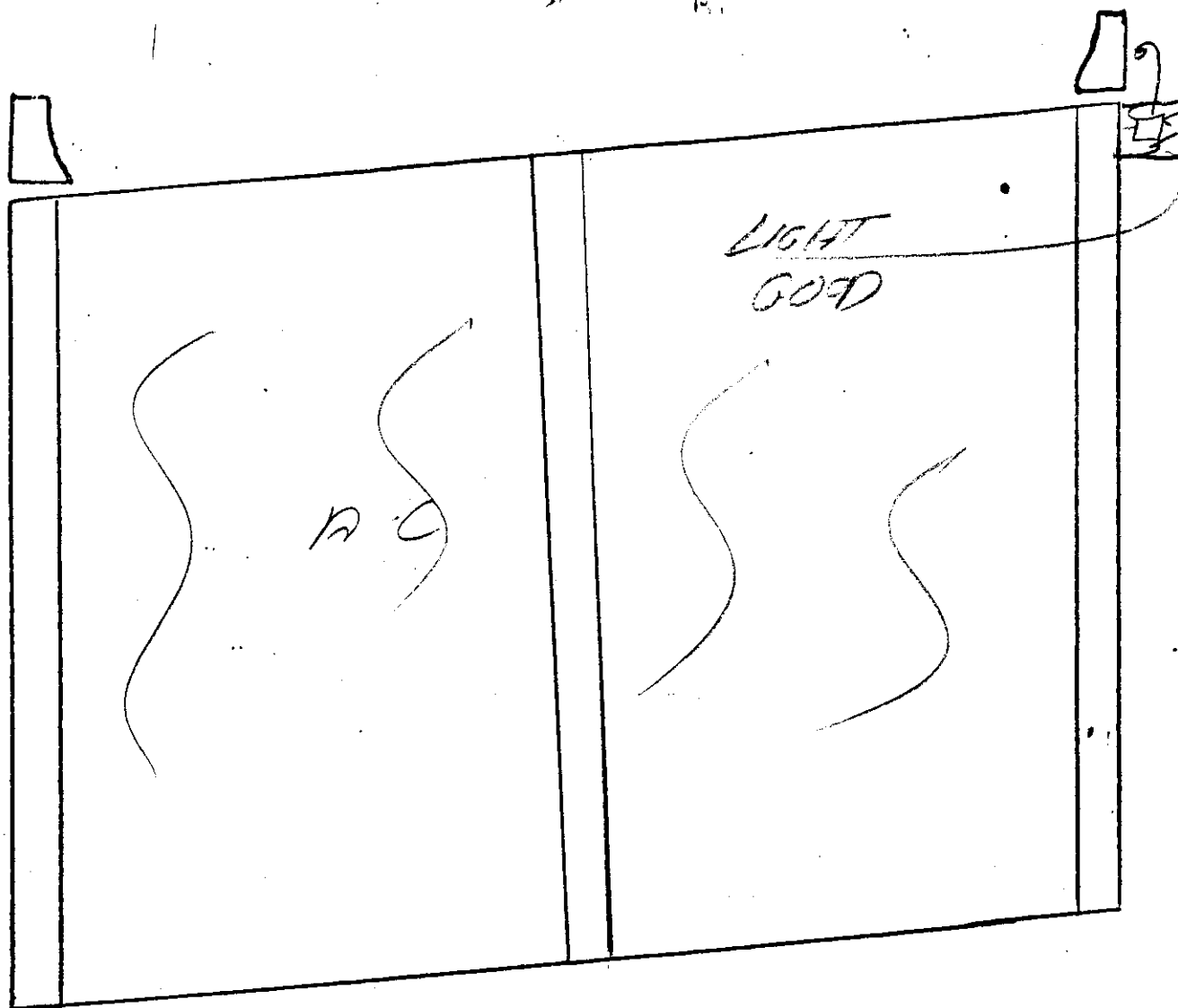
RT.

SPAN

NO.

2

RT



DECK

G (F) P C

 $\frac{1}{4}$ " CRACKS

APET

G (F) P C

FINE CRACKS

INS

G (F) P C

NT

G F P C

ARD

G F P C

IL

LIGHT (C)

AUG 07 2001

BENT NO. _____ SPAN NO. 2 ABT. NO. _____ PIER NO. _____

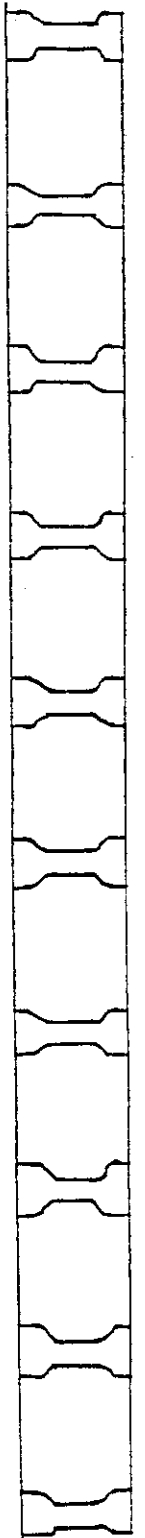
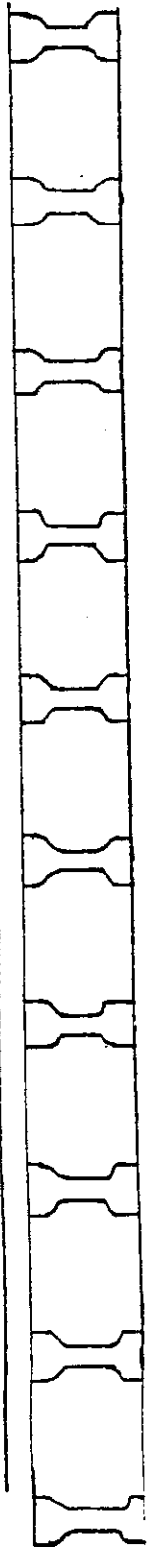
ELEMENT	RATING	COMMENTS
BOTTOM DECK	(G) F P C	RAILS
DIA.	(G) F P C	
CONC. I. B.		
A B	(G) F P C	
C D	(G) F P C	
E F	(G) F P C	
G H	(G) F P C	
I J	(G) F P C	
BACKWALLS	(G) F P C	RAILS

BR. NO. 79 T-405.00 SK. RT

SPAN NO. 2

2

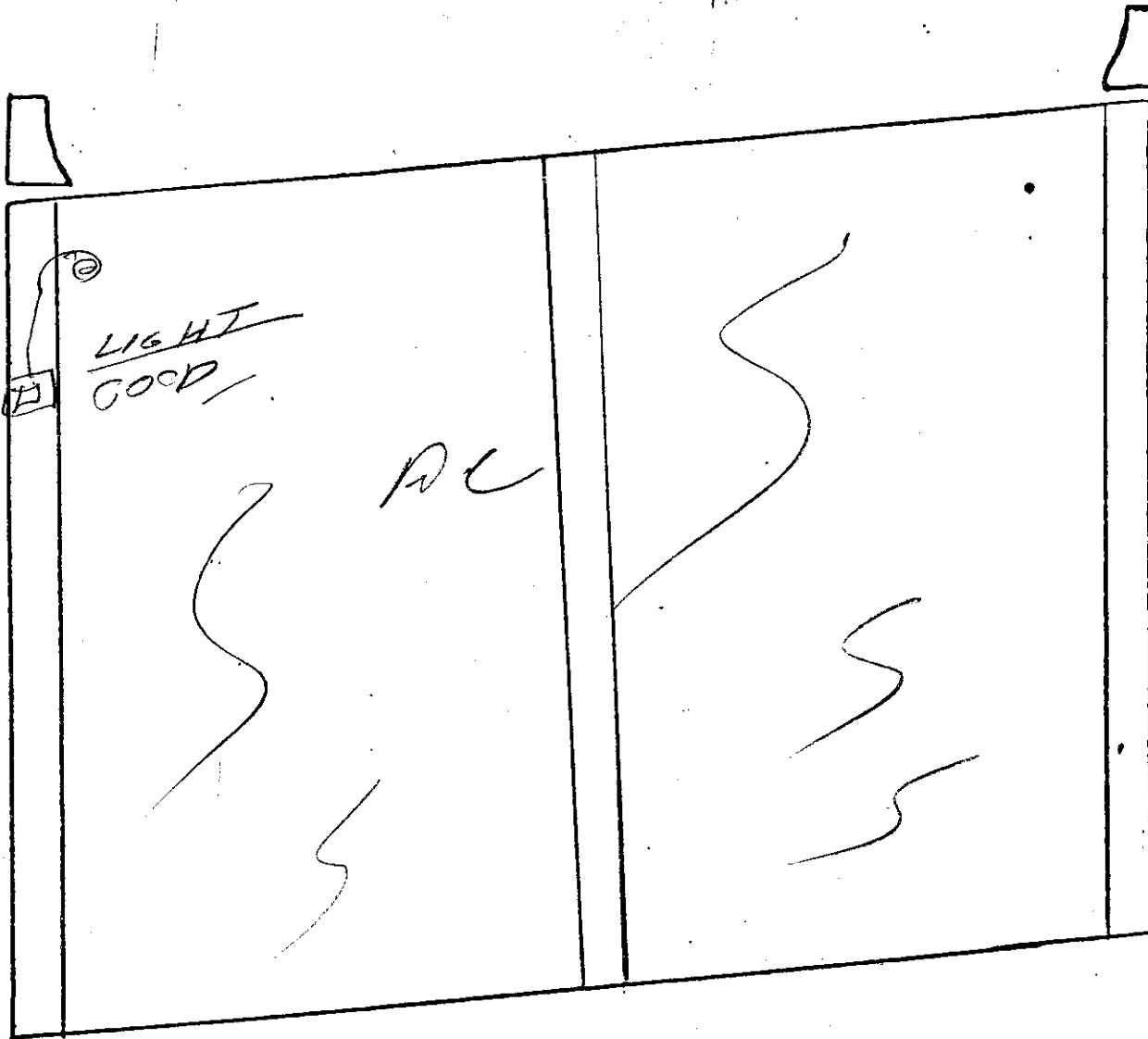
	A	
	B	
	C	
	D	
	E	
	F	
	G	
	H	
	I	
	J	



AUG 03 2001

BRIDGE NO. 29 I-410 5.09 SK. 60 RT. SPAN NO. 3

RT



DECK	G	(F)	P	C
PAVET	G	(F)	P	C
WINS	G	F	P	C
INT	G	F	P	C
WARD	G	F	P	C
IL				

FINE CRACKS 1/4" CRACKS

// //

DRAN (G)

2

BRIDGE NO. 79 T-40 5.09

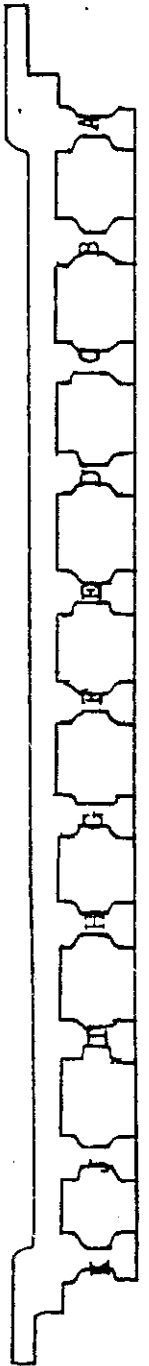
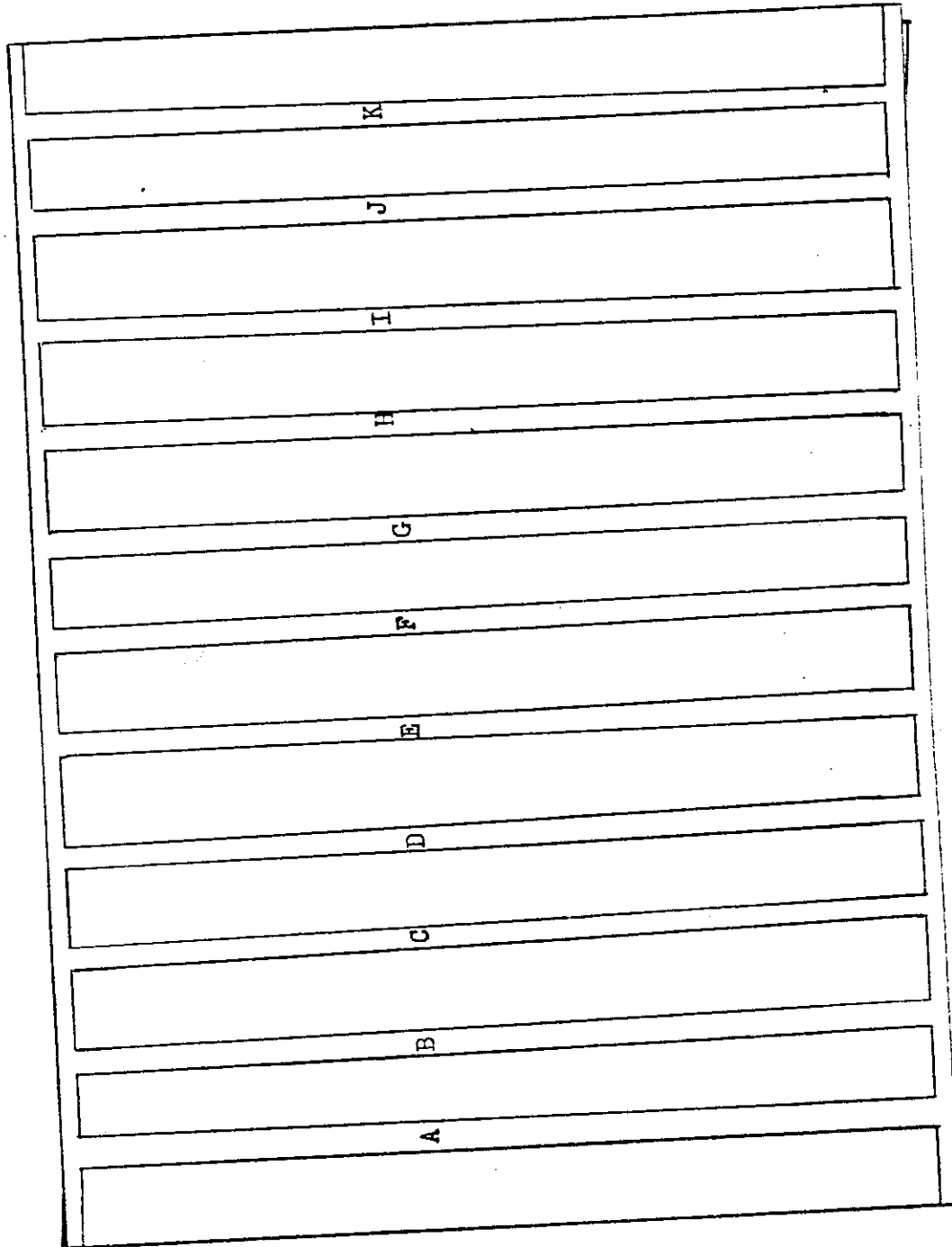
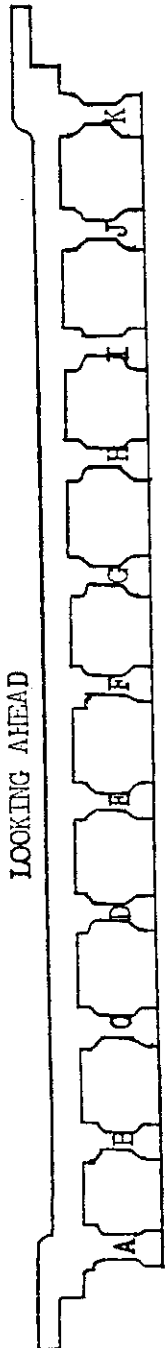
BENT NO. _____ SPAN NO. 3 ABT. NO. _____ PIER NO. _____

ELEMENT	PATING	COMMENTS
BOTTOM DECK	G F P C	SAME
CONC. I. BEAMS		
A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
J	G F P C	
K	G F P C	
DIA.	G F P C	
BACKWALLS	G F P C	N/A

BR. NO. 79 I-40 5.09 SK. 60^{RT}

SPAN NO. 3

AUG 08 2001



DIR. OF ROUTE

BRIDGE NO.

79

I-20

S. 99

SK. 60° RT.

SPAN

NO.

4

AUG 03 2001

OVERNHEAD

RT

TRIANGLE PARAPET TO
PARAPETCRACKING AROUND BASE WITH
EFFECTIVE

LIGHT

O.K.

AC

DIR. OF ROUTE

DECK G (F) P C

PARAPET G (F) P C

WINS G (F) P C

ENT G F P C

WARD G F P C

ILL

SIGN (F)

1/4" CRACKS

FINE CRACKS

TOP VIEW PARAPET
RT SIDE

KT

BRIDGE NO. 79 T-40 5.09

BENT NO. _____ SPAN NO. 4 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	(G) F P C	PAN 118
CONC. I. BEAMS		
A	(G) F P C	
B	(G) F P C	
C	(G) F P C	
D	(G) F P C	
E	(G) F P C	
F	(G) F P C	
G	(G) F P C	
H	(G) F P C	
I	(G) F P C	
J	(G) F P C	
K	(G) F P C	
DIA.	(G) F P C	
BACKWALLS	G F P C	118

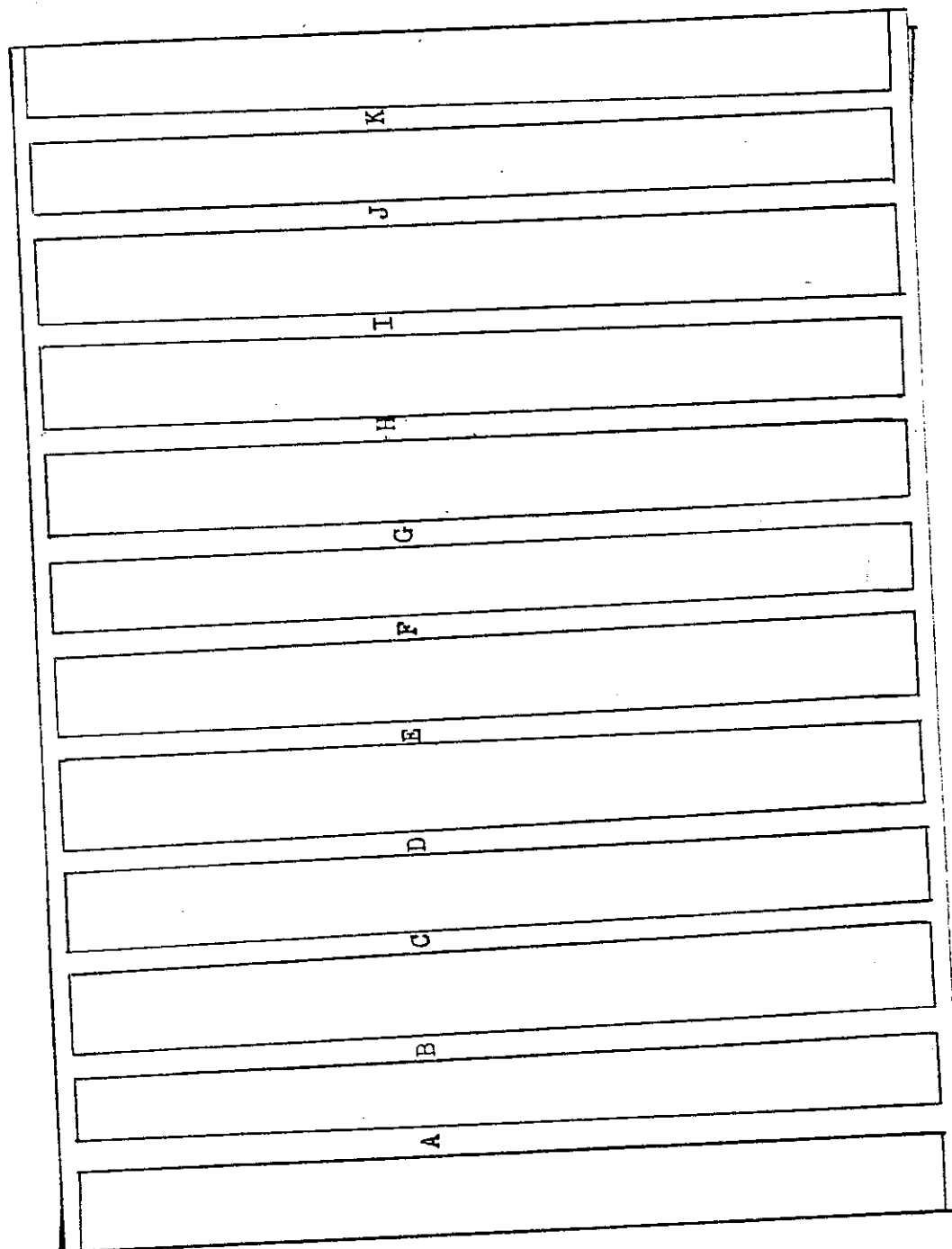
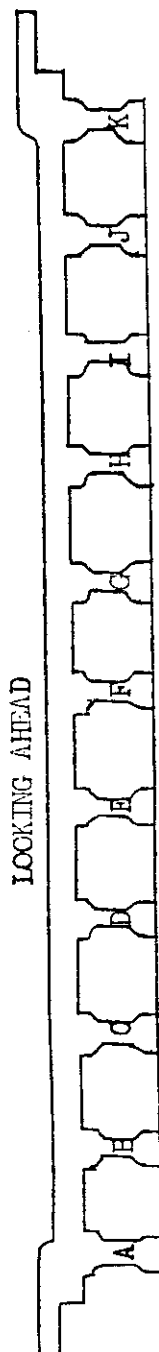
AUG 08 2001

BR. NO. 79 T-45 509 SK. 60² 27

SPAN NO. 4

RT

(Handwritten mark)



DIR. OF ROUTE

AUG 68 2001

BRIDGE NO. 29 T-LL 509 SK. 60° RT. SPAN NO. 5
RT



DECK	G	(F)	P	C
PAVET	G	(F)	P	C
WINS	G	F	P	C
INT	G	F	P	C
WARD	G	F	P	C
AIL				

$\frac{1}{4}$ " CRACKS
FINE CRACKS

AUG 03 2001

BRIDGE NO. 79 J-40 S.C.B.BENT NO. _____ SPAN NO. 5 ABT. NO. _____ PIER NO. _____

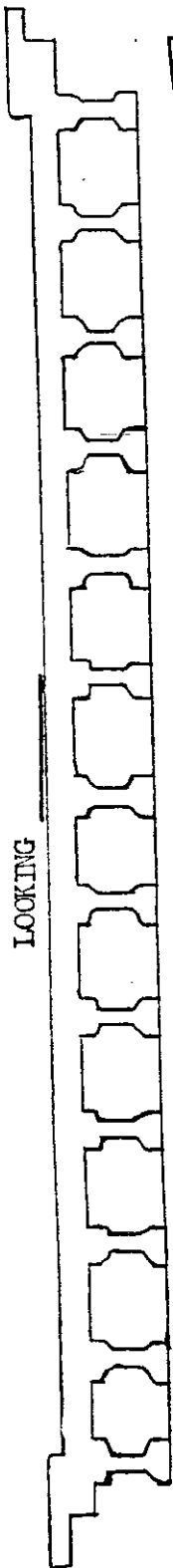
ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	WANGLES
CONC. I. BEAMS	G F P C	
A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
J	G F P C	
K	G F P C	
L	G F P C	
M	G F P C	
DIA.	G F P C	
BACKWALLS	G F P C	11/4

BR. NO. 79 140 5.09 SK

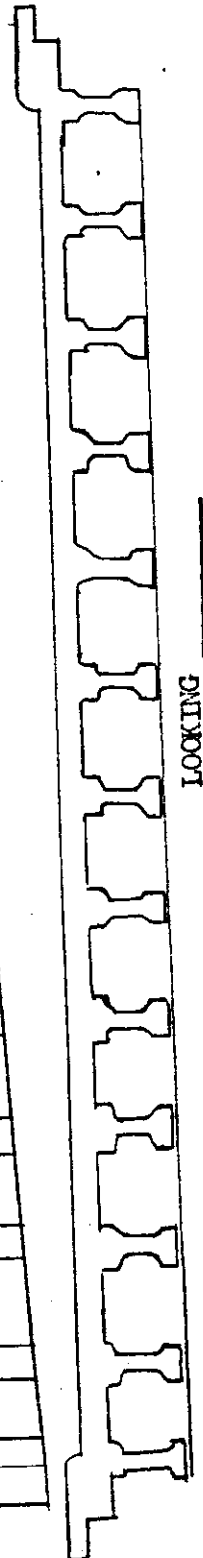
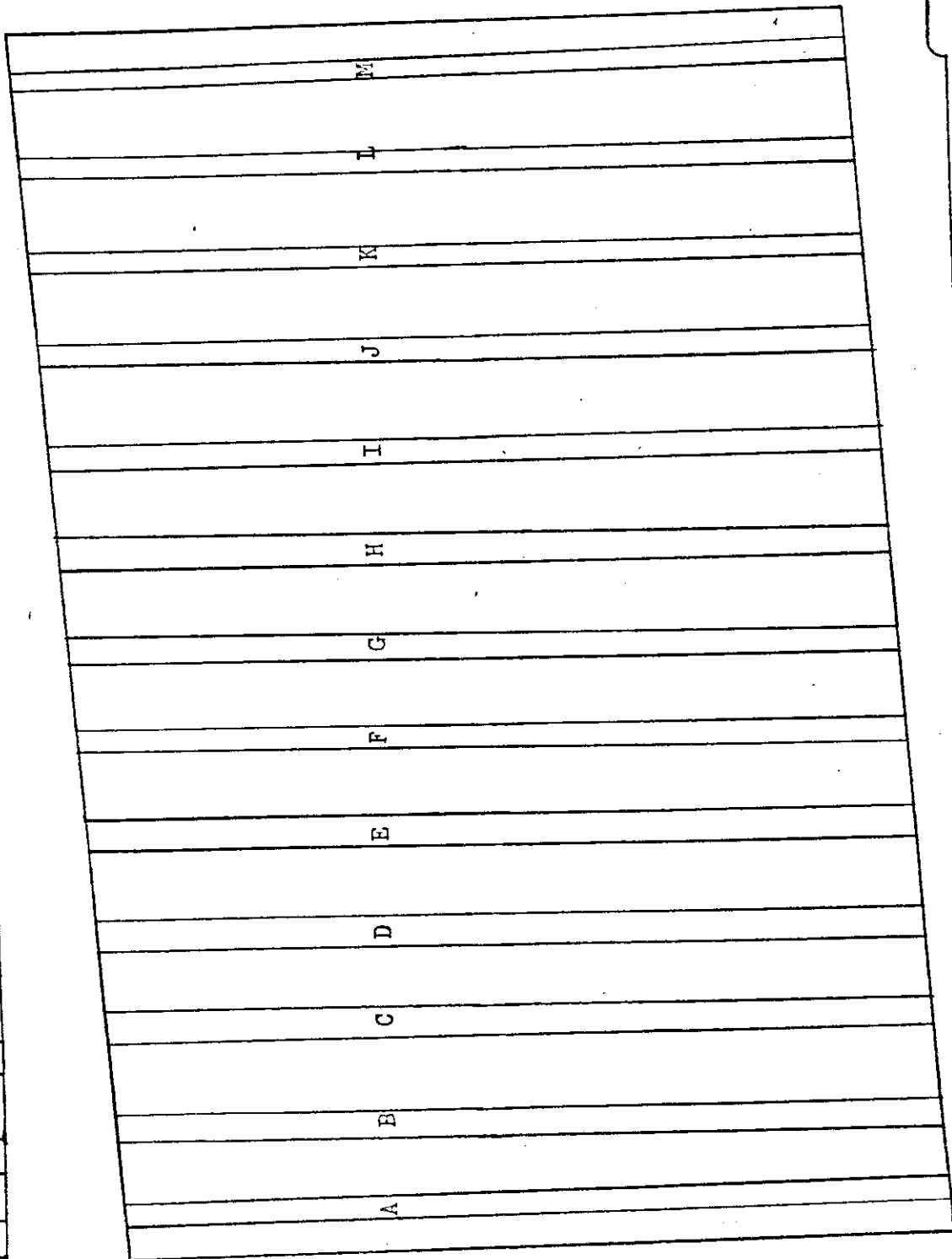
SPAN NO. 5

RT.

← DIR. OF ROUTE



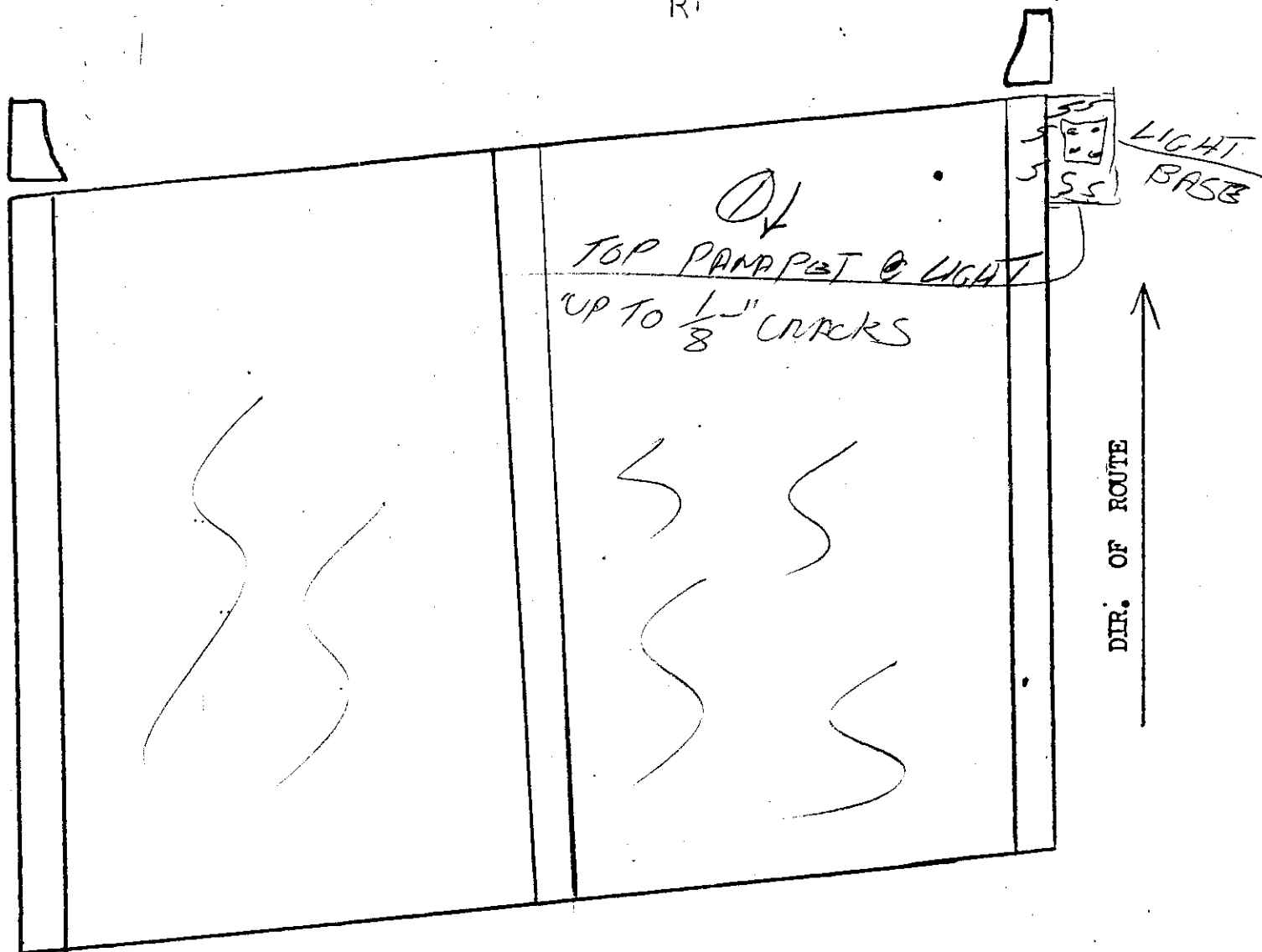
LOOKING



LOOKING

BRIDGE NO. 79 T.M. 5.09 SK. 60° RT. SPAN NO. 6
RT

AUG 03 2009



DECK	G	F	P	C
PARAPET	G	F	P	C
RAILS	G	F	P	C
INT	G	F	P	C
WARD	G	F	P	C
AIL				

$\frac{1}{4}$ " CRACKS

FINE CRACKS - SEE (D)

RT

AUG 6 1969

BRIDGE NO. 79 T-40 5.09BENT NO. _____ SPAN NO. 6 ABT. NO. _____ PIER NO. _____

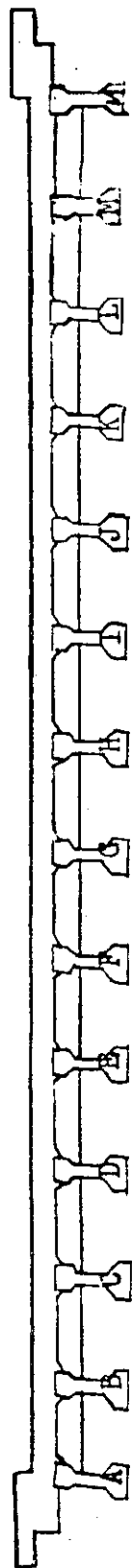
ELEMENT	RATING	COMMENTS
BOTTOM DECK	<u>G</u> F P C	PANELS
CONC. I. BEAM		
A	<u>G</u> F P C	
B	<u>G</u> F P C	
C	<u>G</u> F P C	
D	<u>G</u> F P C	
E	<u>G</u> F P C	
F	<u>G</u> F P C	
G	<u>G</u> F P C	
H	<u>G</u> F P C	
I	<u>G</u> F P C	
J	<u>G</u> F P C	
K	<u>G</u> F P C	
L	<u>G</u> F P C	
M	<u>G</u> F P C	
N	<u>G</u> F P C	
BACKWALLS	G F P C	N/A
DTA.	<u>G</u> F P C	

BRIDGE NO. 79 I-40 5.09

RT
SPAN NO. 6

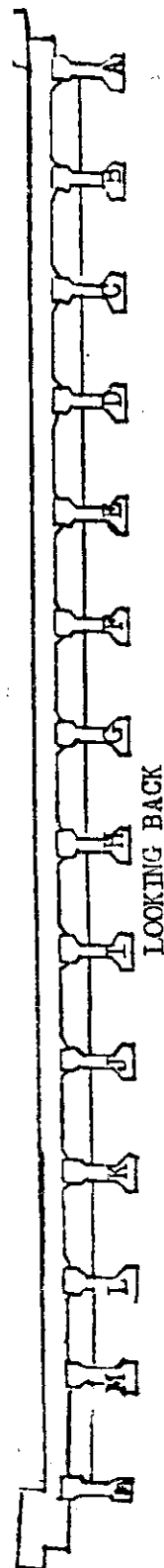
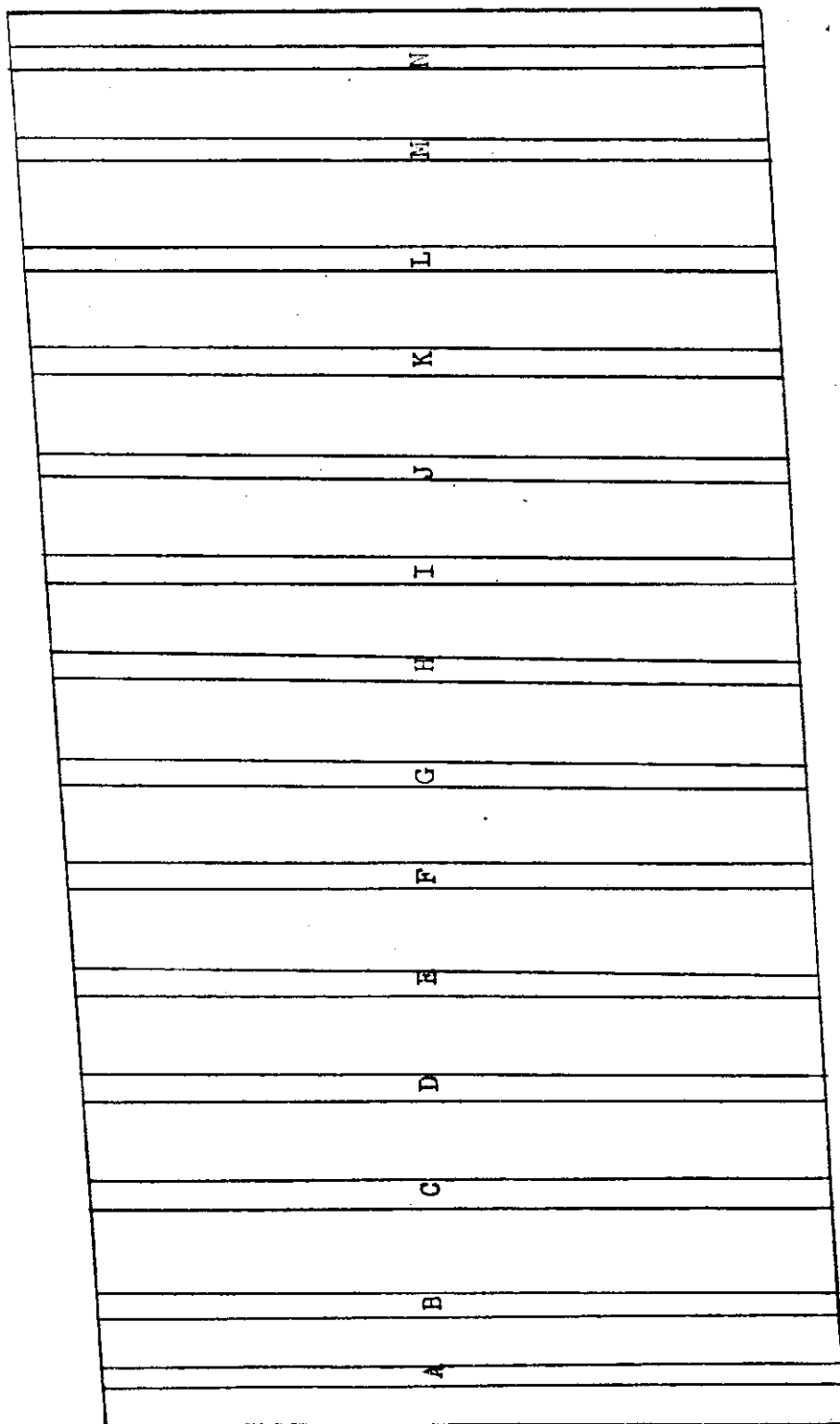
AUG 03 2001

SKEW 60° RT.



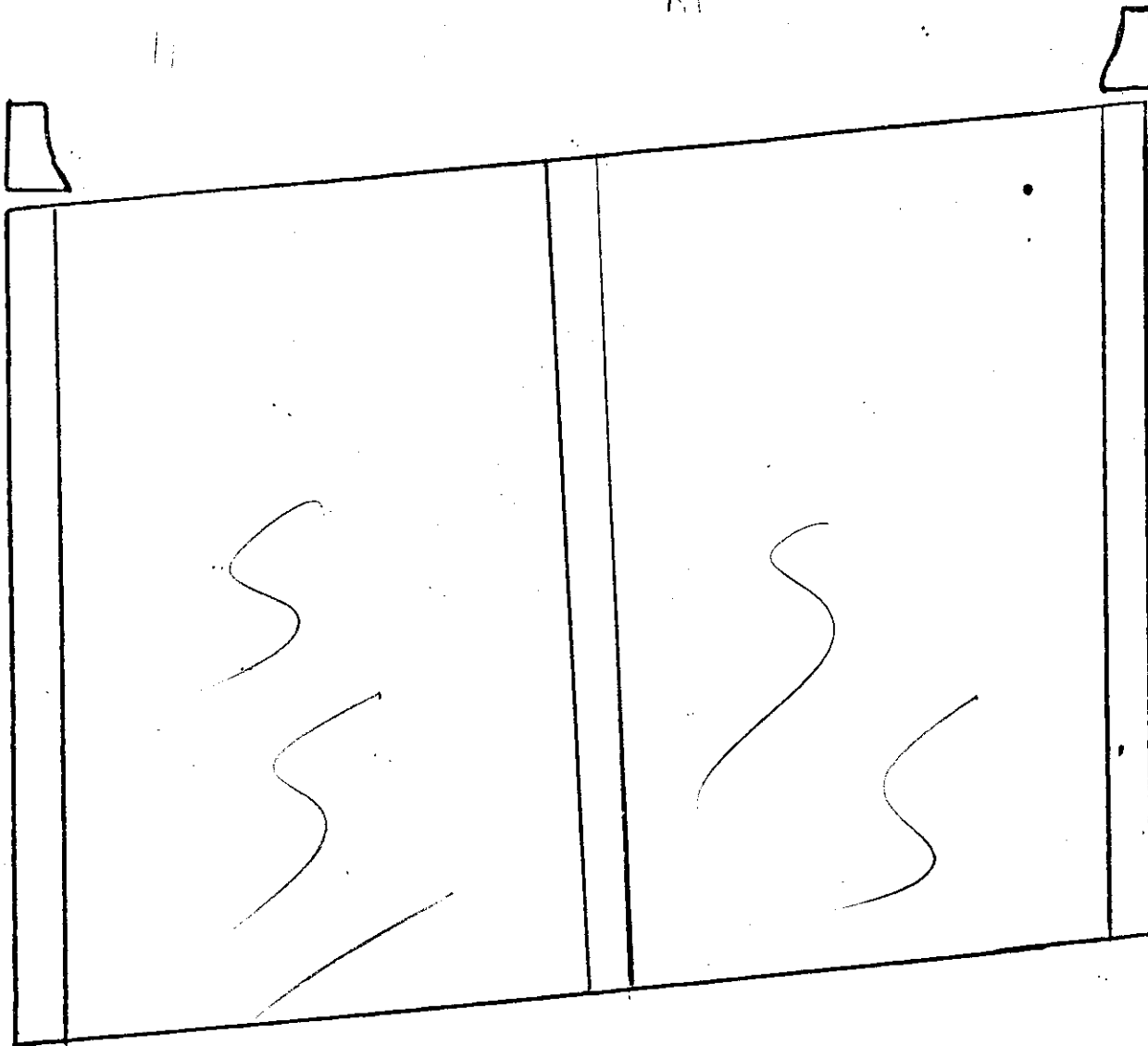
LOOKING AHEAD

← DIR. OF ROUTE



LOOKING BACK

BRIDGE NO. 79 I-L 509 SK. 60° RT. SPAN NO. 7
RT



DECK	G	F	P	C
RAPET	G	F	P	C
AINS	G	F	P	C
INT	G	F	P	C
JARD	G	F	P	C
WIL				

1/4" CRACKS

FINE CRACKS

BRIDGE NO. 79 T-40 5.09

BENT NO. _____ SPAN NO. 7 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	<u>G</u> F P C	<i>REPAIR</i>
CONC. I. BEAMS		
A B C	<u>G</u> F P C	
_____	G F P C	
D E F	<u>G</u> F P C	
_____	G F P C	
G H I	<u>G</u> F P C	
_____	G F P C	
J K L	<u>G</u> F P C	
_____	G F P C	
M N O	<u>G</u> F P C	
_____	G F P C	
P	<u>G</u> F P C	
BACKWALLS	G F P C	<i>N/A</i>
DIA.	<u>G</u> F P C	

AUG 03 2001

BR. NO. 79 T-40 5.09 SPAN NO. 7

RT

SK. 60 RT.

	A	
	B	
	C	
	D	
	E	
	F	
	G	
	H	
	I	
	J	
	K	
	L	
	M	
	N	
	O	
	P	

LOOKING

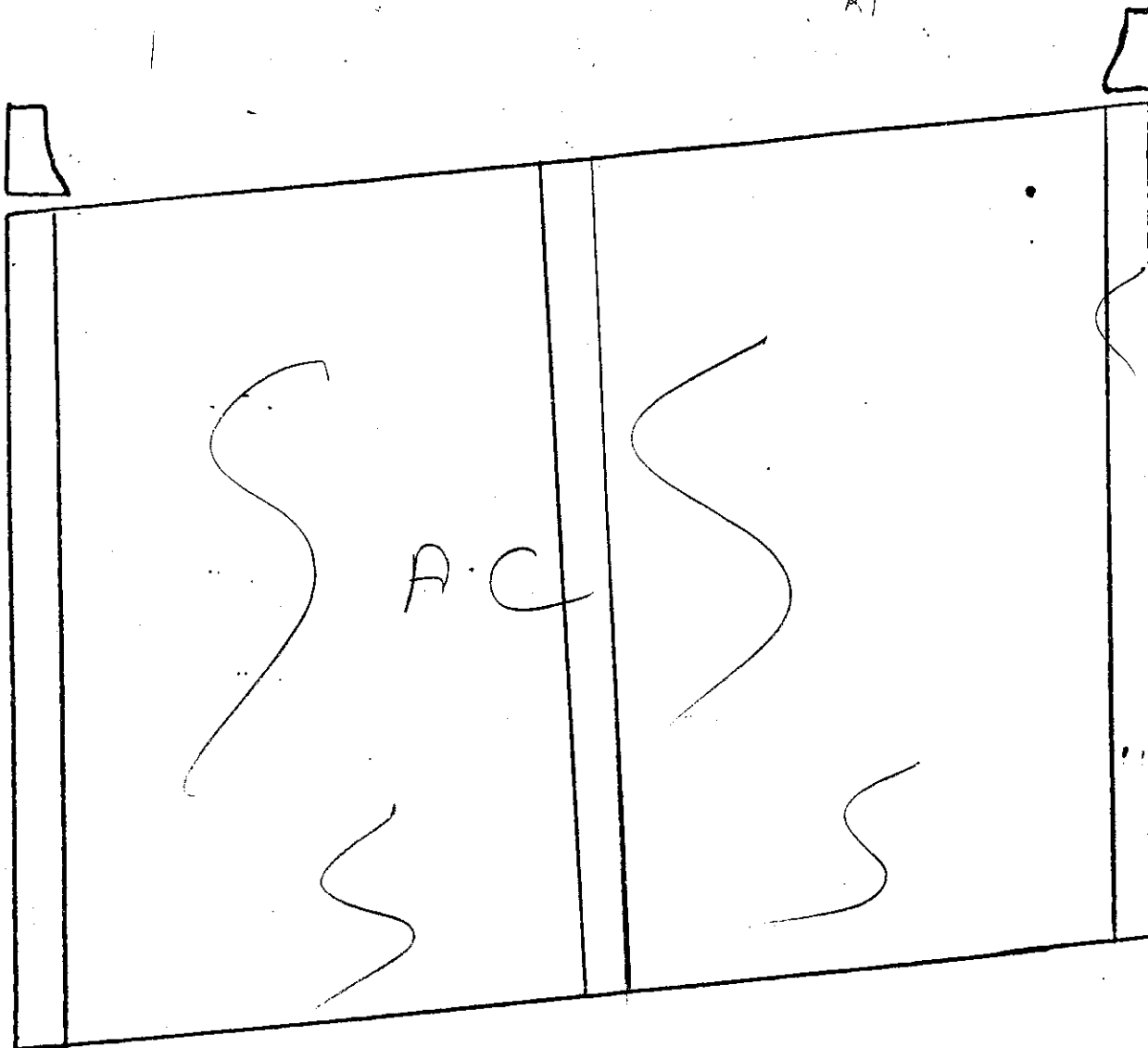
LOOKING

DIR. OF ROUTE

AUG 5 2004

BRIDGE NO. 79 ILE 5.09 SK. 60 RT. SPAN NO. 8

RT



DIR. OF ROUTE

DECK	G	(F)	P	C
RAPEL	G	(F)	P	C
AINS	(G)	F	P	C
ENT	G	F	P	C
ARD	G	F	P	C
IL				

1/4 CRACKS
FINE "

AUG 03 2006

BRIDGE NO. 79 T-40 509

BENT NO. _____ SPAN NO. 7 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	<u>G</u> F P C	<i>PANELS</i>
CONC. I. BEAMS		
A B C	<u>G</u> F P C	
	G F P C	
D E F	<u>G</u> F P C	
	G F P C	
G H I	<u>G</u> F P C	
	G F P C	
J K L	<u>G</u> F P C	
	G F P C	
M N O	<u>G</u> F P C	
	G F P C	
P - Q	<u>G</u> F P C	
BACKWALLS	G F P C	<i>W/L</i>
DIA.	<u>G</u> F P C	

BR. NO. 79 I-40 5.09 SK. 40° Rt

SPAN NO. 8

RT

AUG 10 1964

LOOKING

LOOKING

A

B

C

D

E

F

G

H

I

J

K

L

M

N

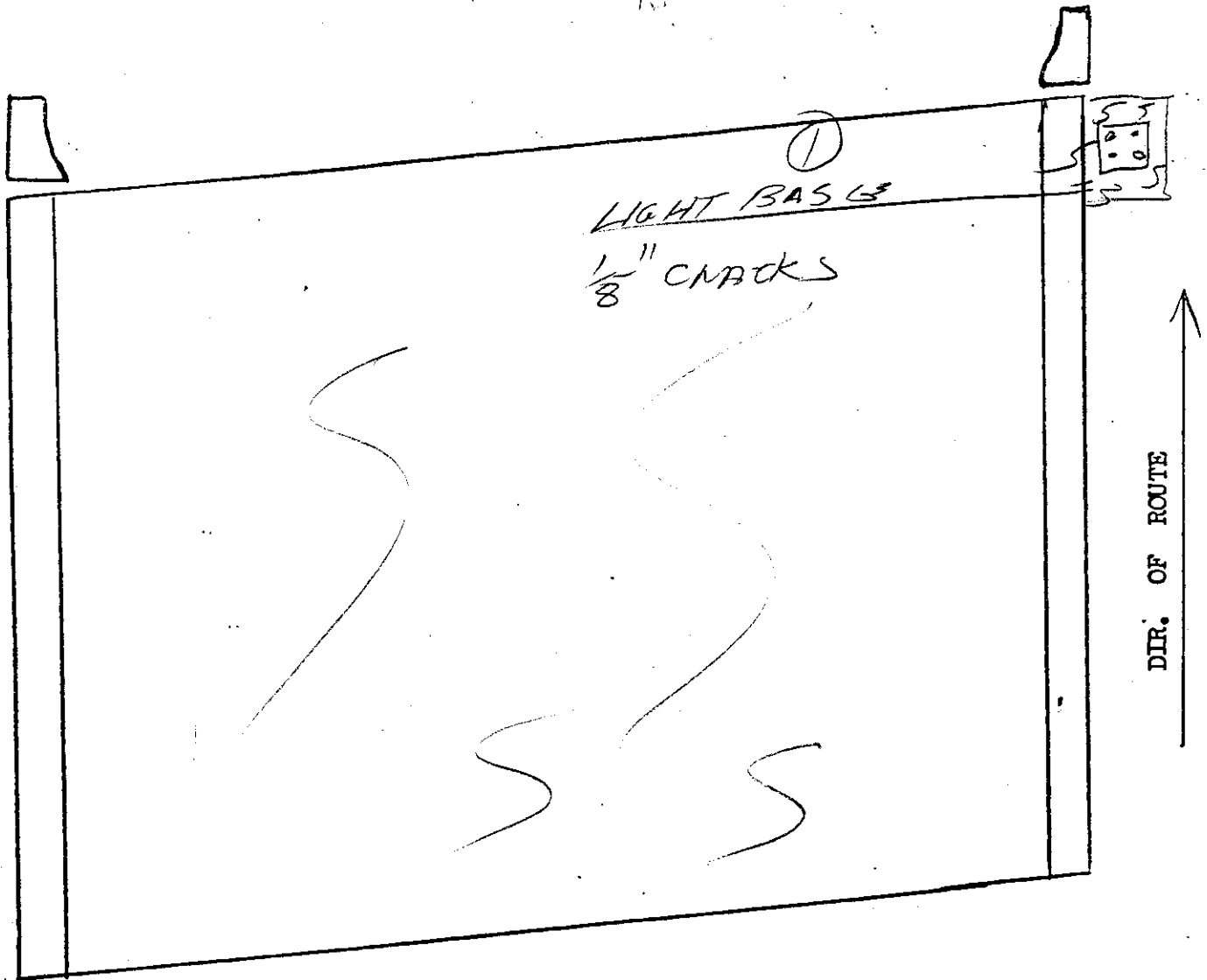
O

P

Q

AUG 03 2001

BRIDGE NO. 79 I-40 5.59 SK. 60' RT. SPAN NO. 9-A
RT



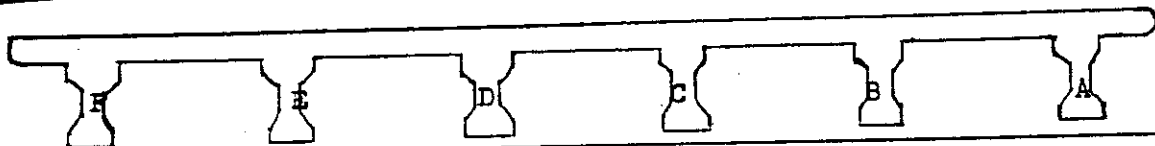
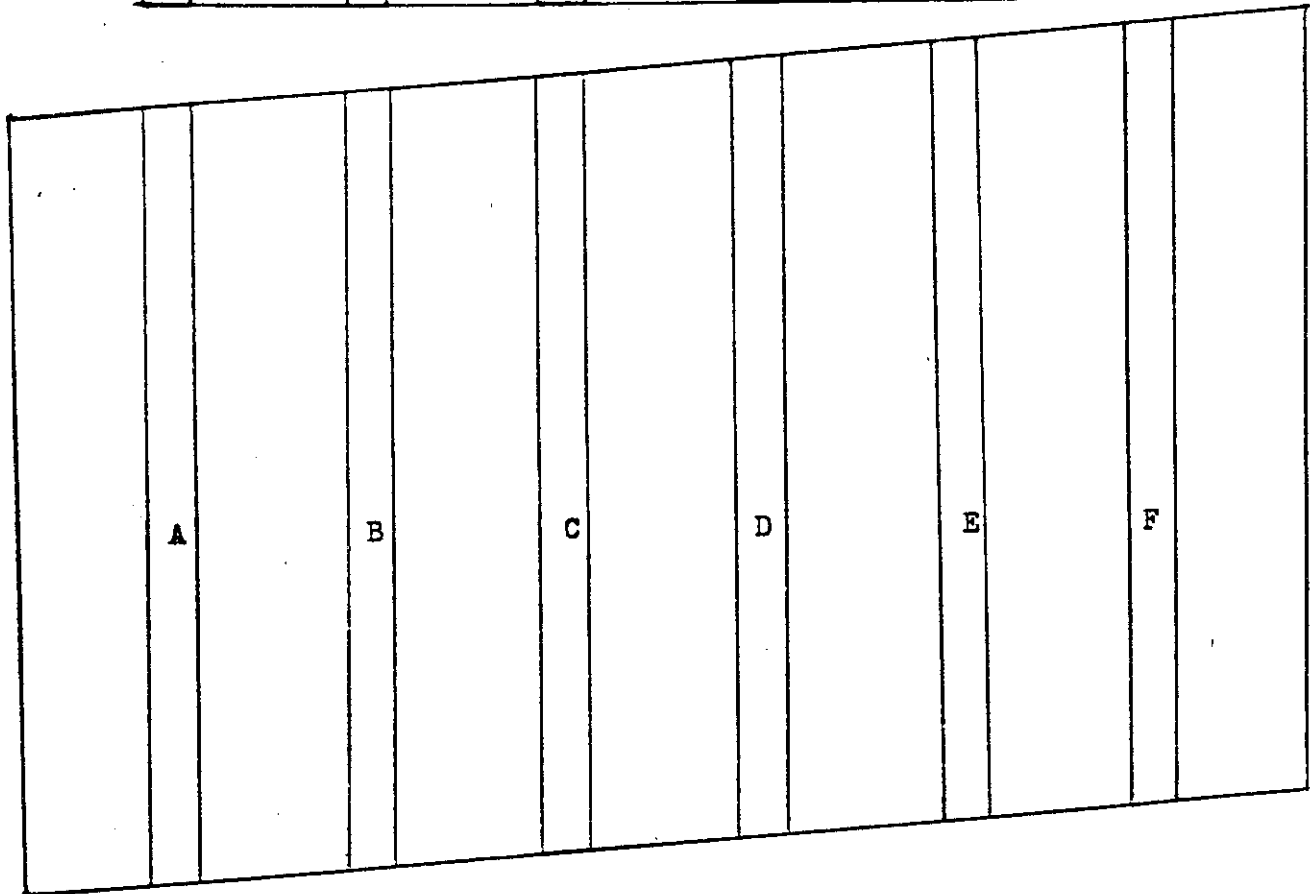
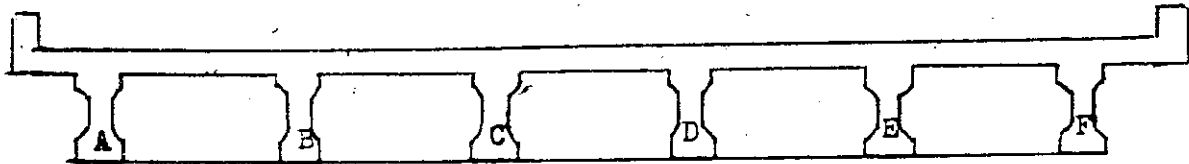
DECK	G	(F)	P	C
ARAPET	G	(F)	P	C
RAINS	(G)	F	P	C
JOINT	G	F	P	C

1/4\"

FINE CRACKS SGE (1)

RT

AUG 08 2001

BR. NO. 72Two 509'SK. 60 RT.SPAN NO. 9-A

ELEMENT

RATING

COMMENT

BOTTOM DECK

G F P C

PANELS

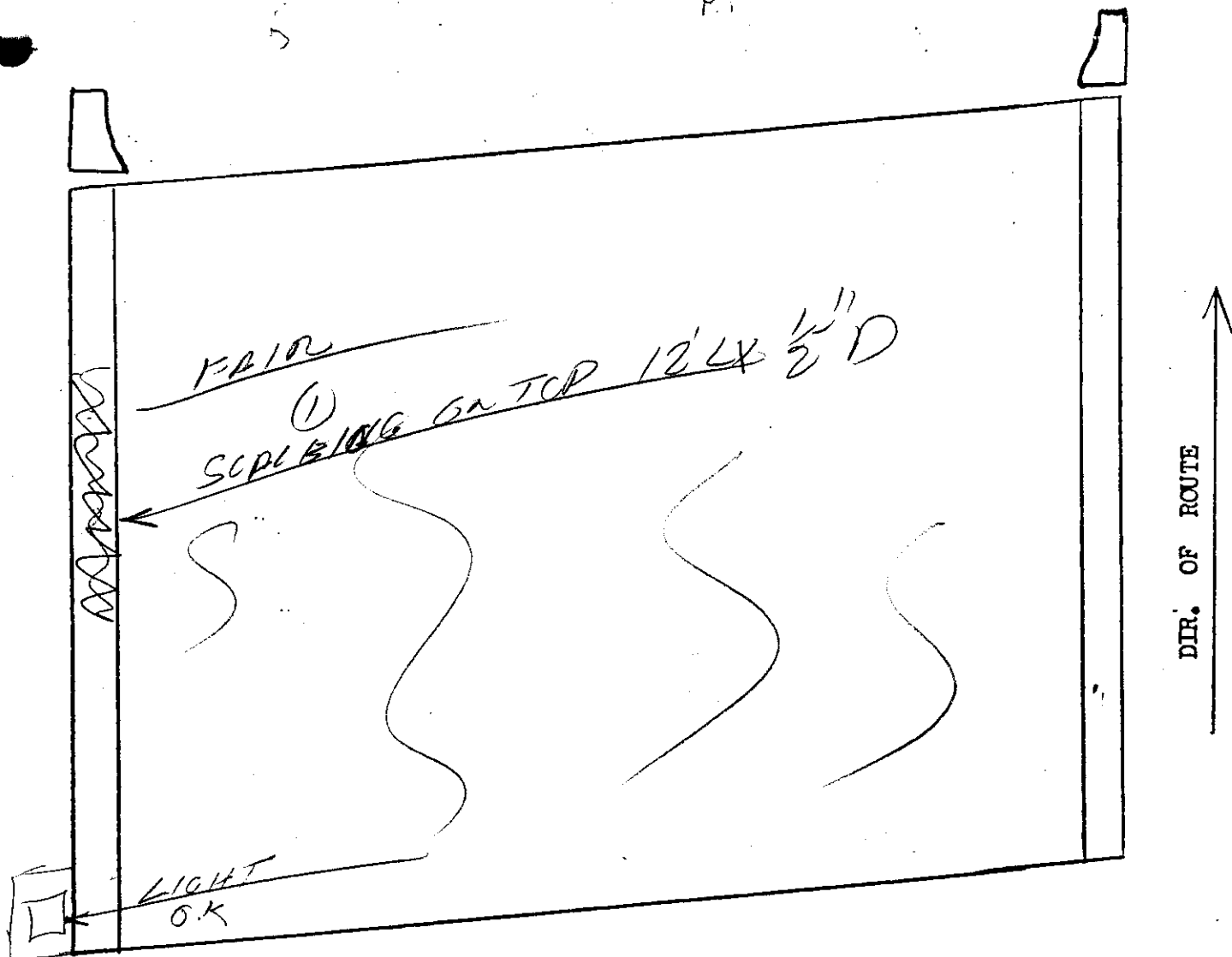
CONC. I. BEAMS

A G F P C
 B G F P C
 C G F P C
 D G F P C
 E G F P C
 F G F P C

DIA.

G F P C

BRIDGE NO. 79 T.H.C. 509 SK. 60° RT. SPAN NO. 9-B



DECK	G	(F)	P	C
ARAPET	G	(F)	P	C
LIGHT RAINS	(G)	F	P	C
JINT	G	F	P	C
Drain	(G)			

$\frac{1}{4}$ " CRACKS

FINE CRACKS SUB (1)

R_i BRIDGE NO. 79 140 509

BENT NO. _____ SPAN NO. 9-B ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	PS 14110
CONC. I. BEAM A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
DIA.	G F P C	
BACKWALLS.	G F P C	NA

Rt. Lamp

AUG 19 1943

BR. NO.

79 Two 509

SK.

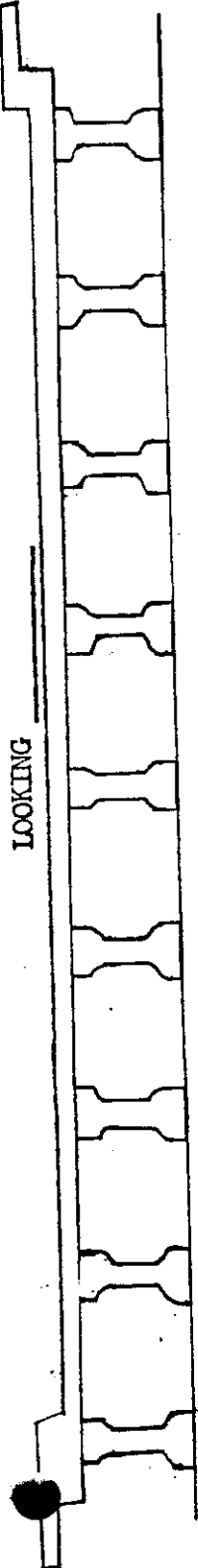
60° 24

SPAN NO.

9-B

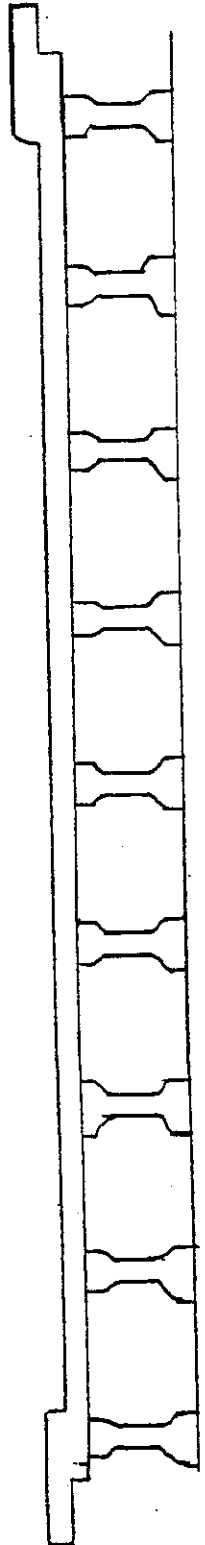
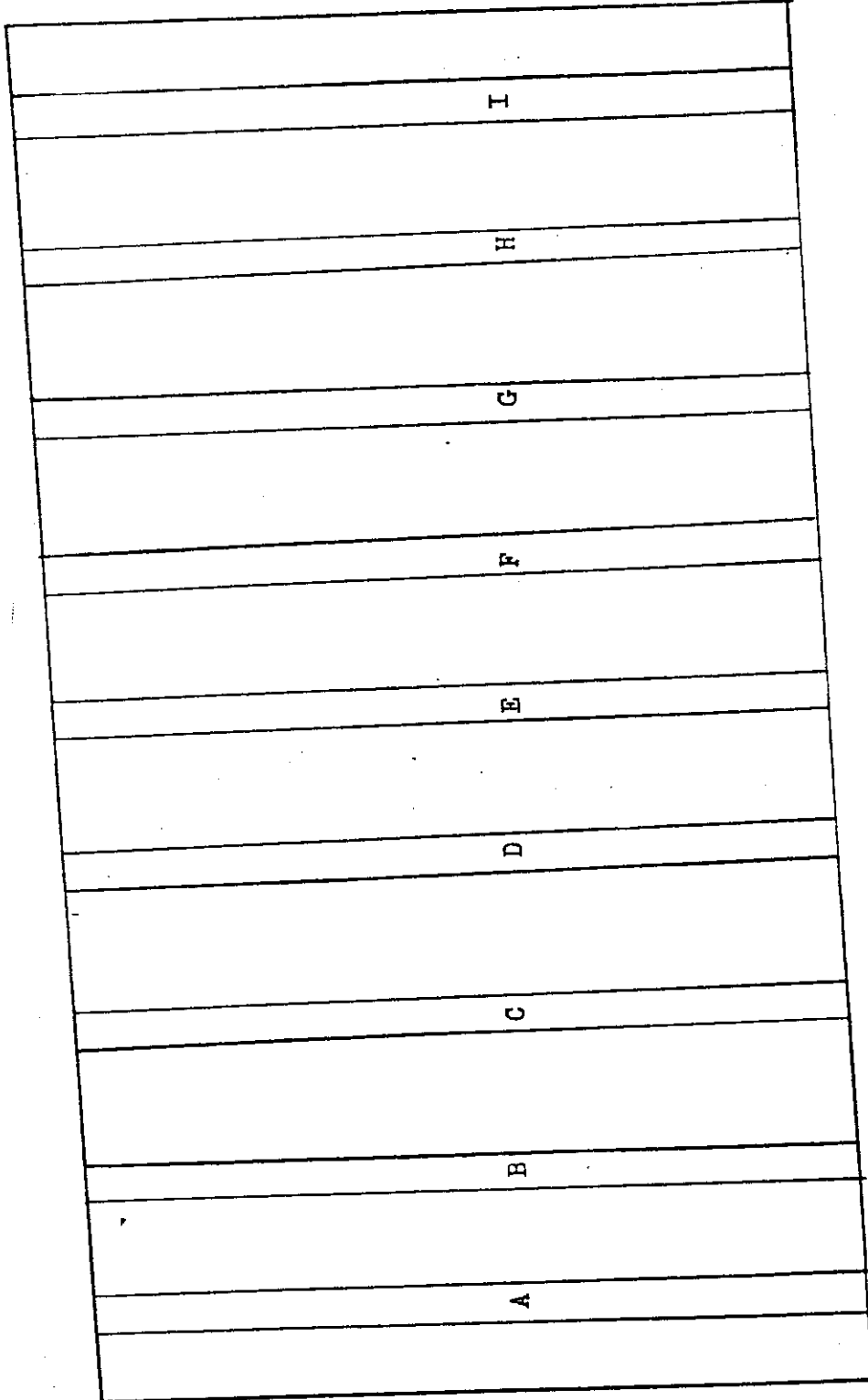
RT

DIR. OF ROUTE



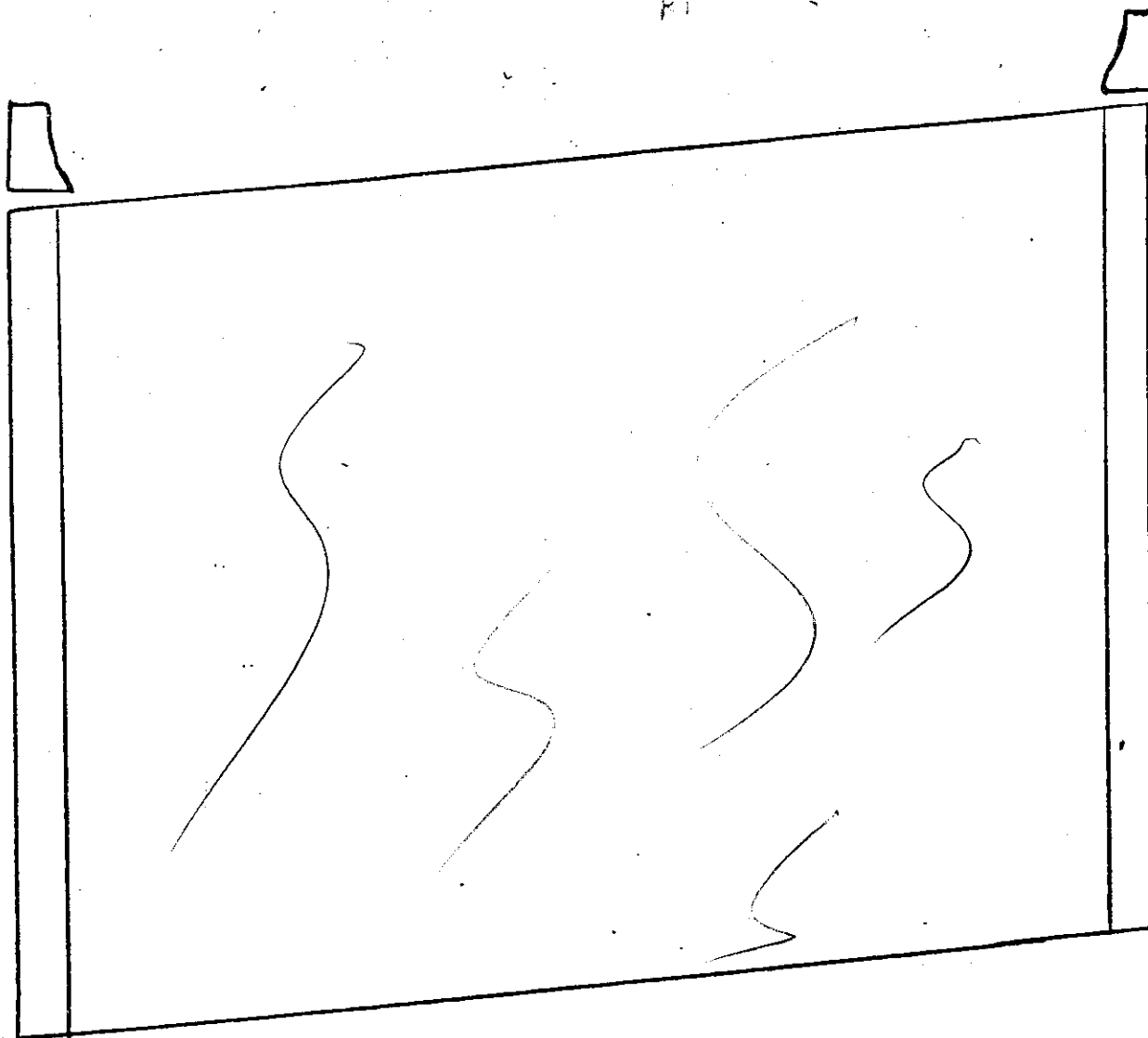
LOOKING

LOOKING



LOOKING

BRIDGE NO. 79 T-10 509 SK. 60° RT. SPAN NO. 10-A
 FT

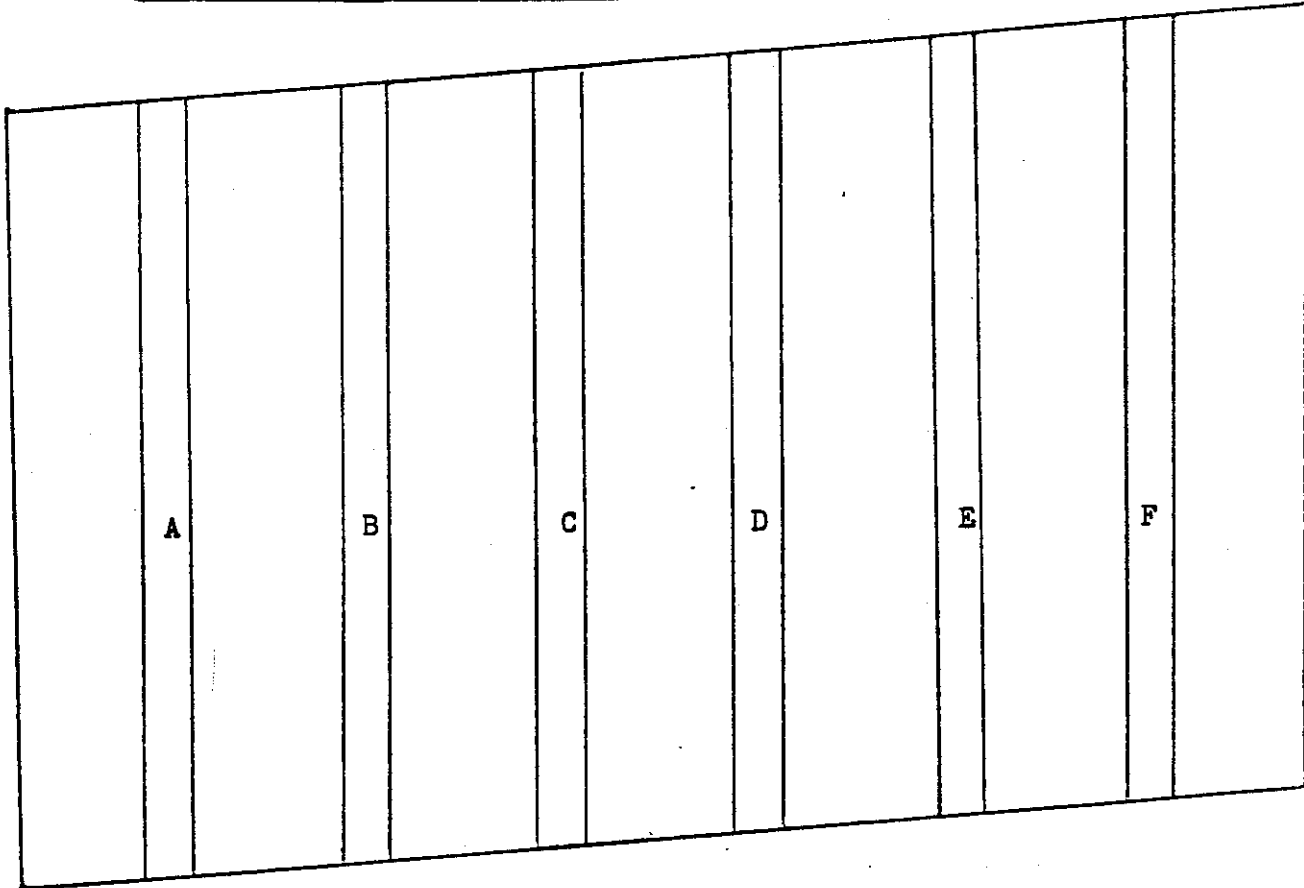
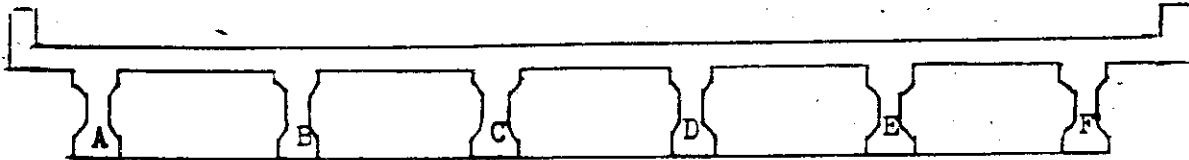


DIR. OF ROUTE

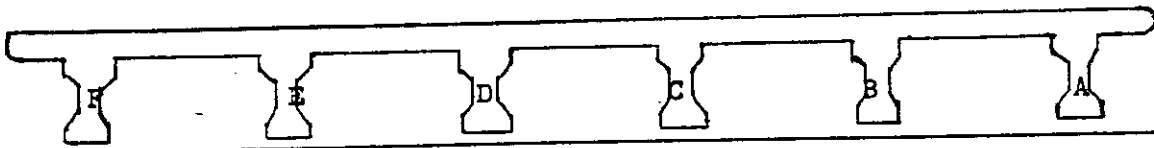
DECK	G	F	P	C	1/4" CRACKS
PARAPET	G	F	P	C	FINE CRACKS
RAILS	G	F	P	C	
JOINT	G	F	P	C	

AUG 03 2001

BR. NO. 79 ^{RT} 1.40 ^{Lt} 5.07 SK. 60 RT. SPAN NO. 10-A

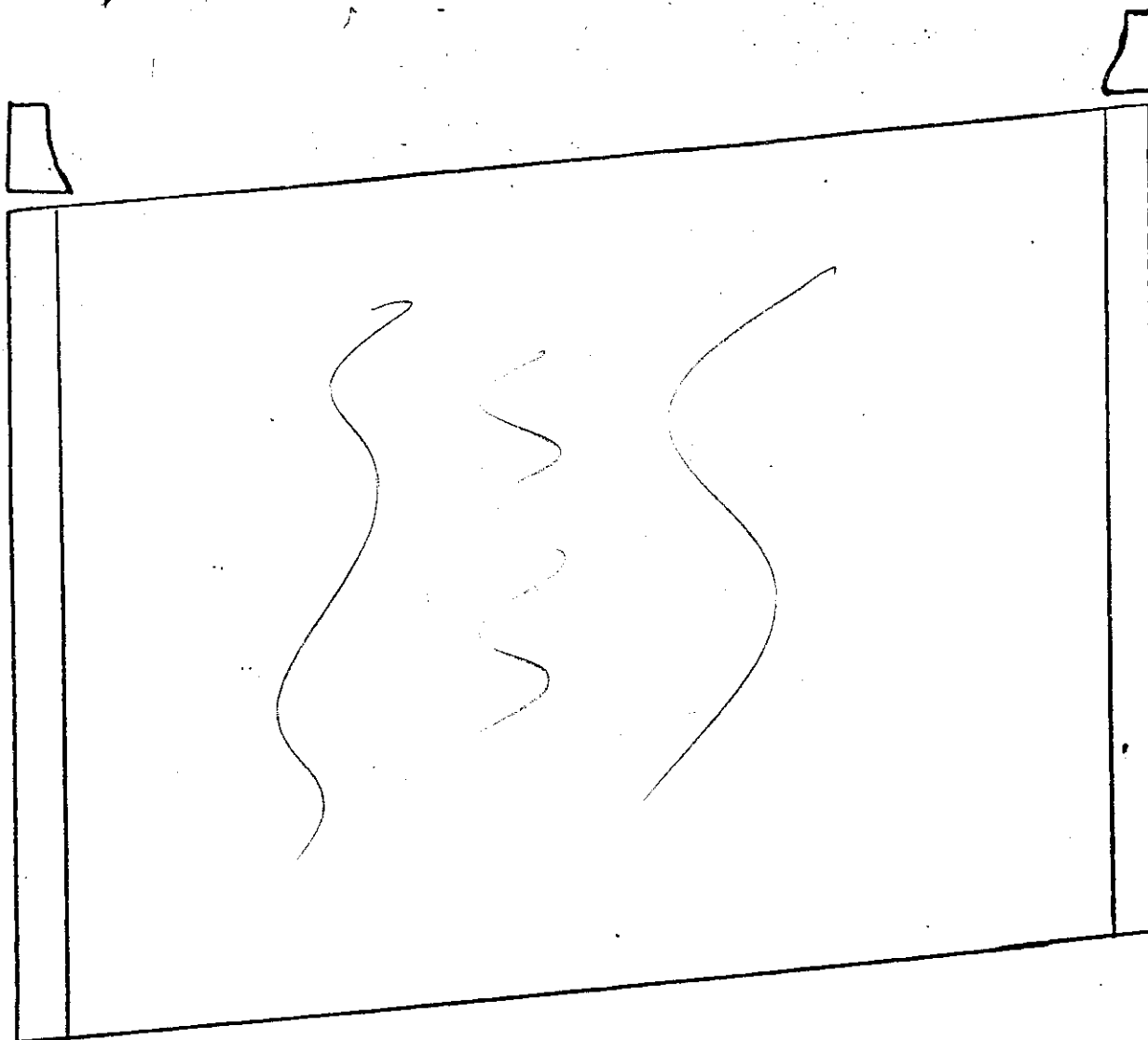


↑
DIR. OF ROUTE



ELEMENT	RATING	COMMENT
BOTTOM DECK	G F P C	PANELS
CONC. I. BEAMS	A G F P C B G F P C C G F P C D G F P C E G F P C F G F P C	
DIA.	G F P C	

BRIDGE NO. 79 T-210 5.09 FT SK. 60' RT. SPAN NO. 10-B



↑
DIR. OF ROUTE

DECK	G	(F)	P	C
PARAPET	G	(F)	P	C
RAILS	(G)	F	P	C
JOINT	G	F	P	C

1/4" CRACKS

FINE CRACKS

BRIDGE NO. 79 I-170 5109

BENT NO. _____ SPAN NO. 10-B ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	PANELS
CONC. I. BEAM A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
DIA.	G F P C	
BACKWALLS.	G F P C	N/A

Rt. Ramp

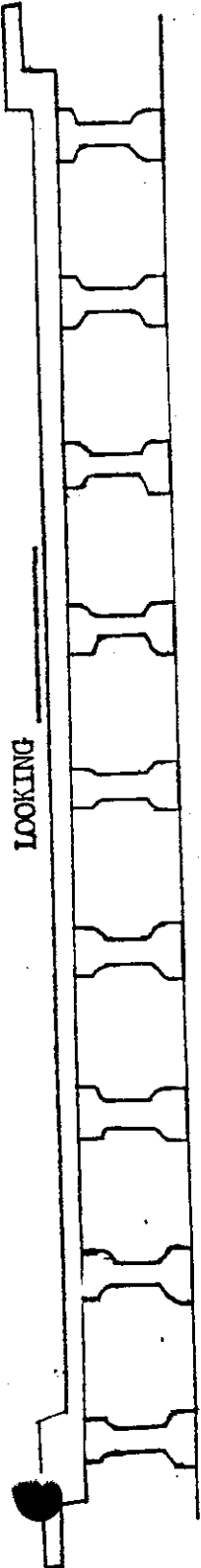
AUG 08 2001

BR. NO. 79 120 5.09 SK.

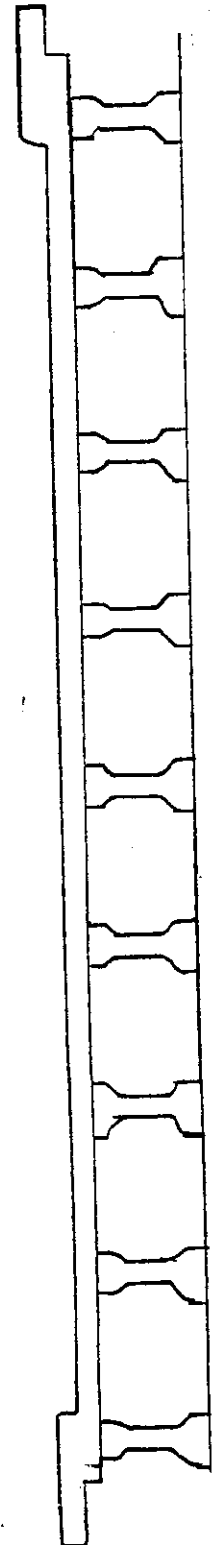
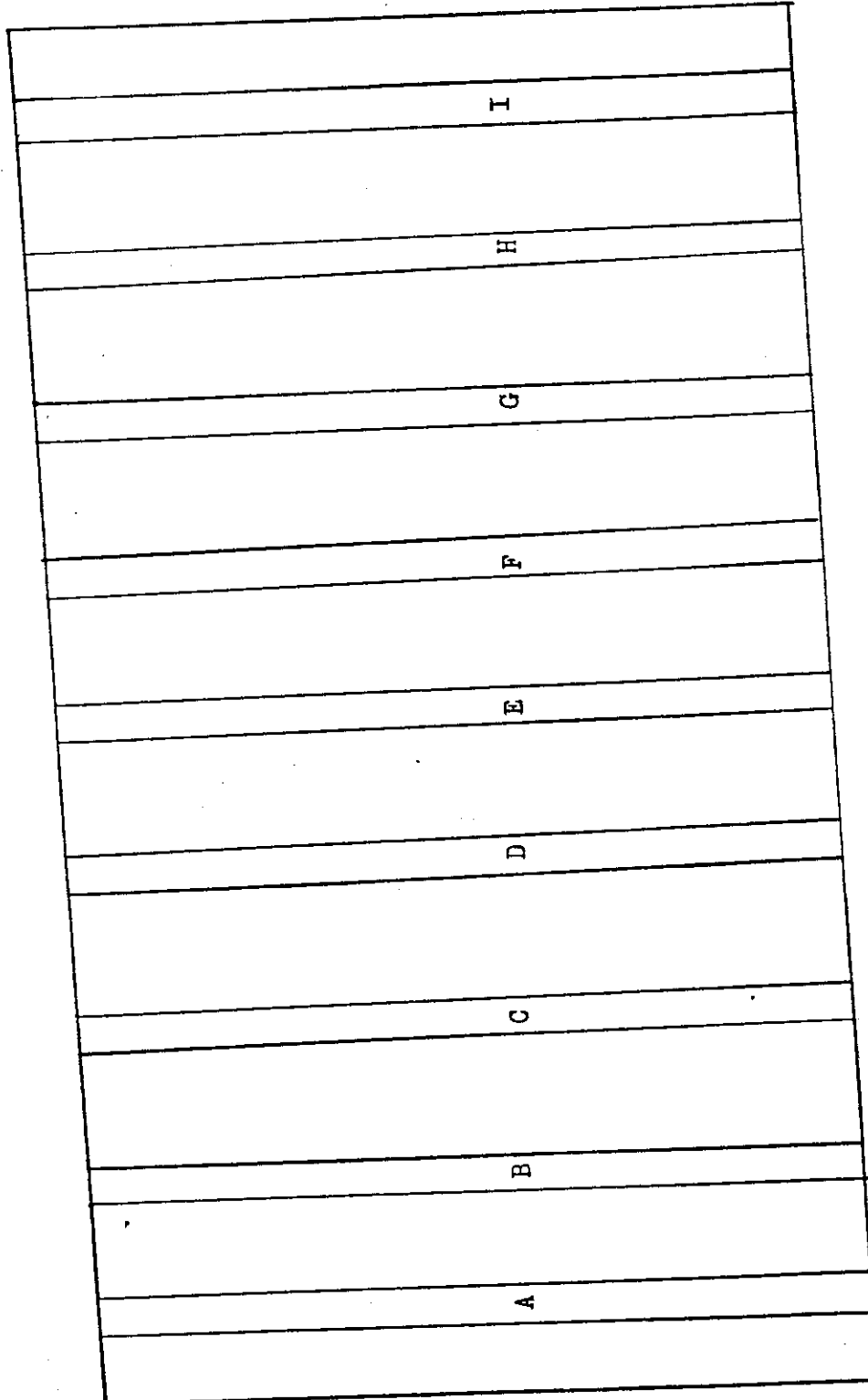
SPAN NO. 10 B

RT

← DIR. OF ROUTE

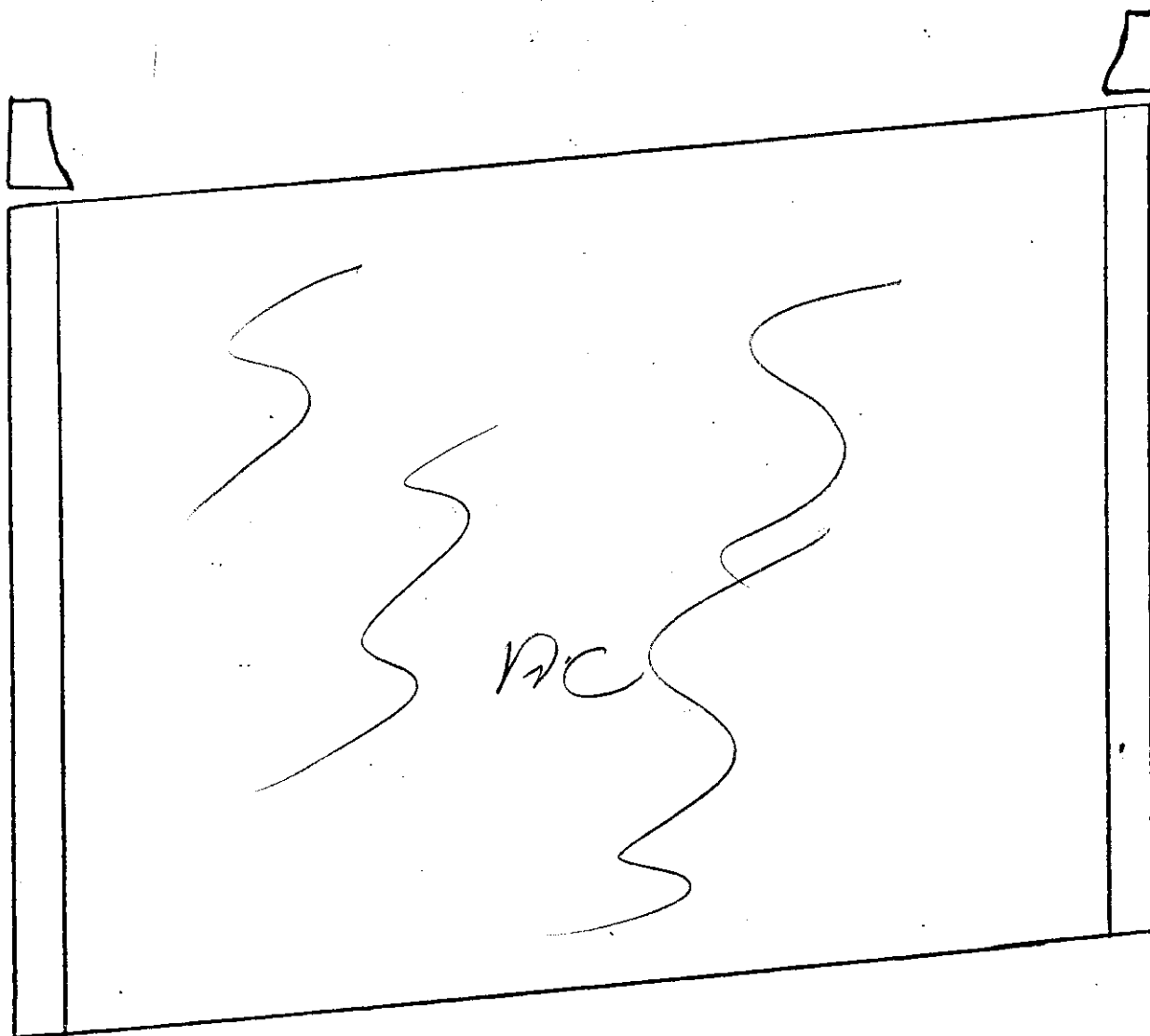


LOOKING

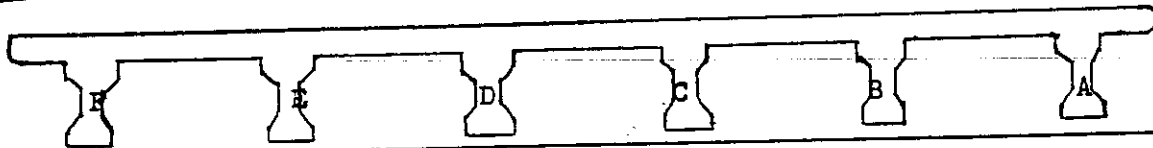
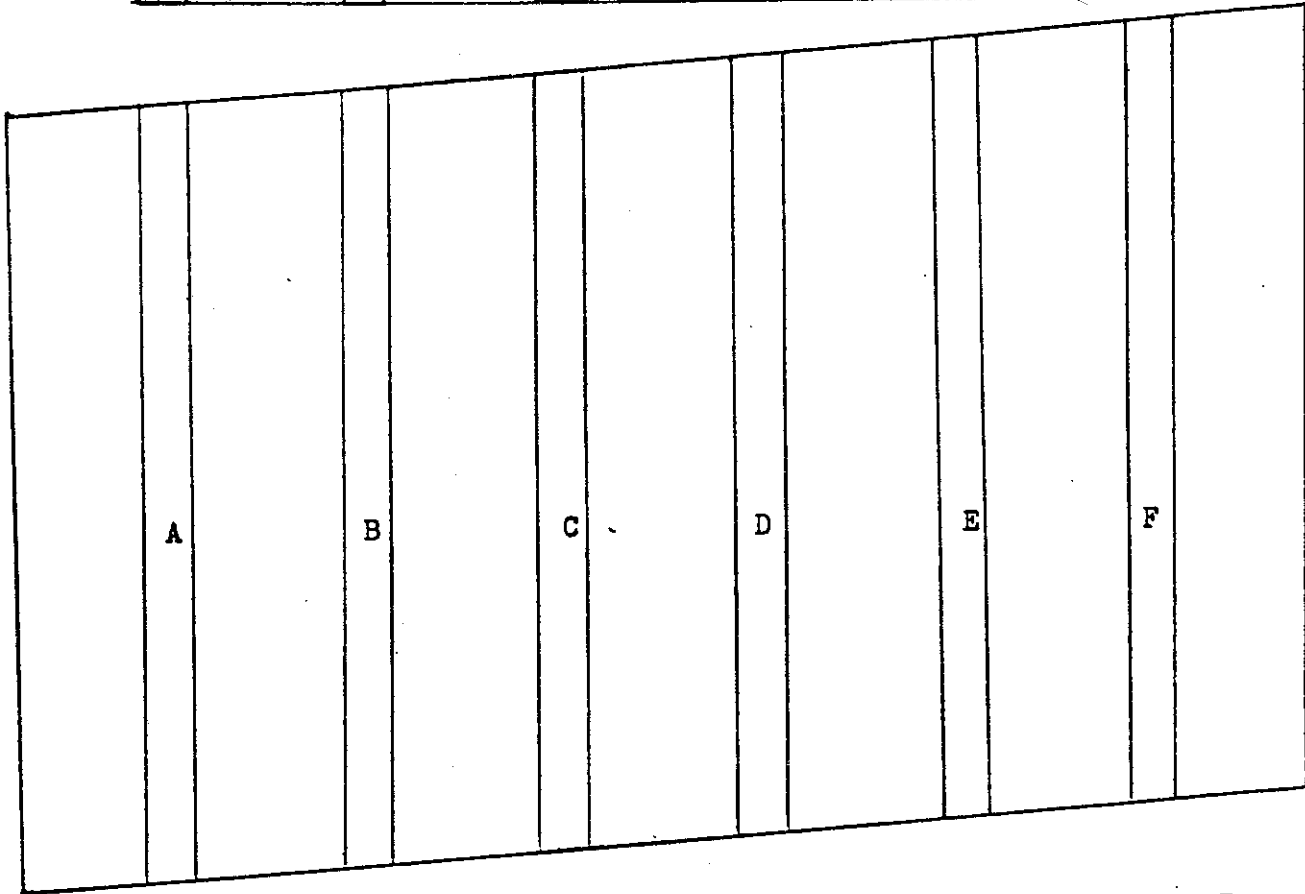
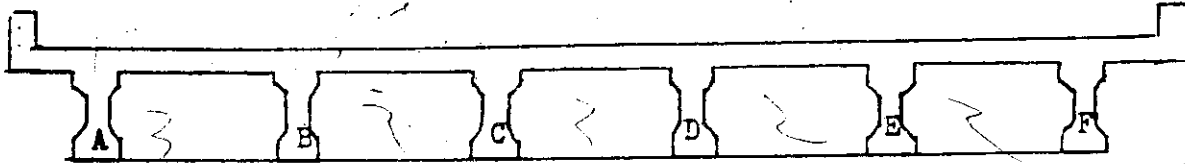


LOOKING

BRIDGE NO. 79 I-40 5.09 RT. SK. 60 RT. SPAN NO. 11-A



DECK	G (F) P C	1/4" CRACKS
PARAPET	G (F) P C	FINE CRACKS
RAILS	G F P C	N/D
JOINT	G F P C	

BR. NO. 79T. No. 509SK. 60 RT.SPAN NO. 11-A

ELEMENT

RATING

COMMENT

BOTTOM DECK

G F P C

CONC. I. BEAMS

A G F P C
 B G F P C
 C G F P C
 D G F P C
 E G F P C
 F G F P C

DIA.

G F P C

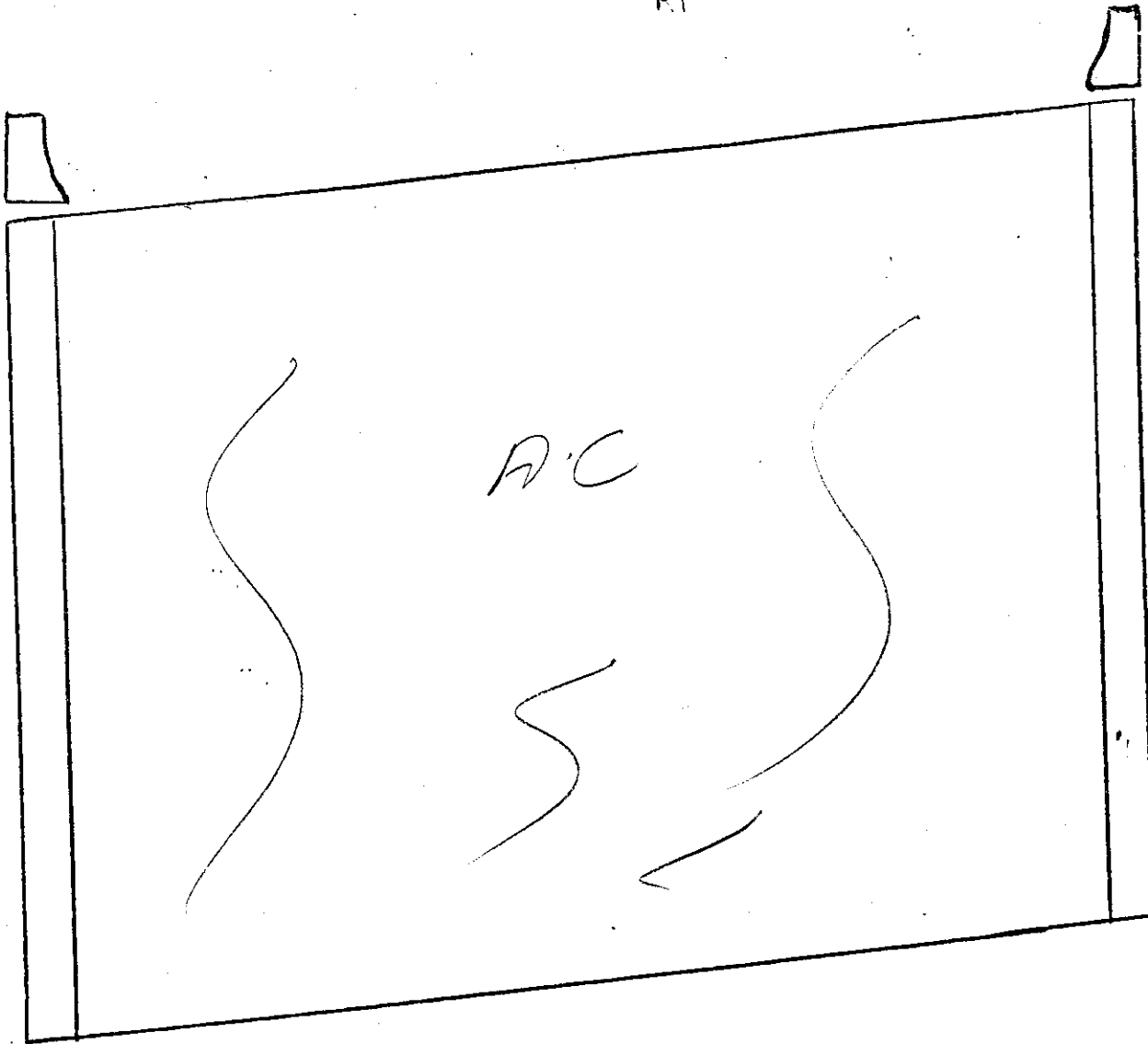
BACKWALL

F

FINE CLACKS

AUG 03 2001

BRIDGE NO. 79 T-10 5.09 SK. 60 RT. SPAN NO. 11-B
FT



DECK	G (F) P C	1" CRACKS
PARAPET	G (F) P C	4 PINE CRACKS
DRAINS	G F P C	N/A
JOINT	G F P C	

BRIDGE NO. 79 I-40 5.09 RT

BENT NO. _____ SPAN NO. 11-1 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	<u>G</u> F P C	PANELS
CONC. I. BEAM A	<u>G</u> F P C	
B	<u>G</u> F P C	
C	<u>G</u> F P C	
D	<u>G</u> F P C	
E	<u>G</u> F P C	
F	<u>G</u> F P C	
G	<u>G</u> F P C	
H	<u>G</u> F P C	
I	<u>G</u> F P C	
DIA.	<u>G</u> F P C	
BACKWALLS.	<u>G</u> F P C	FIVE MARKS
		SMOKE STAINING ON
		PANELS BETWEEN CIE.
		G + H

BR. NO. 79 T-40 5.09
RT

SK. 60°RT SPAN NO. 11-B

← DIR. OF ROUTE

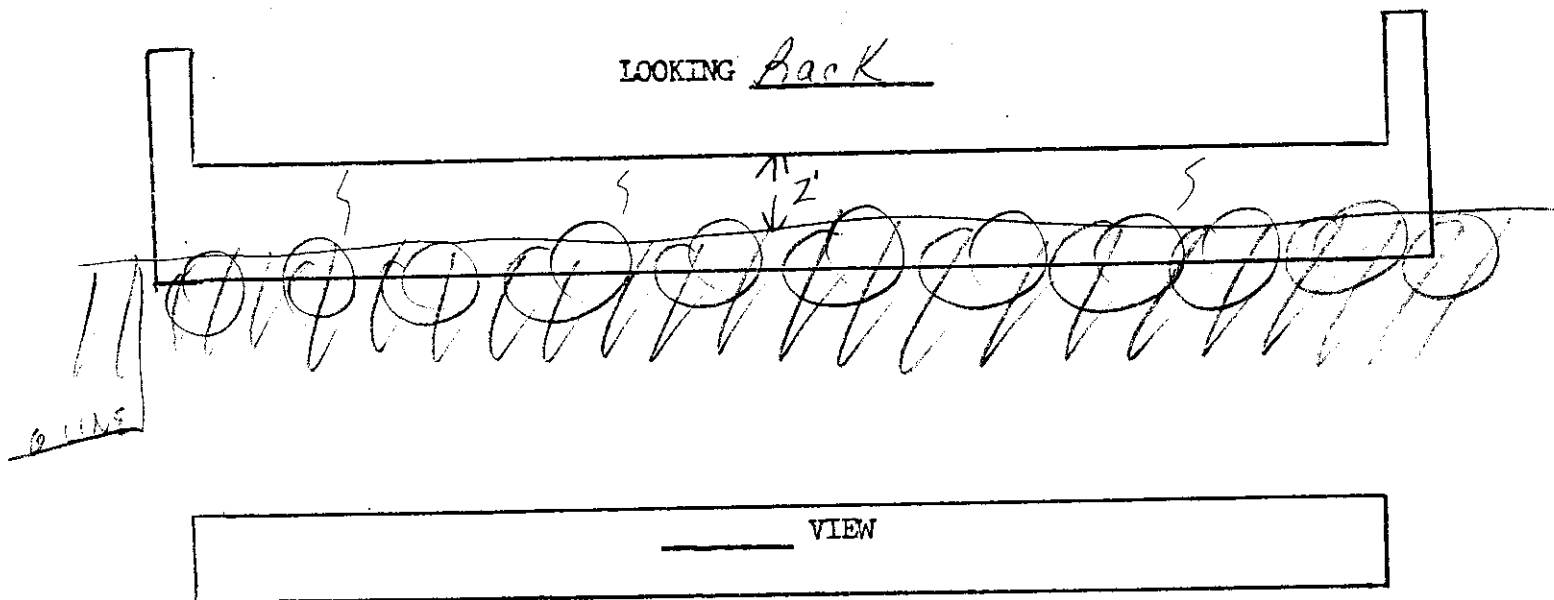
LOOKING

LOOKING AHEAD

LOOKING

BRIDGE NO 79 T-116 5.09

AUG 00 2001

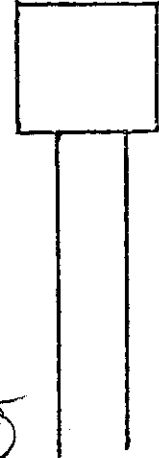
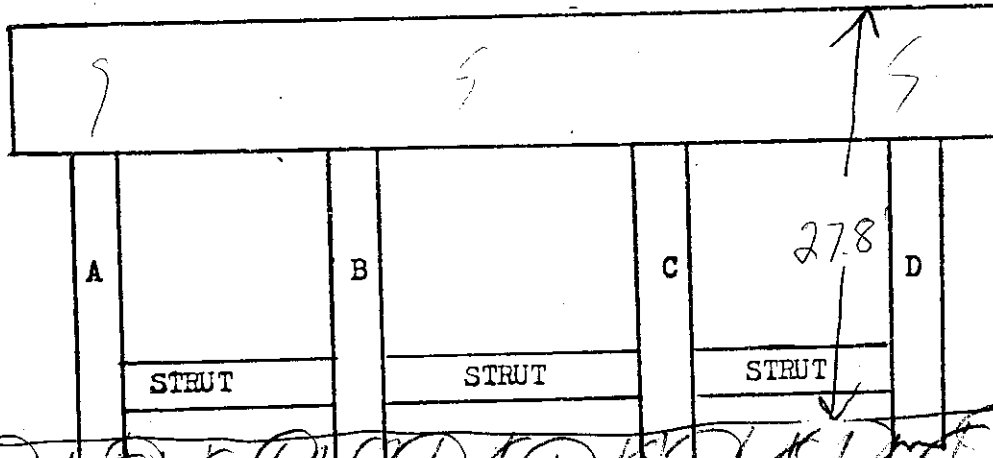
ABUT. NO. 1
RTDBLOOKING BackNOTE: ALL HEIGHTS
MEASURED ON
RT. SIDE OF
BRIDGE

ELEMENT	RATING	COMMENT
BEARING	G (F) P C	LIGHT. CORR.
CAP	G (F) P C	FINE CRACKS
WINGS	(G) F P C	
EMBANKMENT	(G) F P C	
SLOPAVEMENT	G F P C	N/A
VEG.	G F (P) C	HEAVY IN GEN.
RIPRAP	(G)	

BR. NO. 79 I-40 5.09SK. 60° RTAUG 02 2001
BENT NO. 1

FRONT VIEW

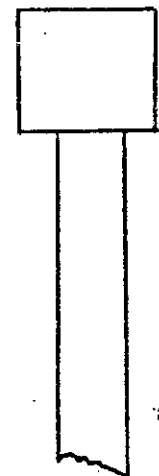
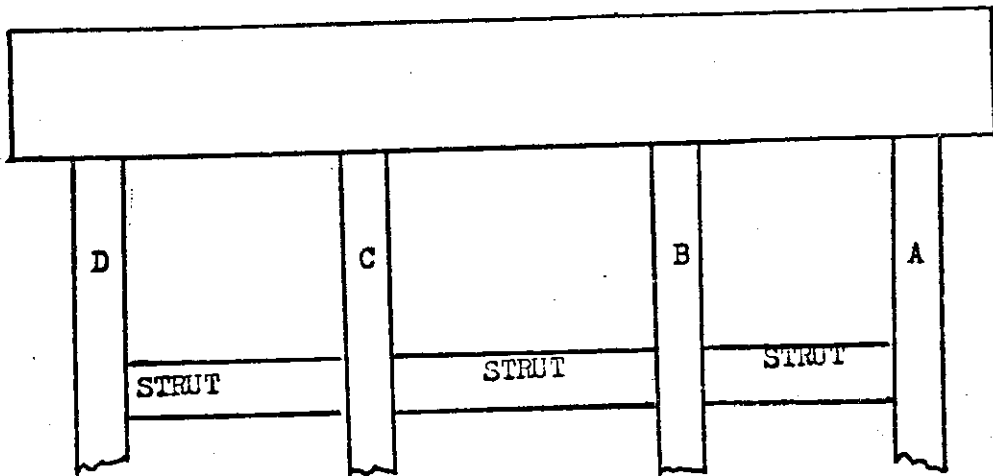
END VIEW



DB

REAR VIEW

END VIEW



ELEMENT

RATING

CONTENT

BEARINGS

G F P C

CAP

G F P C

FINE CRACKS

PILING

A

G F P C

B

G F P C

C

G F P C

D

G F P C

STRUT

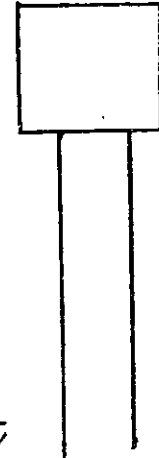
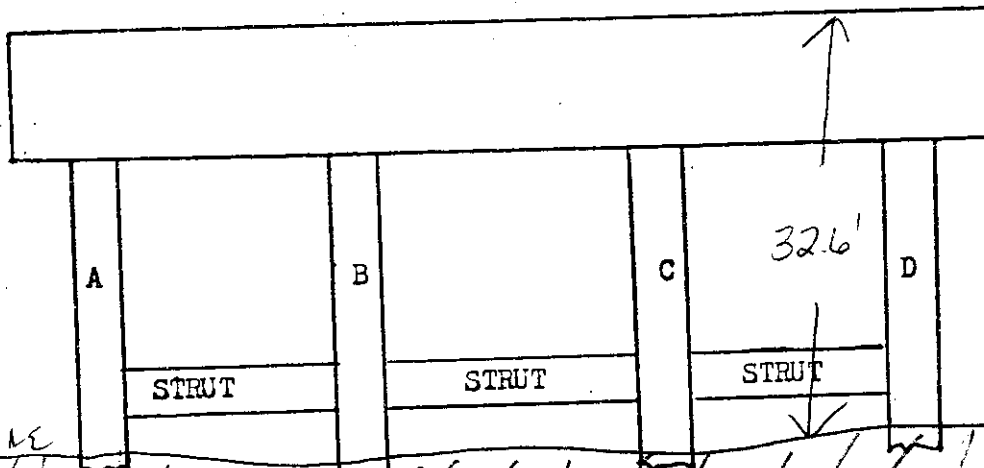
G F P C

BR. NO. 79 I-40 5.09SK. 60° RTAUG 08 2001
BENT NO. 2

RT

FRONT VIEW

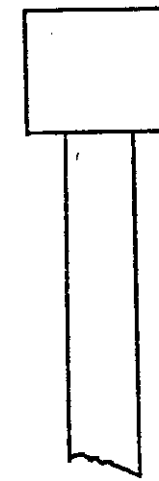
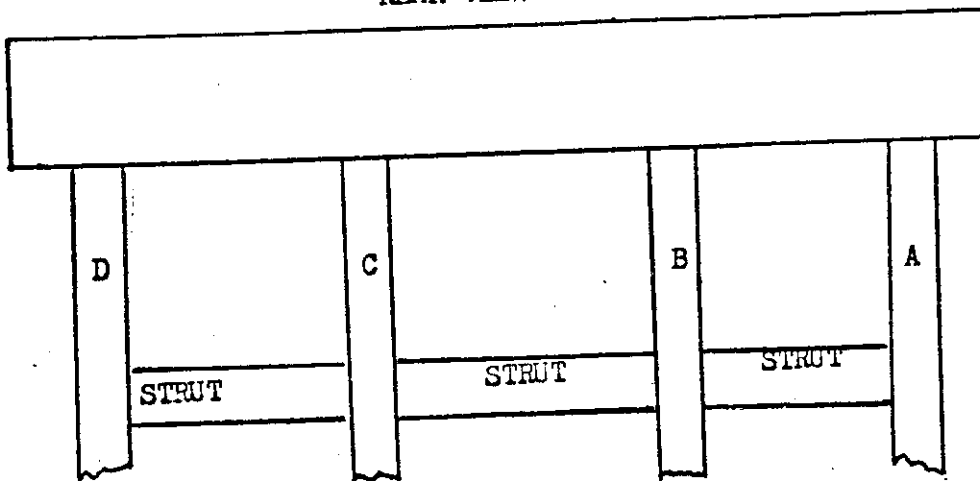
END VIEW



DB

REAR VIEW

END VIEW



ELEMENT

RATING

COMMENT

BEARINGS

(G) F P C

CAP

(G) F P C

PILING

A (G) F P C

B (G) F P C

C (G) F P C

D (G) F P C

STRUT

(G) F P C

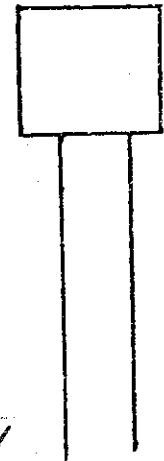
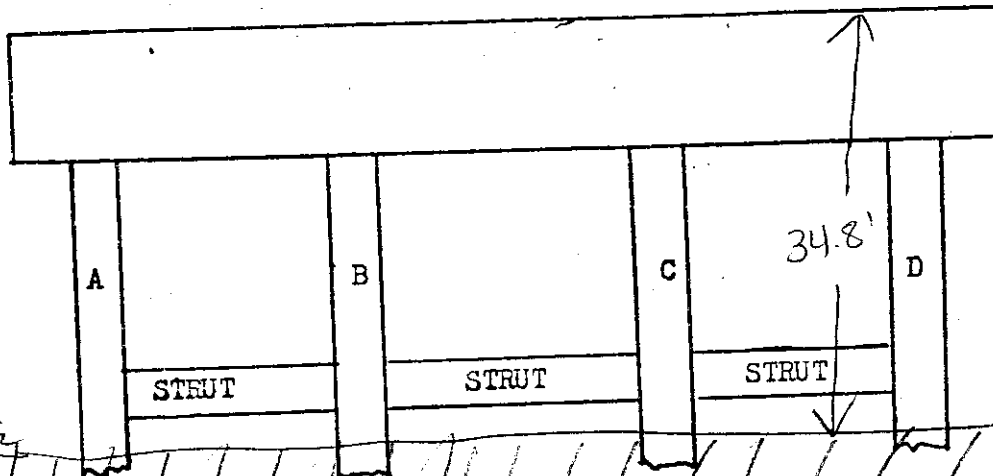
LIGHT POROUS

AUG 08 2001

BR. NO. 79 L-410 5.09SK. 60°RtBENT NO. 3

FRONT VIEW

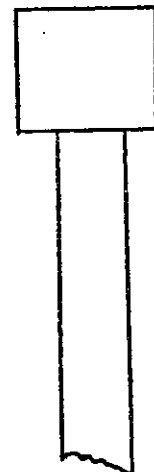
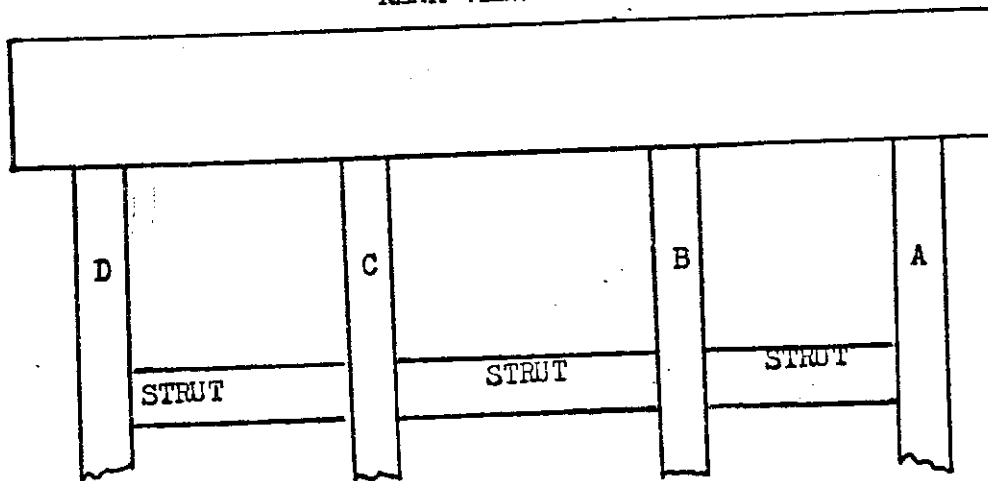
END VIEW



DB

REAR VIEW

END VIEW



ELEMENT - RATING COMMENT

BEARINGS

(G) F P C

CAP

(G) F P C

PILING

A (G) F P C

LIGHT P. P. BOUTS

B (G) F P C

C (G) F P C

D (G) F P C

STRUT

(G) F P C

ADD 06 2001

BR. NO. 79 F-40 5.09

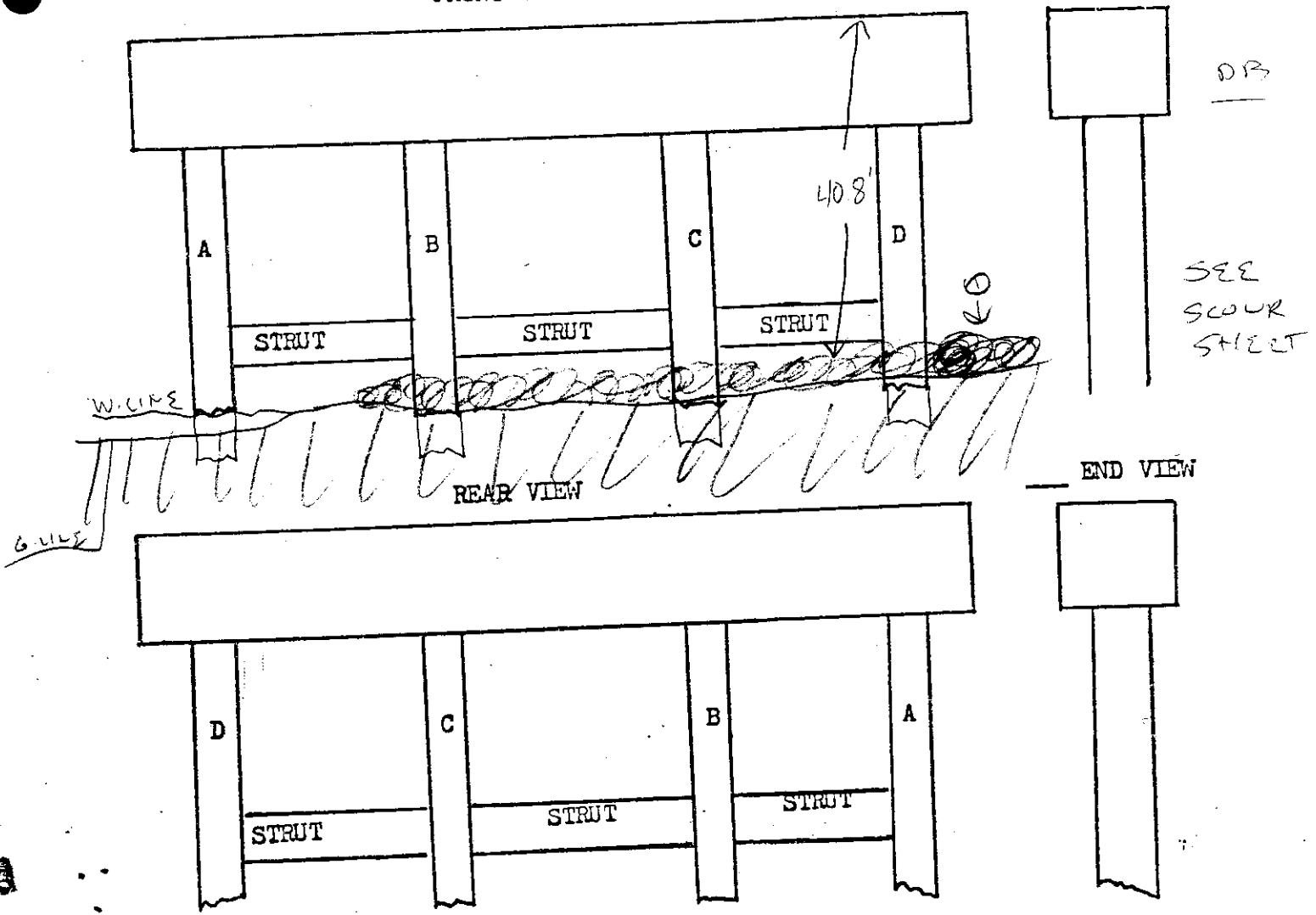
SK.

BENT NO. 4

RT

FRONT VIEW

END VIEW



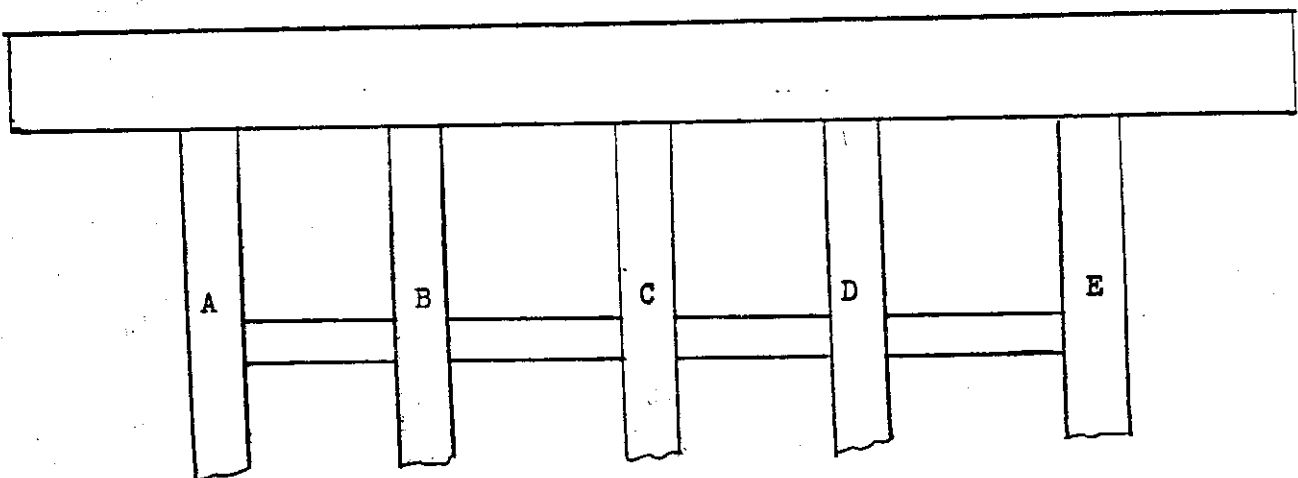
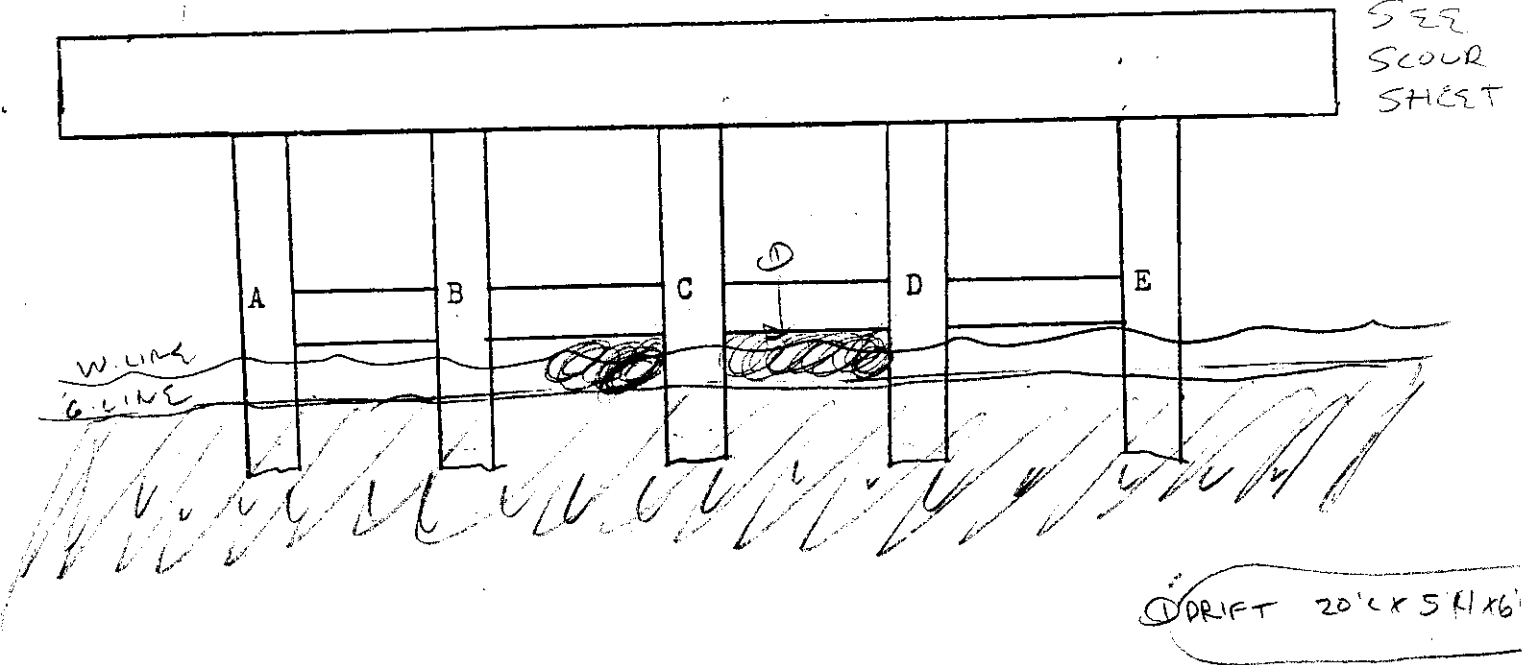
ELEMENT		RATING	COMMENT
BEARINGS		(G) F P C	(D) DRIFT 35' L X 6' H X 10' W
CAP		(G) F P C	
PILING	A	(G) F P C	LIGHT DRIFT 2
	B	(G) F P C	
	C	(G) F P C	
	D	(G) F P C	
STRUT		G F P C	
DRIFT		(P)	SEE (C)

AUG 03 2001

BRIDGE NO. 79 I-40 5.09 RT

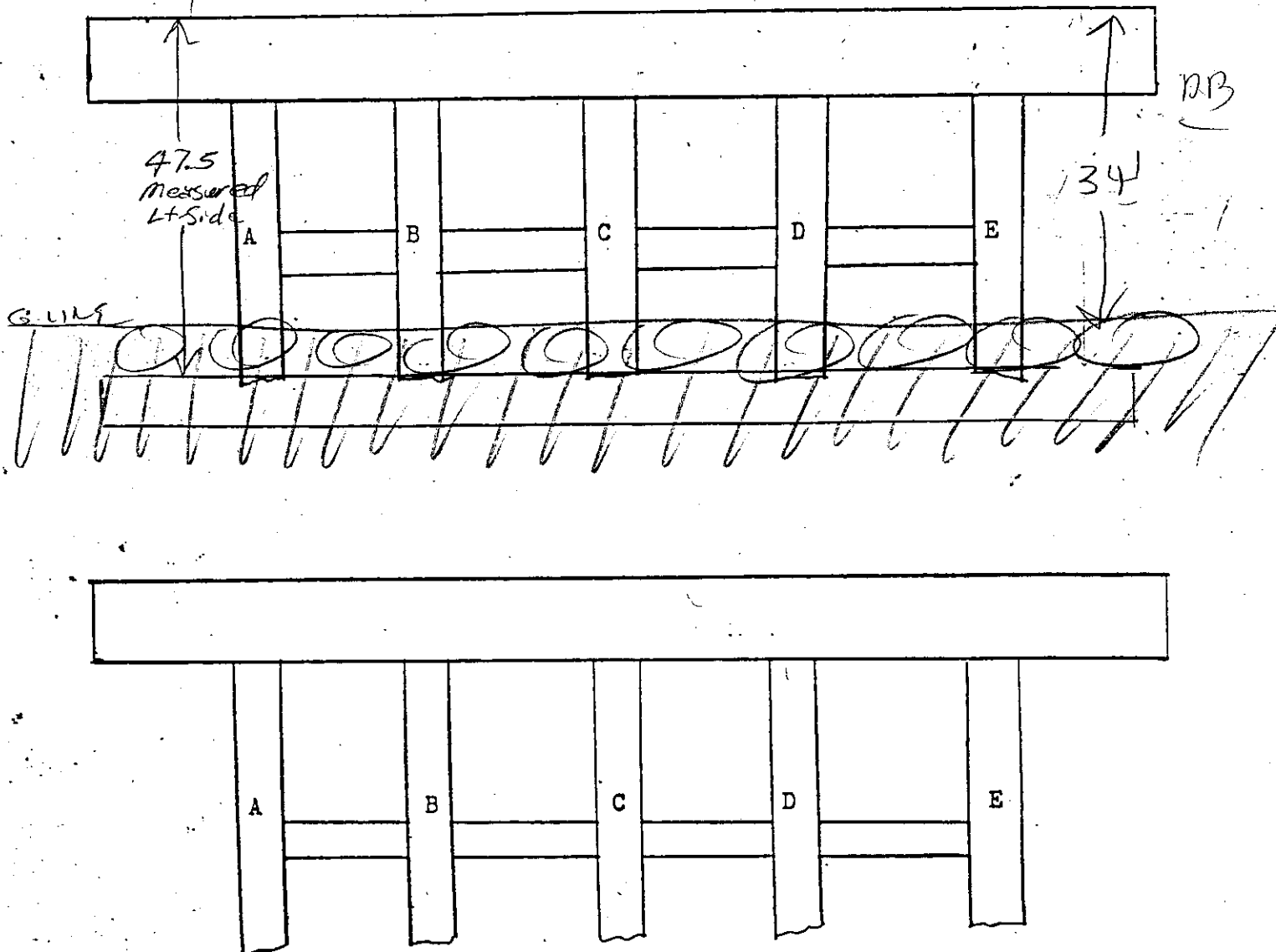
BENT NO. 5

SEE
SCOUR
SHEET



ELEMENT	RATINGS	COMMENT
CAP	(G) F P C	LIGHT POPOUTS & WATER ABR.
COLUMN A-E	(G) F P C	
	G F P C	
	G F P C	
	G F P C	
BEARINGS	(G) F P C	SEE (1)
STRUTS	(G) F P C	
DRIFT	(P)	

AUG 03 2001

BRIDGE NO. 79 I-40 5.09 RTBENT NO. 6

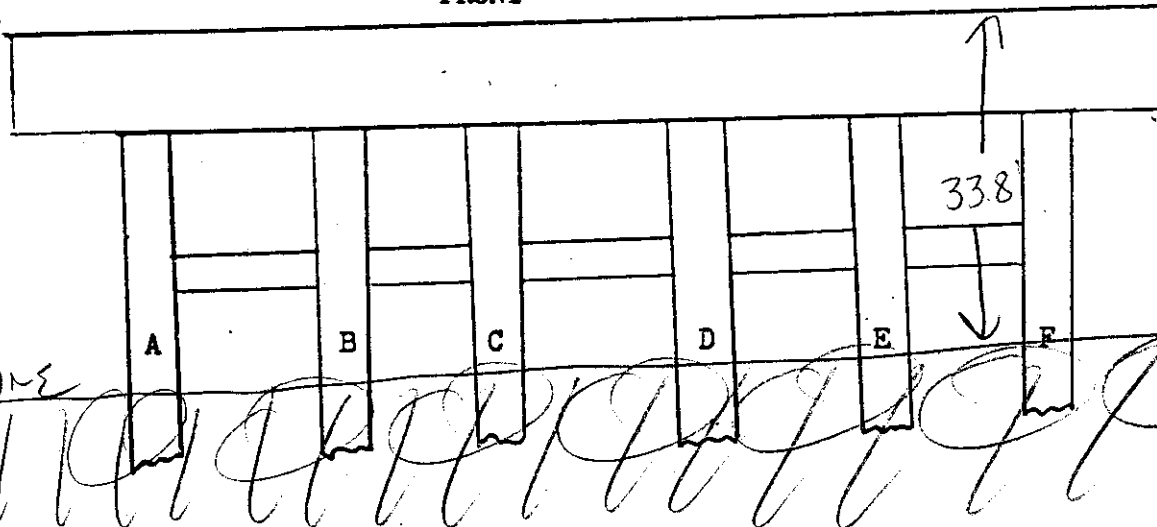
ELEMENT	RATINGS	COMMENT
CAP	(G) F P C	LIGHT PONDITS
COLUMN A-	G (F) P C	
PIPE	(G) F P C	
	G F P C	
	G F P C	
BEARINGS	(G) F P C	
STRUTS	(G) F P C	

AUG 08 2001

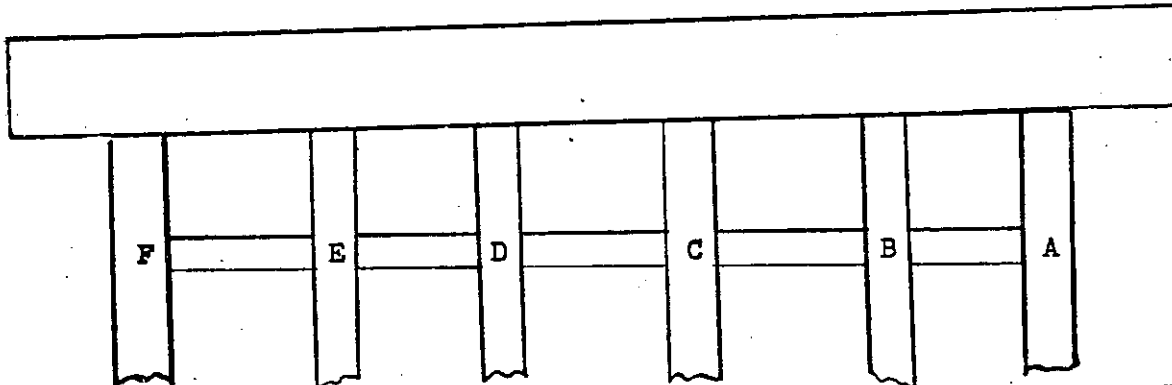
BR. NO. 79 I-40 5.09 ^{RT} SK. BENT NO. 7

DB

FRONT



REAR



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

CAP

G F P C

COLUMNS A

G F P C

B

G F P C

C

G F P C

D

G F P C

E

G F P C

F

G F P C

STRUTS

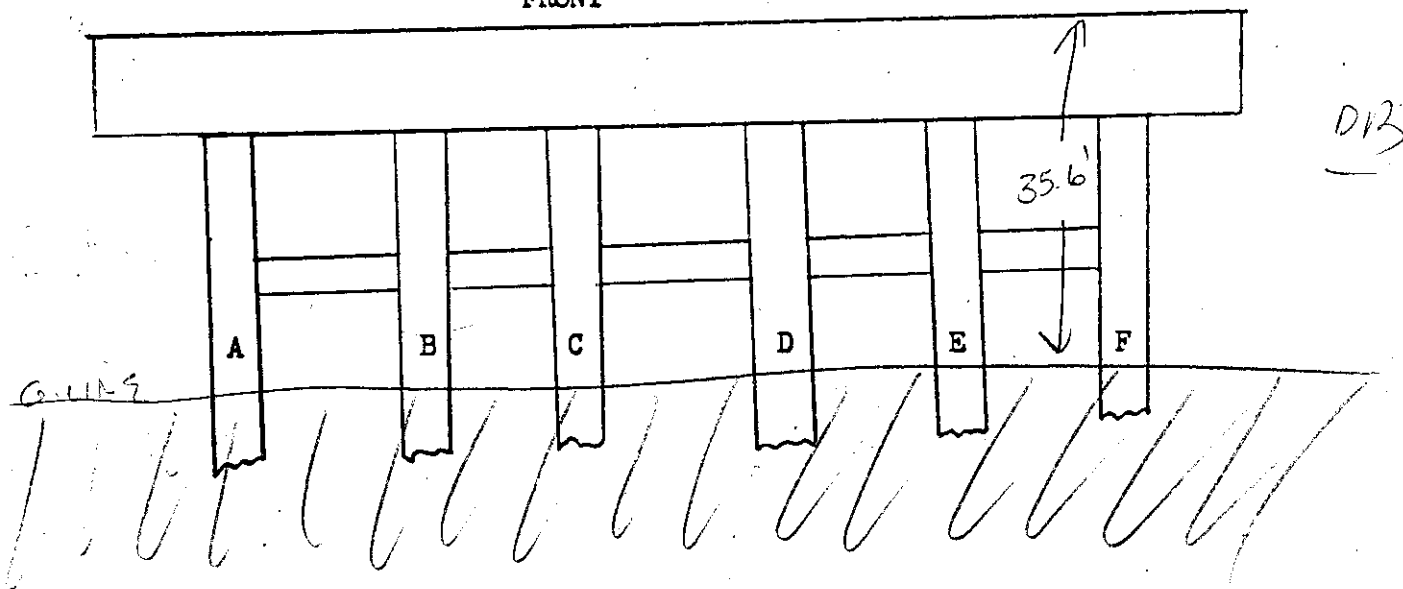
G F P C

LIGHT POPOUTS

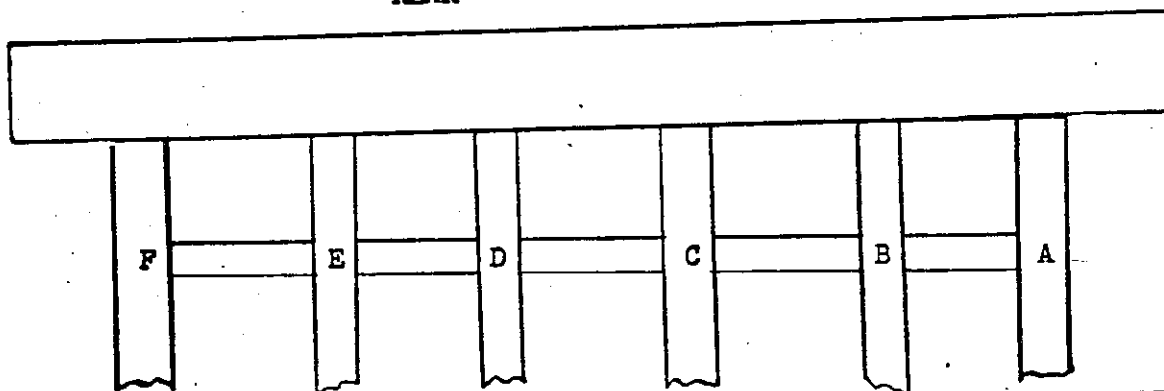
BR. NO. 79 I-20 ^{RT} 5.09 SK.

BENT NO. 8

FRONT



REAR



ELEMENT	RATING	COMMENT
BEARINGS	(G) F P C	
CAP	(G) F P C	
COLUMNS A	(G) F P C	LIGHT RODS
B	(G) F P C	
C	(G) F P C	
D	(G) F P C	
E	(G) F P C	
F	(G) F P C	
STRUTS	(G) F P C	

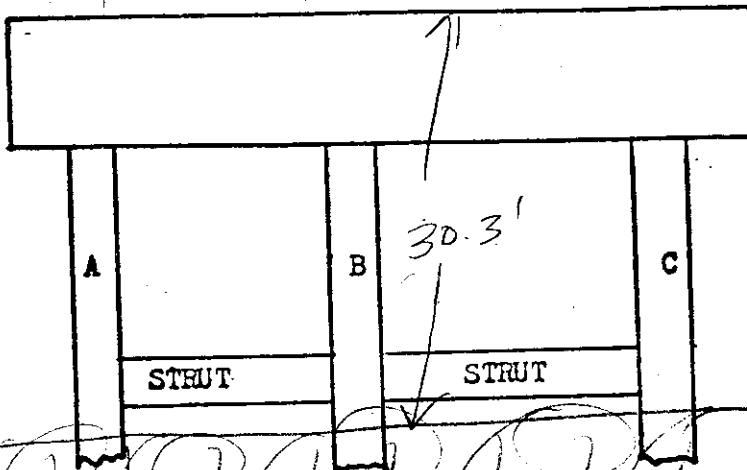
AUG 03 2001

BR. NO. 79 I-40 5.09 RT

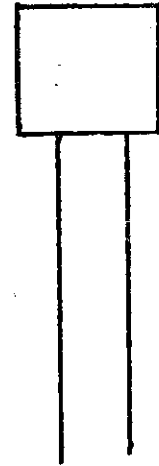
SK. _____

BENT NO. 9-A

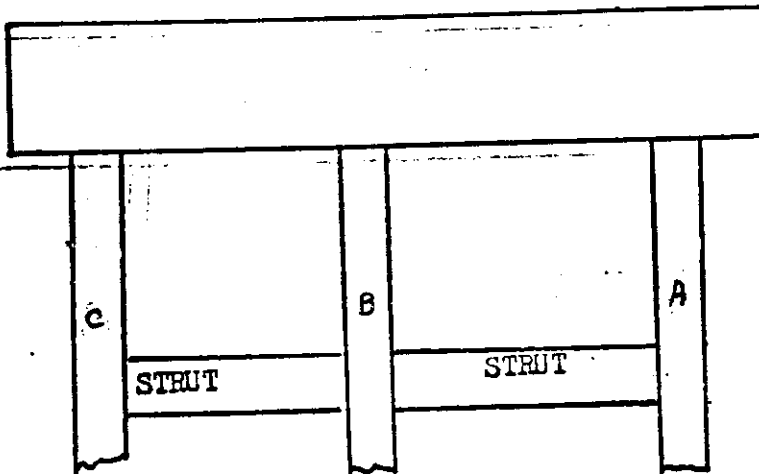
FRONT VIEW



END VIEW



REAR VIEW



END VIEW



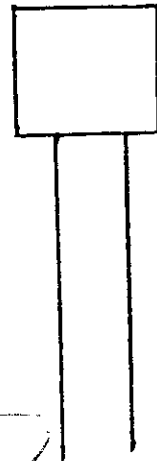
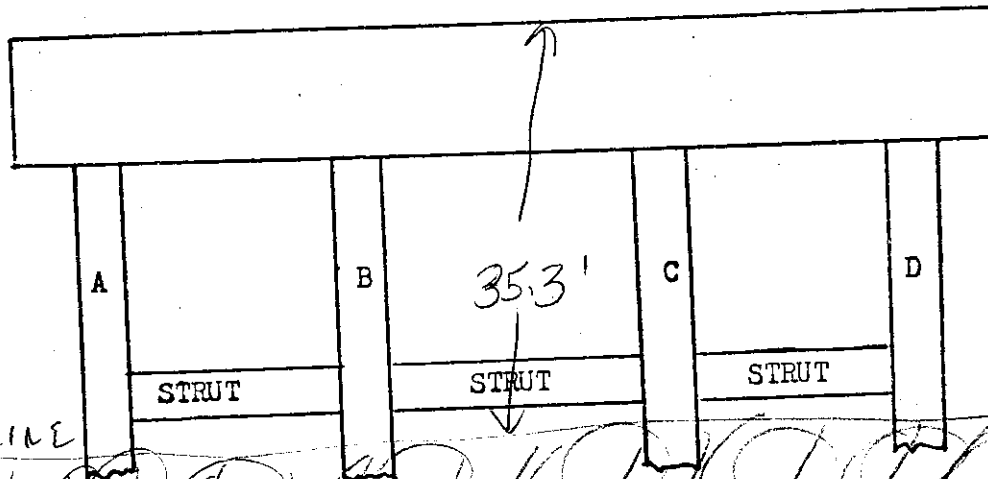
ELEMENT	RATING	COMMENT
BEARINGS	(G) F P C	
CAP	(G) F P C	
PILING	A	(G) F P C
	B	(G) F P C
	C	(G) F P C
STRUT	(G) F P C	LIGHT DROPOUTS
RIPRAP	(G)	

BR. NO. 79 I-40 5.09 *RT. Ramp* SK. *AUG 08 2003* BENT NO. 9-B

RT

FRONT VIEW

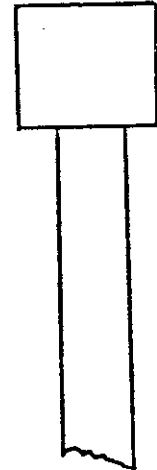
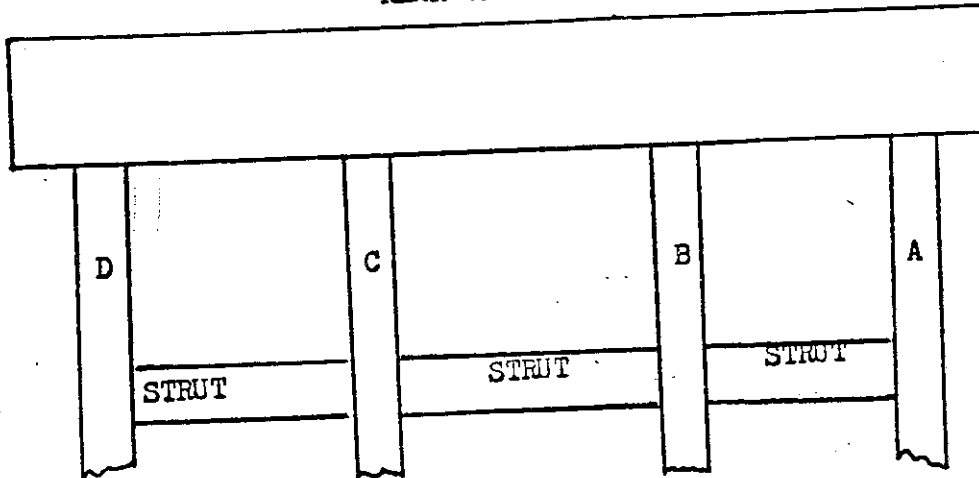
END VIEW



PR

REAR VIEW

END VIEW



ELEMENT		RATING	COMMENT
BEARINGS		G F P C	
CAP		G F P C	
PILING	A	G F P C	LIGHT POPOUTS /
	B	G F P C	
	C	G F P C	
	D	G F P C	
STRUT		G F P C	
RIPRAP		G	

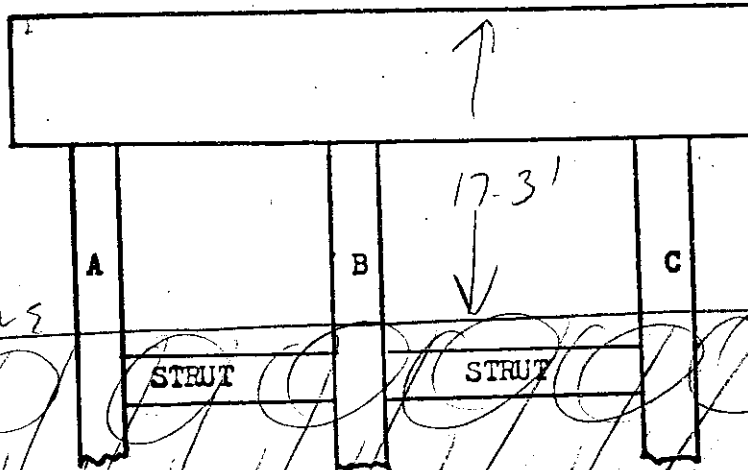
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SK. _____

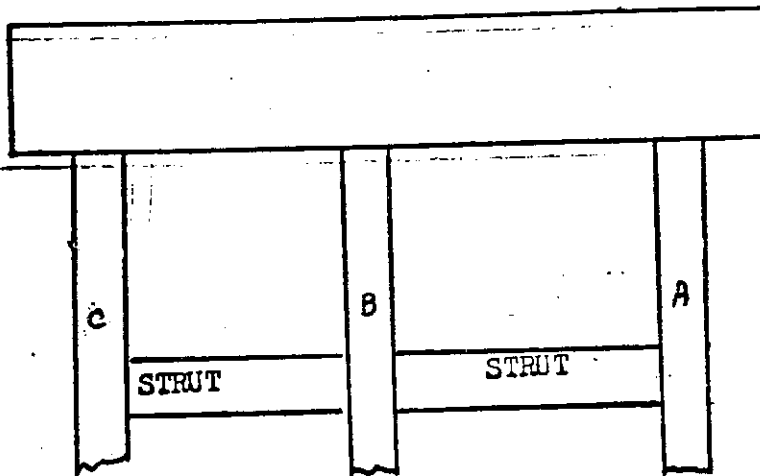
AUG 08 2001

BENT NO. 10-A

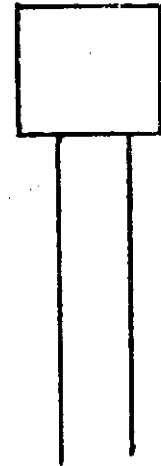
FRONT VIEW



REAR VIEW



END VIEW



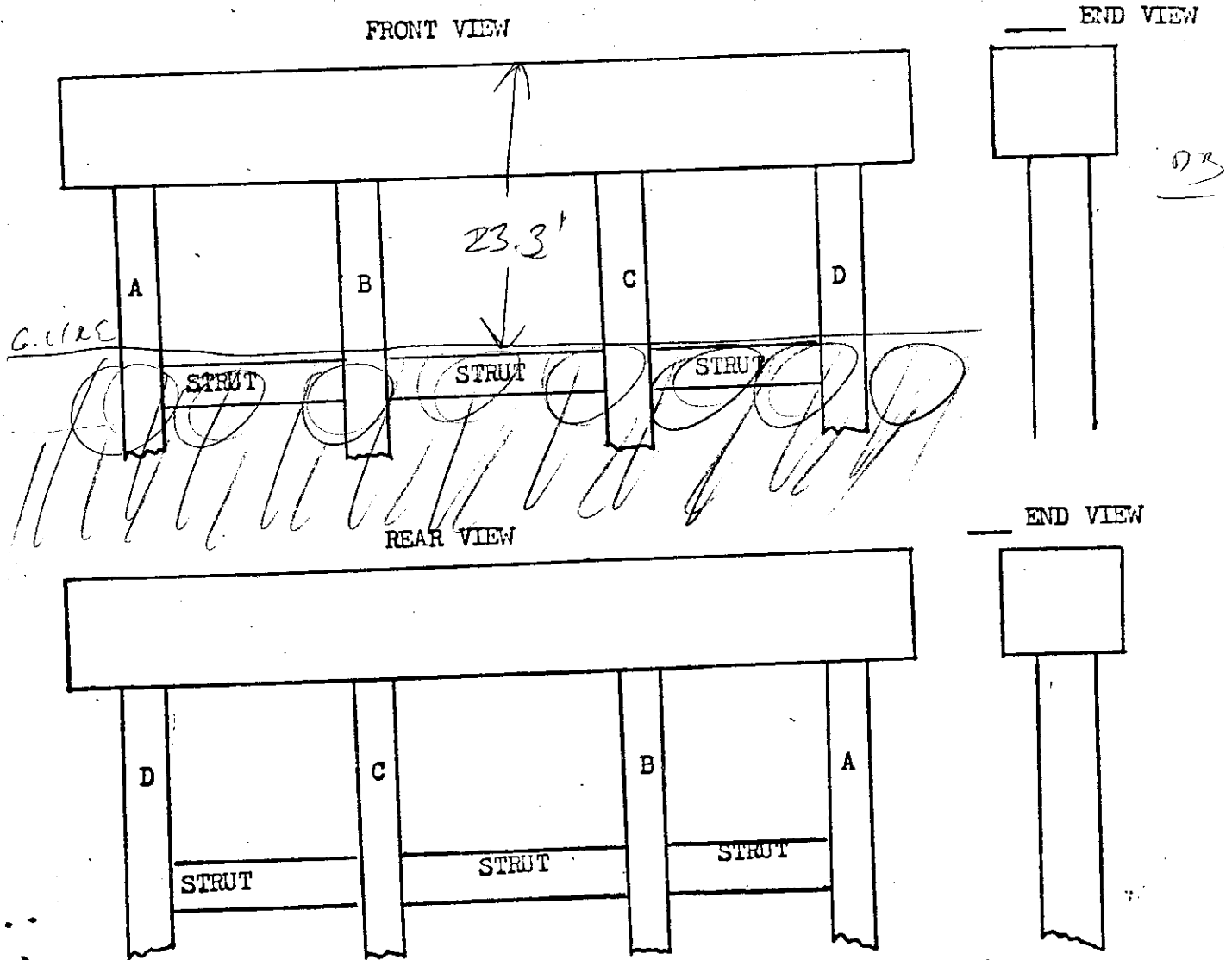
END VIEW



ELEMENT	RATING	COMMENT
BEARINGS	(G) F P C	
CAP	(G) F P C	
PILING	A (G) F P C	LIGHT POPS OUTS
	B (G) F P C	
	C (G) F P C	
STRUT	(G) F P C	
RIPRAP	(G)	

BR. NO. 79 I-40 5.09 RT Rt. Ramp SK.

AUG 03 2001
BENT NO. 10-B



ELEMENT		RATING	COMMENT
BEARINGS		(G) F P C	
CAP		(G) F P C	
PILING	A	(G) F P C	LIGHT RODENTS
	B	(G) F P C	
	C	(G) F P C	
	D	(G) F P C	
STRUT		(G) F P C	
RIPRAP		(G)	

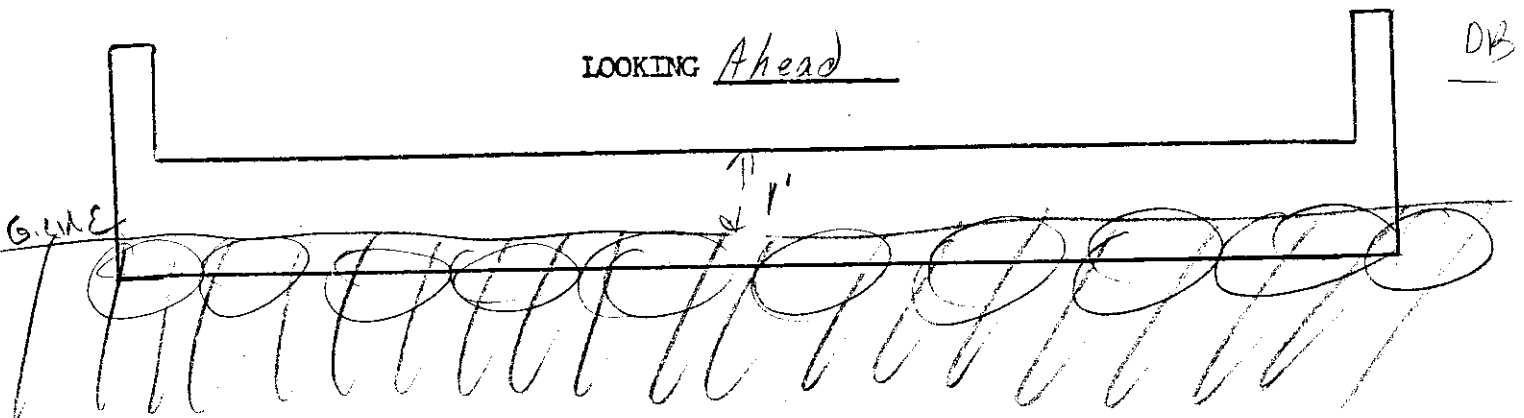
BRIDGE NO 79 T-40 509

AUG 09 2001
 ABUT. NO. 2-A
 RT

LOOKING Ahead

DB

G.M.E.



VIEW

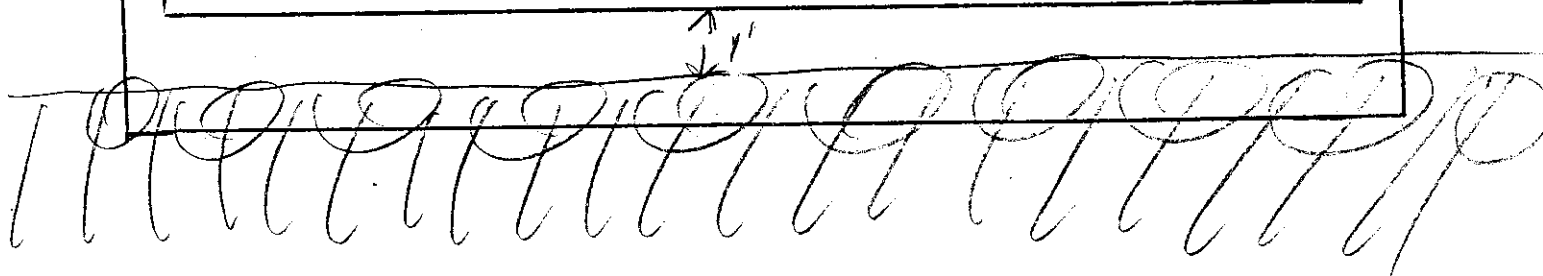
ELEMENT	RATING	COMMENT
BEARING	(G) F P C	
CAP	(G) F P C	
WINGS	(G) F P C	
EMBANKMENT	(G) F P C	
SLOPAVEMENT	G F P C	
VEG.	G F (P) C	
RIPRAP	(G)	N/A HEAVY IN GEN.

BRIDGE NO 79 7-40 5.09

ABUT. NO. 2-B
RT

LOOKING Ahead

D3



VIEW

ELEMENT	RATING	COMMENT
BEARING	(G) F P C	
CAP	(G) F P C	
WINGS	(G) F P C	
EMBANKMENT	(G) F P C	
SLOPAVEMENT	G F P C	
VEG.	G F (P) C	HEAVY IN GEN.
RIP RAP	(G)	

Date: 10/15/07

BRIDGE NUMBER:

Pg. # _____ of _____

CROSSING:

LAST EXPOSURE	ABUT/BENT/ PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO (OR GROUND LINE/ DATE FOR PILES	(t) FOOTING THICKNESS	W/FTG @ H= TOP OF CAP TO TOP OF FOOTING	EXPOSURE
	A-1			2	2'
	P-1	45.2'	4.25	42.44 L 38.90 R	38.9' 27.8'
	P-2	55.8	4.5	52.86 L 49.70 R	49.7 32.6'
	P-3	61.5	4.5	58.10 L 55.90 R	55.4 34.8'
47.0'	P-4	61.6	3.75	59.41 L 56.10 R	56.1 45.4'
46.0'	P-5	61.6	3.25	60.03 L 56.70 R	56.7 51.6'
	P-6	52.5'	3.25	50.95 L 47.17 R	49.06 34'
	P-7	53.3	3.25	51.78 L 49.20 R	49.2 33.8'
	P-8	49.3'	3.25	47.62 L 44.58 R	44.5 35.6'
	P-9A	46.1'	3.25	43.20 L 42.50 R	42.5 30.3'
	P-9B	38.1'	3.25	35.26 L 34.66 R	34.7 35.3'
	A-10-A B-10-A				17.3'
					23.3'
					1'

TOP OF CAP TO TOP OF WATER: _____

RIP-RAP: YES: (X) NO: ()

100.00' UPSTREAM: 47.3'

@ BENT/PIER NO.: _____

THRU STRUCTURE: _____

100.00' DOWNSTREAM: 48.8'

COMMENTS: _____

BRIDGE NO. _____
CO. _____ ROUTE _____ L/M _____ R/L _____

DATE: _____

TOTAL HEIGHT
(Top of Cap to Bottom of FTG)

61.6

BENT/PIER NO. 83

FOOTING THICKNESS (T)

3.25

SCOUR: G F P C

TOP OF CAP TO TOP OF FTG

56.1

DRIFT: G F P C

TOP OF CAP TO TOP OF WATER

45.3

MAXIMUM EXPOSURE _____

WATER DEPTH TO TOP OF FTG: _____

DEPTH

DISTANCE

DIRECTION OF FLOW

NOT

WATER

AUG 6 2001

BRIDGE NO. 79100400059

79 10040 0509 R
Co.: Route LogMile L/R

DATE: _____

TOTAL HEIGHT _____

(Top of Cap to Bottom of Ftg)

FOOTING THICKNESS (T) _____

TOP OF CAP TO TOP OF FTG
(W/ FTG @ H=) _____

TOP OF CAP TO TOP OF WATER 43.8' @ BENT #4

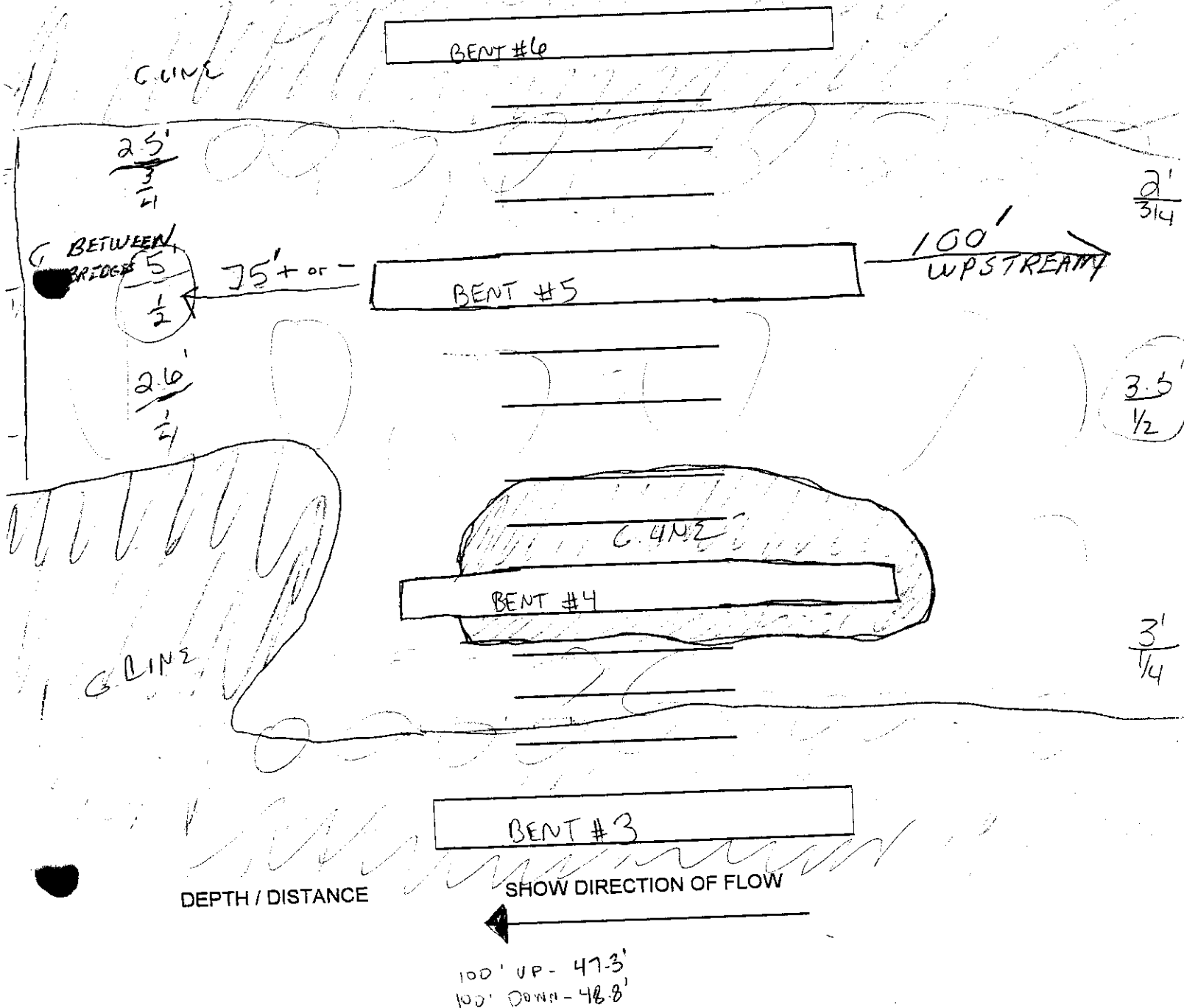
BENT/PIER NO. _____

SCOUR: G (F) P C

DRIFT: G F (P) C

MAXIMUM EXPOSURE 48.8'

WATER DEPTH TO TOP OF FTG: _____



BRIDGE NO. 79100400059

79 10040 0509 R
Co.: Route LogMile L/R

DATE: _____

TOTAL HEIGHT
(Top of Cap to Bottom of Ftg)

61.4'

BENT/PIER NO. 4

FOOTING THICKNESS (T)

3.75

SCOUR: G F P C

TOP OF CAP TO TOP OF FTG

56.10 RT SIDE

DRIFT G F (P) C

TOP OF CAP TO TOP OF WATER

43.8'

MAXIMUM EXPOSURE 45.4

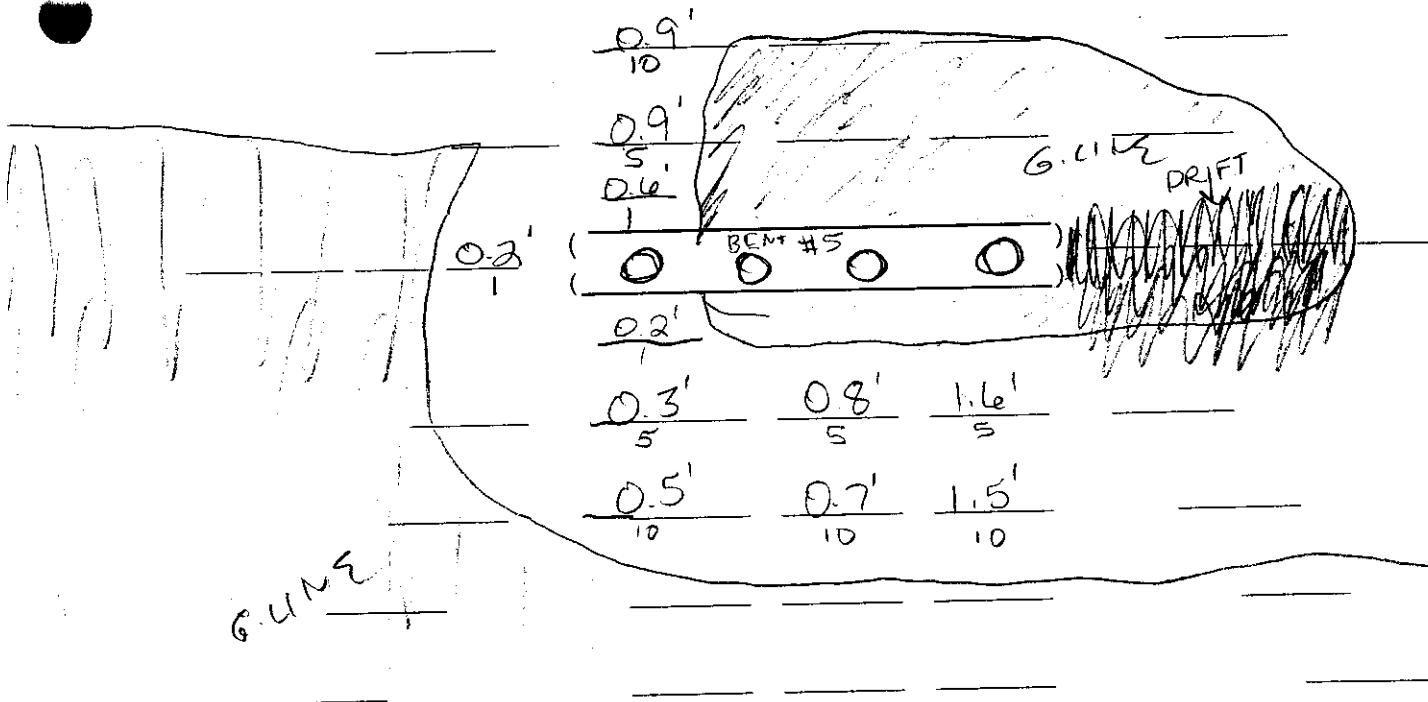
WATER DEPTH TO TOP OF FTG:

7

DEPTH
DISTANCE



DIRECTION OF FLOW



BRIDGE NO. 79100400059

79 10040 0509 R
Co.: Route LogMile L/R

DATE: _____

TOTAL HEIGHT
(Top of Cap to Bottom of Ftg)

FOOTING THICKNESS (T)

TOP OF CAP TO TOP OF FTG

TOP OF CAP TO TOP OF WATER

WATER DEPTH TO TOP OF FTG:

BENT/~~PIER~~ NO. 9

SCOUR:

G F P C

DRIFT

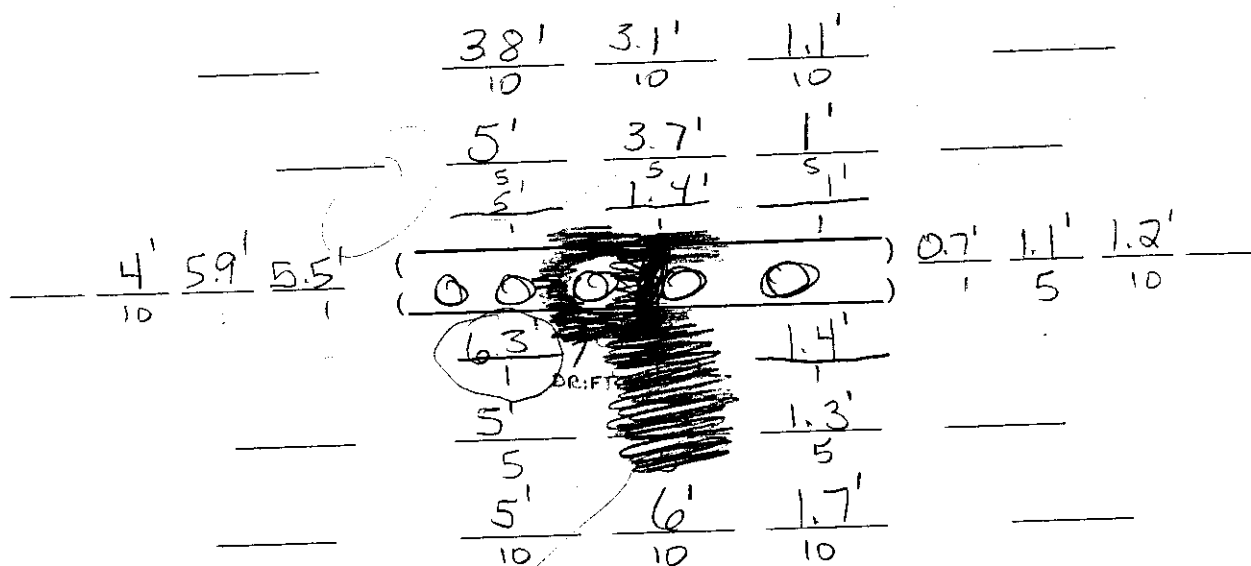
G F P C

MAXIMUM EXPOSURE 51.6'

DEPTH

DISTANCE

DIRECTION OF FLOW



DRIED POOL 20' x 5' x 1'
WATER

ENC. 10 21 06

BRIDGE NO. 79 - I40 - 509 - R
CO. ROUTE L/M R/L

DATE: _____

TOTAL HEIGHT
(Top of Cap to Bottom of FTG)

61.6

BENT/PIER NO. 46

FOOTING THICKNESS (T)

3.75'

SCOUR: G F P C

TOP OF CAP TO TOP OF FTG

56.1'-2

DRIFT: G F P C

TOP OF CAP TO TOP OF WATER

43.8'

RT. SLOPE
MAXIMUM EXPOSURE _____

WATER DEPTH TO TOP OF FTG: _____

DEPTH

DISTANCE

DIRECTION OF FLOW

NO WATER

100 FEET UPSTREAM

, STREAMBED ELEVATIONS BANK TO BANK

RIGHT OR LEFT OF BRIDGE

FEDERAL NUMBER --- 79I00400059

BRIDGE NO. . . : ----- 79-100400-509-r

DATE : 10/26/98

NO CHANGE AS OF 08-03-01

CROSSING ----- wolf

NUMBER OF PIERS : ----

LOCATION OF PIERS : ---

BENCH MARK ELEV. -- 300

INSPECTORS

BENCH MARK LOC. ---- Parapet rail

Williams

Parker

Coats

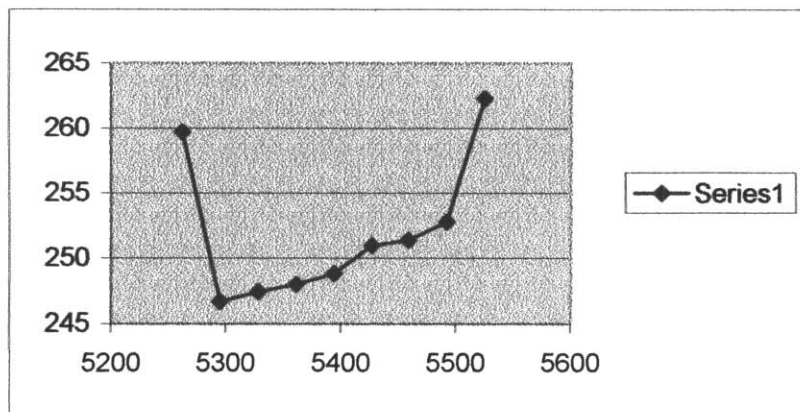
Kiestler

WATER ELEVATION : ---- 254.2

DISTANCE OF 0.00 = TOP OF BANK APPROACH 1 SIDE

DISTANCE AND ELEVATIONS IN I ARE IN STANDARD MEASUREMENT

5262	259.7
5295	246.7
5328	247.4
5361	248
5394	248.8
5427	251
5459	251.4
5492	252.8
5525	262.3



AUG 04 2001

100 FEET UPSTREAM

, STREAMBED ELEVATIONS BANK TO BANK

RIGHT OR LEFT OF BRIDGE

FEDERAL NUMBER --- 79100400059

BRIDGE NO. : ----- 79100400509 r

DATE :

10/26/98

CROSSING ----- wolf

NUMBER OF PIERS : ----

LOCATION OF PIERS : ---

BENCH MARK ELEV. -- 300

INSPECTORS

BENCH MARK LOC. ---- Parapet rail

Williams

Parker

Coats

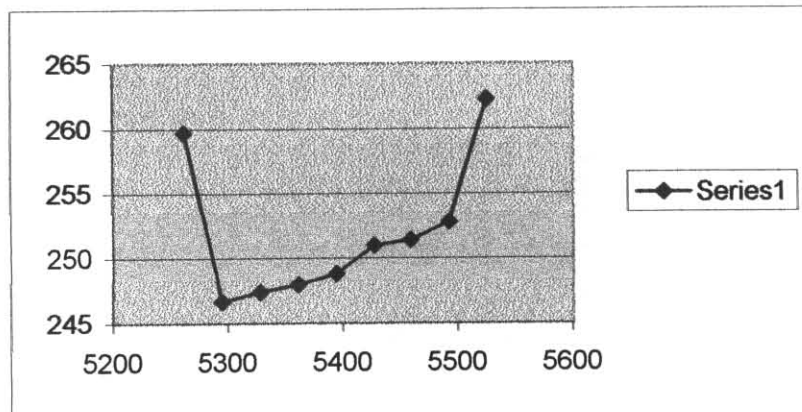
Kiestler

WATER ELEVATION : ---- 254.2

DISTANCE OF 0.00 = TOP OF BANK APPROACH 1 SIDE

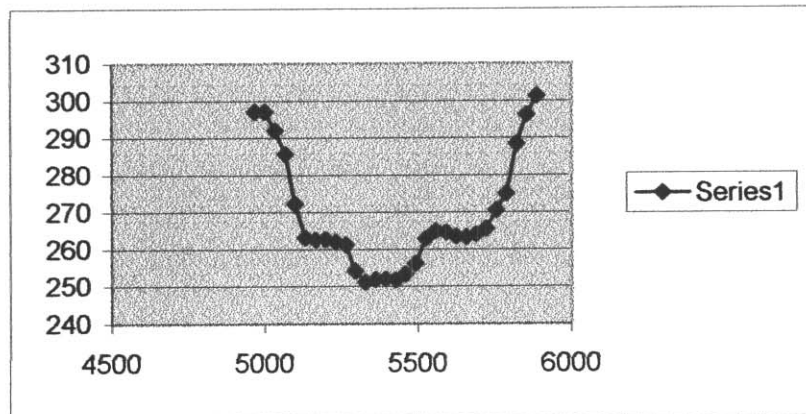
DISTANCE AND ELEVATIONS IN I ARE IN STANDARD MEASUREMENT

5262	259.7
5295	246.7
5328	247.4
5361	248
5394	248.8
5427	251
5459	251.4
5492	252.8
5525	262.3



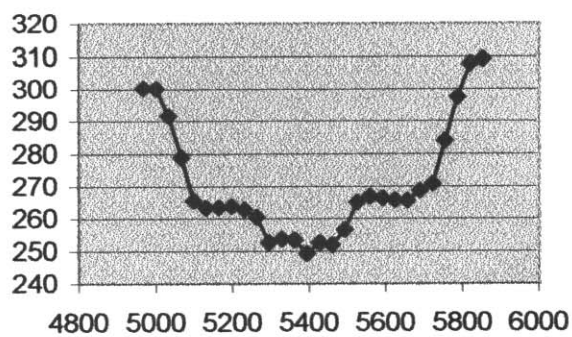
UPSTREAM GROUND ELEVATIONS @ EDGE OF BRIDGE

4967	297.1
5000	297.1
5033	292
5066	285.8
5098	272.3
5131	263.1
5164	262.5
5197	262.6
5230	261.9
5263	261.2
5295	254.2
5328	251.2
5361	251.9
5394	252.1
5427	251.8
5459	253.3
5492	256.2
5525	262.8
5558	264.9
5591	264.5
5623	263.4
5656	263.3
5689	263.9
5722	265.6
5755	270.6
5788	275
5820	288.5
5853	296.2
5886	301.2



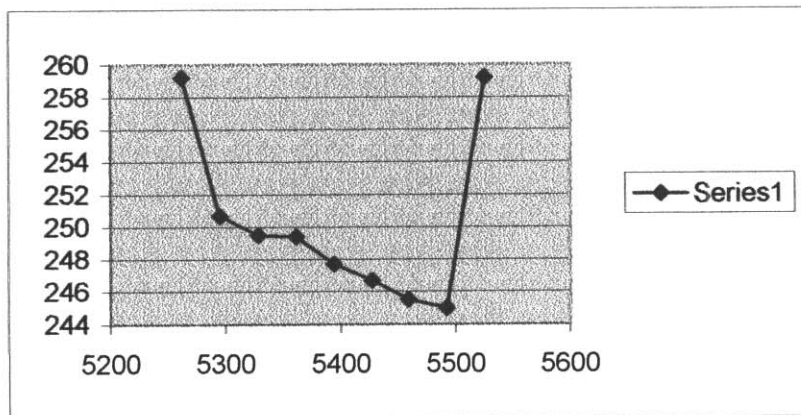
DOWNSTREAM GROUND ELEVATIONS @ EDGE OF BRIDGE

4967	300.4
5000	300.1
5033	291.7
5066	279
5098	265.5
5131	263.3
5164	263.4
5197	263.8
5230	262.8
5263	260.5
5295	252.6
5328	253.7
5361	253.4
5394	249.4
5427	252.61
5459	251.9
5492	256.6
5525	265.1
5558	266.9
5591	266.4
5623	265.8
5656	265.6
5689	268.7
5722	270.8
5755	283.9
5788	297.6
5820	307.9
5853	309.4



100 FEET DOWNSTREAM, STREAM ELEVATIONS BANK TO BANK

5262	259.2
5295	250.7
5328	249.5
5361	249.4
5394	247.7
5427	246.7
5459	245.5
5492	245
5525	259.2



REV. 07-01-91

SEP 16 1999

DATE : 9-16-99

BRIDGE NO. 79 COUNTY I40 ROUTE 5.09R LOG MILE

BENT/PIER NO. 4+5

TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : _____

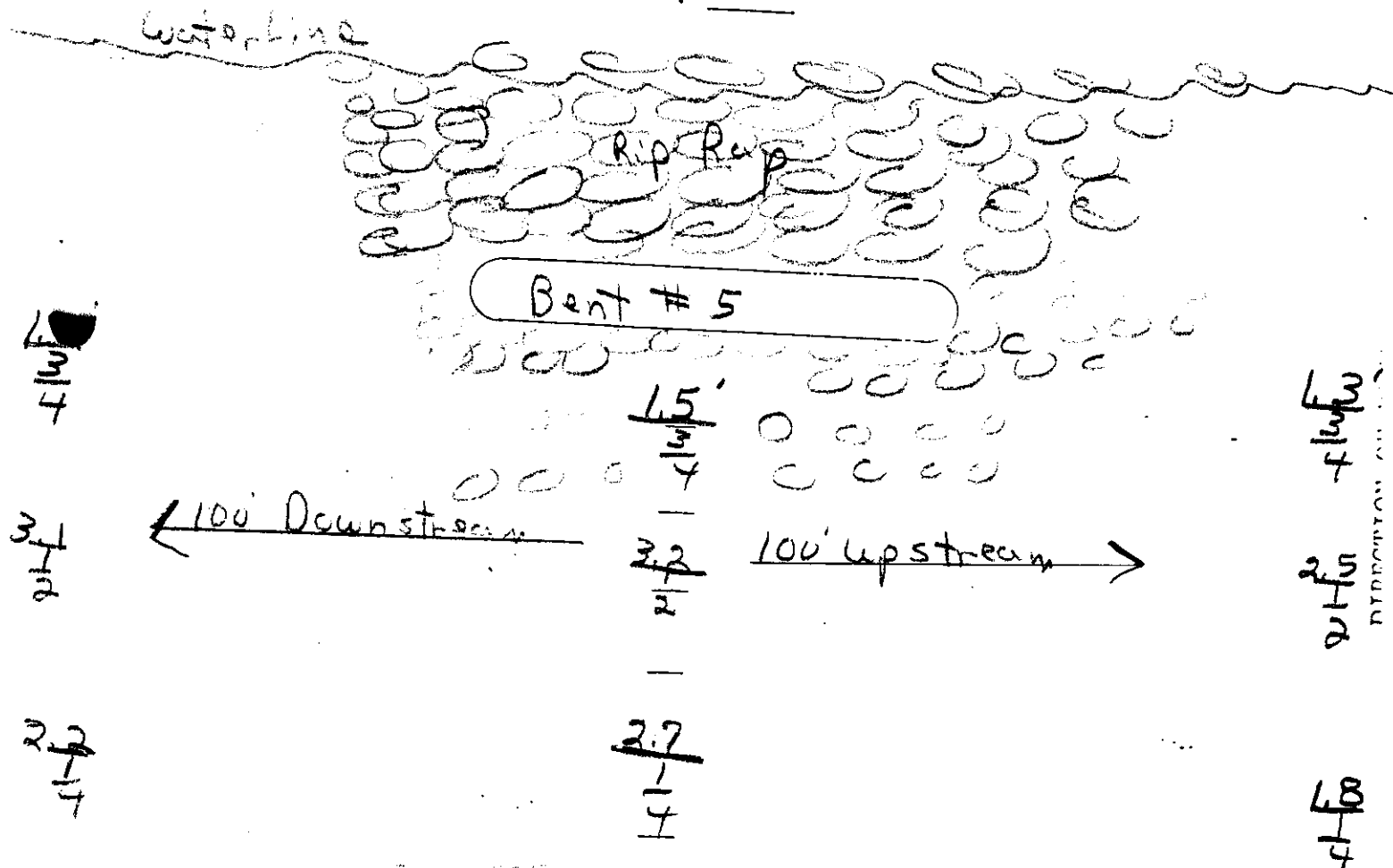
FOOTING THICKNESS (t) : _____

(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : _____

TOP OF CAP TO TOP OF WATER : _____

WATER DEPTH TO TOP FOOTING : _____

SCOUR : G ☒ P C
DRIFT : G F ☒ C
MAXIMUM EXPOSURE : _____



DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

SEP 16 1999

BRIDGE NO. 79 - I40 - 509 - R
CO. ROUTE L/M R/L

DATE: 9-16-99

TOTAL HEIGHT 61.6
(Top of Cap to Bottom of FTG)

BENT/~~PER~~ NO. 4

FOOTING THICKNESS (T) 3.9

SCOUR: G (F) P C

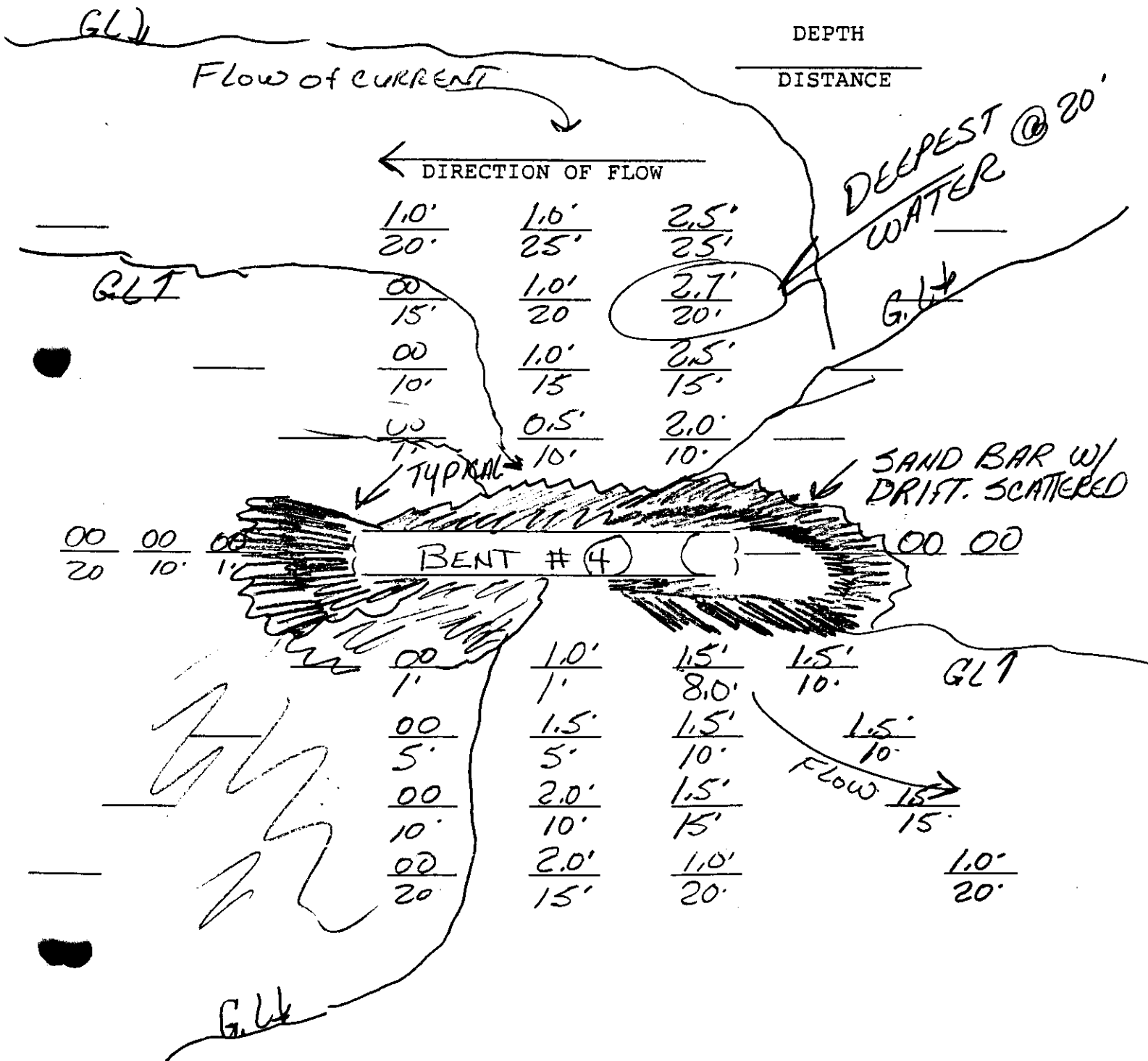
TOP OF CAP TO TOP OF FTG 57.7'

DRIFT: G F (P) C

TOP OF CAP TO TOP OF WATER 45'

MAXIMUM EXPOSURE 47

WATER DEPTH TO TOP OF FTG: _____



SEP 16 1999

BRIDGE NO. 7A - I 40 - 5.09 - R
CO. ROUTE L/M R/L

DATE: 9-16-99

TOTAL HEIGHT
(Top of Cap to Bottom of FTG) 53.3

BENT/~~PIER~~ NO. 5

FOOTING THICKNESS (T) 3.3

SCOUR: G (F) P C

TOP OF CAP TO TOP OF FTG 50.0

DRIFT: G F (P) C

TOP OF CAP TO TOP OF WATER 42.0

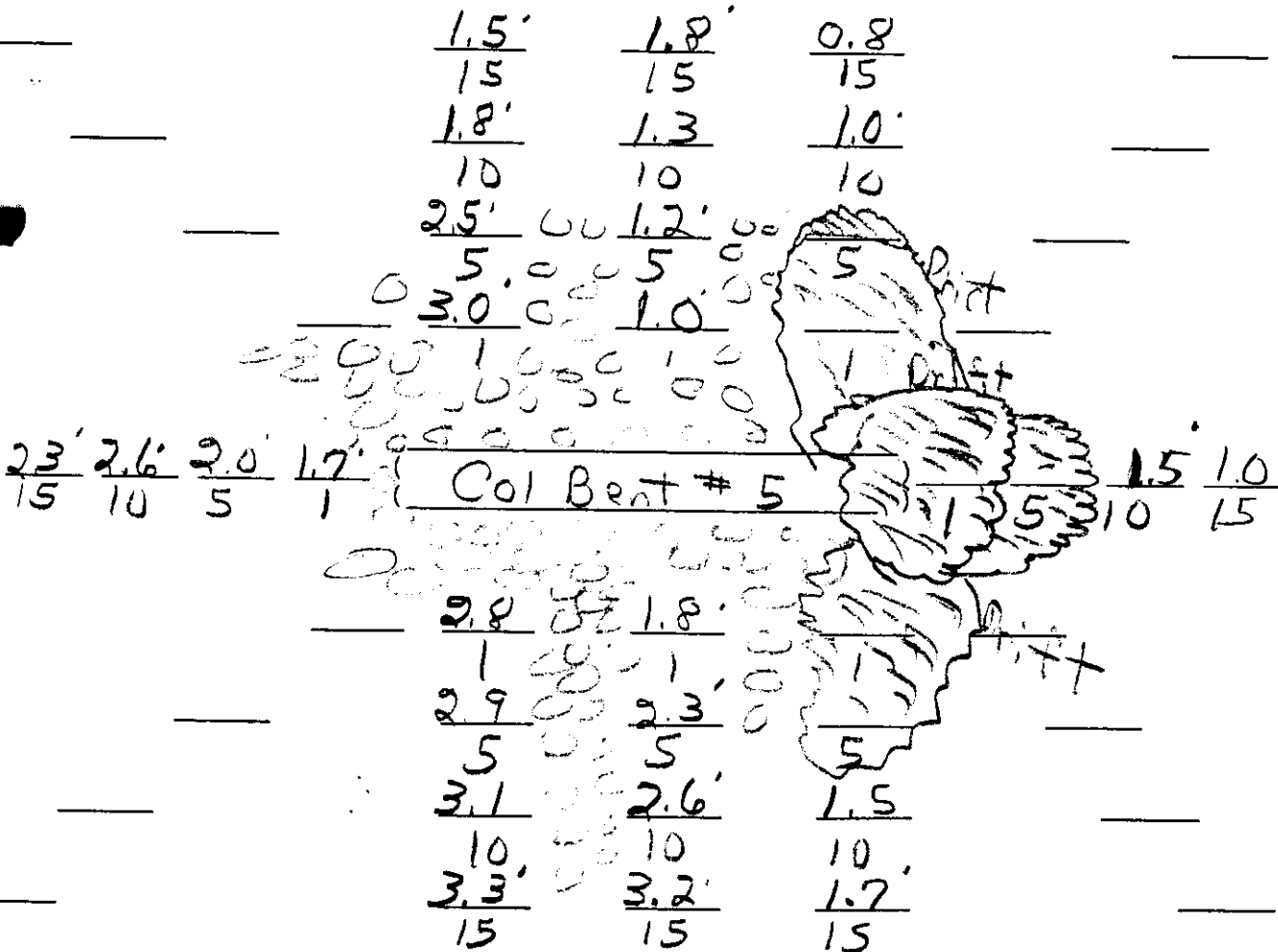
MAXIMUM EXPOSURE 46'

WATER DEPTH TO TOP OF FTG: _____

DEPTH

DISTANCE

△ DIRECTION OF FLOW



SEP 16 1999

Rev.: 07/29/97

DATE: _____

BRIDGE NUMBER: 79 - I40 - 5.09 R Pg. # of
COUNTY ROUTE LOG MILE LOG (km)

ABUT/BENT PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO (OR GROUND LINE/ DATE FOR PILES	(1) FOOTING THICKNESS	W/FTG @ H = TOP OF CAP TO TOP OF FOOTING	EXPOSURE
A-1				$\frac{1}{2}$
P-1	45.2	4.25	42.44 L 38.90 R 38.9	
P-2	55.8	4.5	52.86 L 49.70 R 49.7	
P-3	61.5	4.5	58.10 L 55.90 R 55.4	
P-4	61.6	3.75	59.41 L 56.10 R 56.1	47'
P-5	61.6	3.25	60.08 L 56.70 R 56.7	46'
P-6	52.5	3.25	50.95 L 47.40 R 57.4	
P-7	53.3	3.25	51.78 L 49.20 R 49.2	
P-8	49.3	3.25	47.62 L 44.58 R 44.5	
P-9	46.1	3.25	43.20 L 42.50 R 42.5	
P-10	38.1	3.25	35.26 L 34.66 R 34.7	
A-2				

} only
Bents
in waterTOP OF CAP TO TOP OF WATER: _____ RIP-RAP: YES (X) NO: ()
30.48 m UPSTREAM: _____ @ BENT / PIER NO.: #5, 6, 7, 8, 9

THRU STRUCTURE: _____

30.48 m DOWNSTREAM: _____

COMMENTS: Large Drift Rt Bent 4 & 5Bent 4 = 45' Top Cap to water } measured Rt
Bent #5 = 43' Top Cap to water } Side

DEC 17 1997
165

Rev.: 07/29/97

DATE: 12/17/97

BRIDGE NUMBER: 79 - I40 - 509R Pg. # of
COUNTY ROUTE LOG MILE LOG (km)

ABUT/BENT PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO (OR GROUND LINE/ DATE FOR PILES	(1) FOOTING THICKNESS	W/FTG @ H = TOP OF CAP TO TOP OF FOOTING	EXPOSURE
	MEASURED Rt. SIDE OF STRUCTURE			
A-1				.60
P-1	13.77	1.29	11.85	8.07 m
P-2	17.00	1.37	15.14	9.60 m
P-3	18.74	1.37	17.03	10.97 m
P-4	18.74 18.46	1.37 1.14	17.09	13.85 m
P-5	18.77	0.99	17.28	14.67 m
P-6	16.06 16.06	1.37	14.69	13.45 m
P-7	^{15.62} 16.29 16.24 16.28	0.99 1.21	^{14.99} 14.99	10.45 m
P-8	15.02	0.99	13.58	10.36 m
P-9	14.05	0.99	12.95	10.27 m
P-10	11.61	0.99	10.56	6.85 m
A-2				

TOP OF CAP TO TOP OF WATER: 12.85 m RIP-RAP: YES (X) NO: ()

30.48 m UPSTREAM: 13.60 m @ BENT / PIER NO.: VISIBLE

THRU STRUCTURE: 14.67 AROUND BENT #6

30.48 m DOWNSTREAM: 13.30

COMMENTS: PLAN (YES) NOT IN OFFICE

MEASUREMENT OF PLANS - FOR - PLAN
(93) REPORT

BENT #1 THRU 4
BENT #5 & 6
BENT #7 & 8
BENT #9 & 10
BENT #10A & 10B

BRIDGE NO. 79 I40 5.09R DATE: 12-16-97
COUNTY ROUTE LOG MILE LOG (km)

COUNTY	ROUTE	LOG MILE	LOG (km)
--------	-------	----------	----------

TOTAL HEIGHT
(Top of Cap to Bottom of FTG)

BENT/PER NO

FOOTING THICKNESS (T)

SCOUR: G F P C

TOP OF CAP TO TOP OF FTG
(W/FTG @ H =)

DRIFT G F P C

TOP OF CAP TO TOP OF WATER

MAXIMUM EXPOSURE

WATER DEPTH TO TOP OF FTG.

(SEE P. 125)

$$\frac{3}{4} = 0.25^m$$
$$\frac{1}{2} \pm 0.45^m$$

1.82m

1.35 m

13th

3000

 $\frac{1}{4} = 0.15$

SAND
E
Gravel

74

#4 1907

9.8 ~~4~~ 4.5 x 2.5 = 11.25

DEPTH

~~DISTANCE~~

SHOW DIRECTION OF FLOW

Flow

 $\frac{1}{2} - 6$

24

BRIDGE NO. 79 I40 5.09E
COUNTY ROUTE LOG MILE LOG (km)

TOTAL HEIGHT
(Top of Cap to Bottom of FTG)

FOOTING THICKNESS (T)

SCOUR: G F P C

TOP OF CAP TO TOP OF FTG
(W/FTG @ H =)

G F P C


TOP OF CAP TO TOP OF WATER

MAXIMUM EXPOSURE

WATER DEPTH TO TOP OF FTG:

(SEE PIER 5)

$$\frac{3}{4} = \underline{0.25^m}$$
$$\frac{1}{2} \pm 0.45^m$$

Drift \rightarrow 

Flow

1.35 m

SAND
BAR

Drift
 $1.300m \times 2.00m \times 2.00m$
 Thick $\frac{3}{4} = 0.75$

 $\frac{1}{2} = 0.25$

#5 Column 12.20
Benit

1.82m

Flow

Diagram of a river cross-section showing a sand and gravel bar. The bar is 30.00m wide and 0.15m high. The river is labeled "SAND & Gravel".

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

Embedment

Dr. f + 2.00 m
9.5 m ~~x~~ 4.5 m x Thick
w H

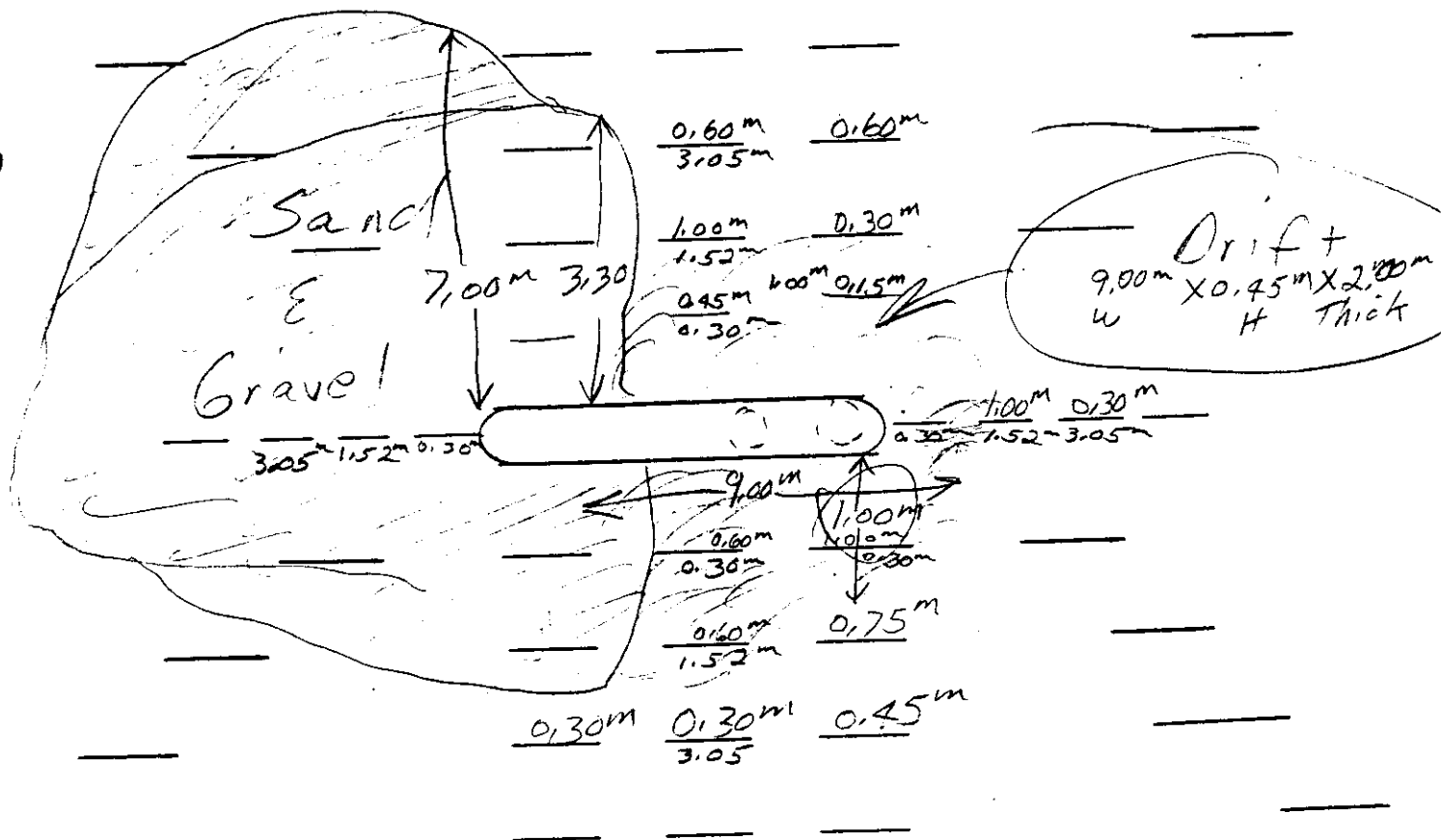
$$\frac{1}{q} = 0,60^m$$

DEC 17 1997

BRIDGE NO. 59 1-40 301 R DATE: 12-16-97 171
 COUNTY ROUTE LOG MILE LOG (km)

TOTAL HEIGHT 18.77m? Column
 (Top of Cap to Bottom of FTG) 16.70 BENT/PIER NO 2 4
 FOOTING THICKNESS (T) 1.14m?
 TOP OF CAP TO TOP OF FTG 17.09m? SCOUR: G (P) P C
 TOP OF CAP TO TOP OF WATER _____ DRIFT: G F (P) C
 WATER DEPTH TO TOP OF FTG: _____ MAXIMUM EXPOSURE 13.85m

DEPTH
 DISTANCE
 DIRECTION OF FLOW



BRIDGE NO. 79 I40 5.09R DATE: 12/16/97 173
COUNTY ROUTE LOG MILE LOG (km)

16.24

BENT/~~PER~~ NO 5

0.99

SCOUR: G E P C

17.28m

DRIFT: G F P C

MAXIMUM EXPOSURE 14.67^m

DISTANCE

DIRECTION OF FLOW

0.30

$$\underline{452m}$$
$$\begin{array}{r} 0.45 \\ \hline 3.05 \end{array}$$
$$\begin{array}{r} 1.35 \\ \hline 3105 \end{array}$$
$$\begin{array}{r} 0.60m \\ \hline 1.52m \end{array}$$
$$\frac{1,15}{1152} m$$

0.30
0.30m

030

$$\begin{array}{r} 0,45 \\ \hline 3,05 \end{array} \quad \begin{array}{r} 0,60 \\ \hline 1,52 \end{array} \quad \begin{array}{r} 0,31 \\ \hline 0,30 \end{array}$$
$$\begin{array}{r} 0.45 \\ 0.75 \\ \hline 0.30 \end{array} \quad \begin{array}{r} 1.52 \\ 3.05 \\ \hline \end{array}$$
$$\begin{array}{r} 0.60^m \\ \hline 0.30 \end{array}$$
$$\frac{0.45^m}{0.30^m}$$
$$\begin{array}{r} 1.82^m \\ \underline{1.52^m} \end{array}$$
$$\frac{1.15^m}{1.52^m}$$
$$\begin{array}{r} 0.30^m \\ \hline 1.52 \end{array}$$
$$\frac{1.05^m}{3.05^m}$$
$$\frac{1.62^m}{3.05^m}$$
$$\frac{0.30^m}{3.65}$$

Drift

13.00m x 2.00m x 2.00m
L H Thick

DEC 17 1997
175

BRIDGE NO. 29 I-40 5.09R

DATE 12-16-97

TOTAL HEIGHT:
(TOP OF CAP TO BOTTOM OF FTG.)

Column
BENT/PIER NO. 6

FOOTING THICKNESS (t):

SCOUR: G F P C

DRIFT: G E P C

TOP OF CAP TO TOP OF FTG.:

17.49m

MAXIMUM EXPOSURE 13.4

TOP OF CAP TO TOP OF WATER:

LC
RL

WATER DEPTH TO TOP OF FTG.:

DEPTH
DISTANCE

DIR. OF FLOW

Limestone Rock Visible
Around Bent Columns
A, B, & C

0.45	0.60	
3.05	3.45	3.05
0.45	0.55	SA
1.52	1.52	1.52
0.45	0.15	N
0.30	0.30	0.30

0.75	0.55	0.45
3.05	1.52	0.30

0.60	0.30
0.30	0.30
0.45	0.60
1.52	1.52
1.00	1.00
3.05	3.05

Flow 1.00m

SAND

Flow



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

P. O. BOX 429
JACKSON, TENNESSEE 38302-0429

April 30, 1997

Memorandum

TO: MR. PAUL SHARP, CIVIL ENGINEERING MANAGR I
FROM: *Cindy Walker FOR P* MR. BILL HAZLERIG, REGION BRIDGE ENGINEER
SUBJECT: BRIDGE #79 - I0040 - 5.09L & R

Please find enclosed scour notes on the above referenced structures. Also enclosed is a copy of the fax requesting this scour inspection footnoted with an explanation of the "reinforcing rods".

BH: csw

cc: File

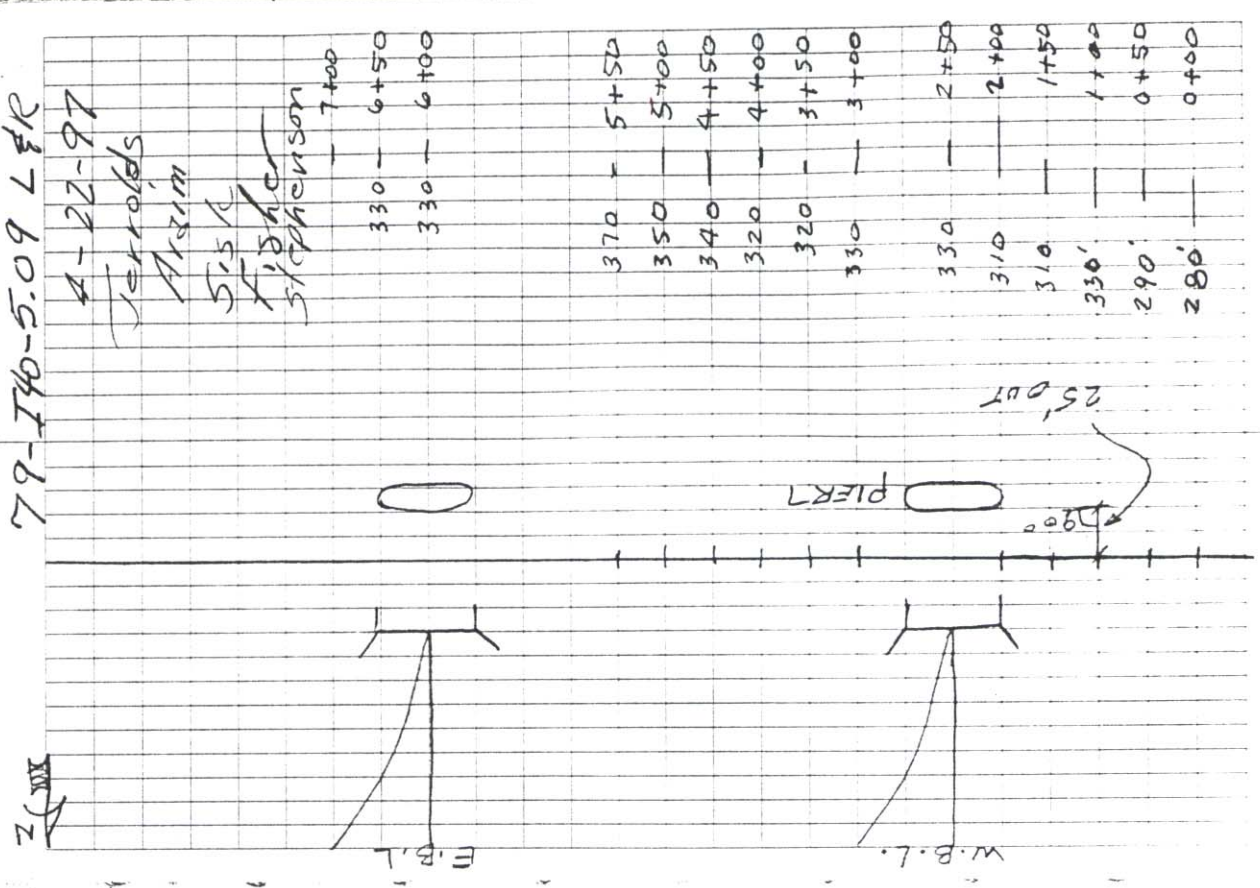
RECEIVED

MAY 2 1997

D.O.T.
BRIDGE INSPECTION & REPAIR

pus

X-Sections 79-140-5.09 LER



(3)

Sta B.S. H. I. F.S. Elev
-1.71 215.54 217.25

1+50

2+00

2+50

K-Fisher
M-506
P-506

4-25-97

Bottom Pier 7 strict At Bridge

0.0 4.2 21.3
5.5 3.4 210.0
9.5 206.0
20.1 9.5 195.4
17.1 1.0 198.4
17.6 210 197.9
9.5 206.0
7.8 300 207.7
7.6 340 207.9

4.7 8.0 200.8
5.5 210.0
9.5 206.0
17.4 9.5 198.1
17.2 1.0 198.3
17.6 210 199.2
9.5 206.0
7.0 303 208.5
7.1 340 208.4

5.4 210.1
4.2 211.3
9.5 206.0
18.8 9.5 192.7
19.2 1.0 196.3
19.3 210 199.2
9.5 206.0
7.3 330 208.2
7.8 350 207.7

4-25-97

(4)

Sta B.S. H. I. F.S. E/I

215.54

3+00

5.2
0.0
210.3
3.8
27
211.7
9.5
28
Edge
water
19.1
90
196.4
18.7
150
196.8
16.6
210
198.9
7.5
280
Edge
water
206.0
7.4
330
208.1
7.5
350
208.0

3+50

5.2
0.0
210.3
4.2
36
211.3
9.5
37
Edge
water
18.5
100
197.0
17.9
160
197.6
16.5
220
199.0
9.5
284
Edge
water
206.0
7.1
320
208.4
6.9
350
208.6

4+00

5.2
0.0
210.3
4.0
39
211.5
9.5
41
Edge
water
17.8
100
197.7
17.2
165
198.3
16.5
230
200.0
9.5
282
Edge
water
206.0
7.1
316
208.4
7.0
350
208.5

+1.71 217.25

B.M. See Pg 3

Tic = 0.00

4-25-97

Sta B.S. H. I. F.S. Elev.
-0.92 2.15.83 216.75

Bottom Pic 7 Strut (Rt. Bridge)

5.1	5.8	7.0	9.8	15.4	19.1	19.3	9.8	7.2	7.1
0.0	210.7	210.0	206.0	200.4	196.7	199.3	206.0	208.6	208.7
			Edge Water				Edge Water		
5.0	5.8	7.0	9.8	15.4	19.1	19.3	9.8	7.2	7.1
210.8	211.2	211.4	206.0	200.4	192.2	198.0	206.0	208.7	208.5
			Edge Water				Edge Water		
4.9	4.4	4.4	9.8	15.4	19.1	19.3	9.8	7.2	7.1
210.9	211.4	211.4	206.0	200.4	197.4	199.1	206.0	208.2	208.3
			Edge Water				Edge Water		
4.9	4.4	4.4	9.8	15.4	19.1	19.3	9.8	7.2	7.1
210.9	211.4	211.4	206.0	200.4	197.4	199.1	206.0	208.2	208.3
			Edge Water				Edge Water		

+0.92 216.75 Tie Brn. (See Above) Tie 0.00

②

Sta B.S. H. I. F.S. Elev.
2.71 214.04 214.75

4-25-97

Bottom Pier 7 Street Rt. B + ridge

209.1
4.9
0.0
2.4
4.2
211.6
8.0
4.8
Edge
water
198.4
15.6
12.5
19.2
2.0
194.8
19.6
12.4
2.75
206.0
3.0
3.9
Edge
water
210.5
3.5
3.78
208.6
5.4
4.10

6400

208.0
6.0
0.0
209.6
24
0.8
206.0
8.0
7.8
Edge
water
198.7
15.3
13.0
22.0
16.0
2.90
206.0
8.0
3.5
3.9
Edge
water
210.7
3.3
3.80
208.8
5.2
4.10

6450

207.8
6.2
0.0
208.2
5.8
5.0
1.00
210.1
206.0
13.8
19.0
195.8
18.2
2.35
17.5
3.00
206.0
8.0
3.9
Edge
water
211.1
2.9
3.89
209.6
4.4
4.20

7400

Cont. Next Pg.

4-25-97

⑦

Sta B.S. H. I. F.S. Elev
214.04

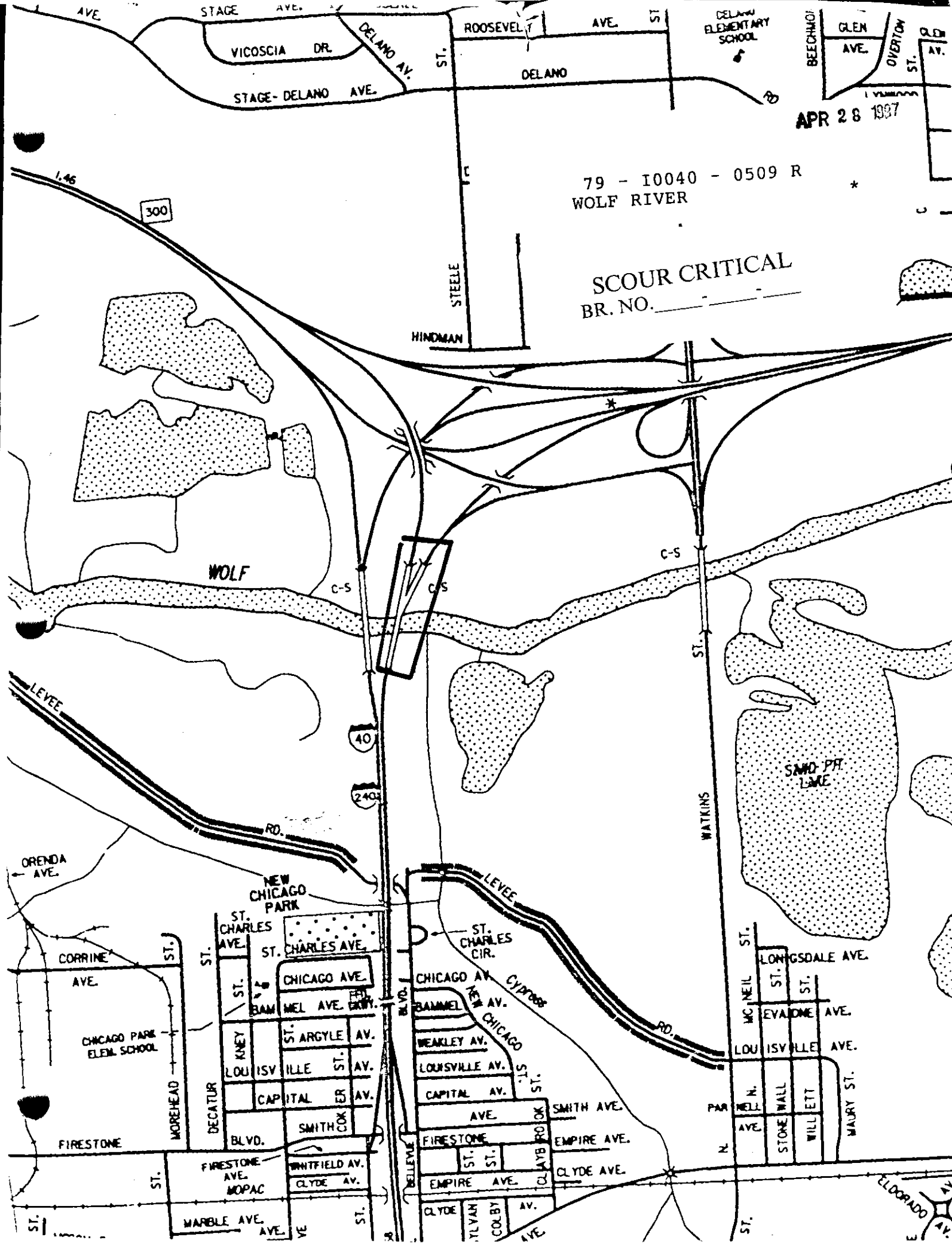
7+50

208.2
5.8
0.0
5.8
5.0
5.0
208.5
210.6
206.0
13.8
15.7
18.6
170
235
300
200.2
198.3
195.4

In Line
→ Channel
to Memphis
Public
works

42.70 216.74

B.M. See Pg. 6 Tie = -0.01



APR 28 1987

79 - 10040 - 0509 R
WOLF RIVER

SCOUR CRITICAL
BR. NO. _____

300

40

2400

SAND PT. LAKE

ORENDA AVE.

CORRINE AVE.

CHICAGO PARK ELEM. SCHOOL

FIRESTONE

ST. CHARLES AVE.

ST. CHARLES AVE.

CHICAGO AVE.

ST. ARGYLE AVE.

LOUISVILLE ST. AV.

CAPITAL ST. AV.

SMITH COX

BLVD.

WHITFIELD AV.

CLYDE AV.

MARBLE AVE.

ST. CHARLES CIR.

CHICAGO AV.

BAMMEL

WEAKLEY AV.

LOUISVILLE AV.

CAPITAL AV.

AVE.

FIRESTONE

EMPIRE AVE.

CLYDE

NEW AV.

CHICAGO

ST. ST.

SMITH AVE.

EMPIRE AVE.

CLYDE AVE.

CLYDE

CLYDE

CLYDE

MC NEIL ST.

LONGSDALE AVE.

ST.

EVADONE AVE.

LOUISVILLE AVE.

PAR

WILL

WILL

WILL

WILL

ST.

ST.

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APR 28 1997

INSPECTION TEAMS' SUMMARY

BRIDGE # 77 I40 5.09 R

✓ SCOUR CHECK SHOWS THE CHANNEL TO HAVE MOVED
100' ± TOWARD BENT #7 WITH BENT #6 NOW LOCATED IN THE
CHANNEL & THE CHANNEL HAS MOVED WITHIN 12' OF BENT #7.

BENT #5 HAS A LARGE DRIFT ON THE AL. AND.

THE SCOUR RATED POOR DUE TO THE MOVEMENT OF THE
CHANNEL

Tom Javala

* STEEL SHEET PILES IN MIDDLE OF CHANNEL UP STREAM
600' ± FROM BRG. MAY CAUSE SCOUR PROBLEM @ BR.

BRIDGE INSPECTION REPORT

APR 28 1997

FORM BIR 3.0
Rev. 6-9-92
DT-0069FIELD REPORT NO.: _____ DATE: _____
PREVIOUS REPORT NO.: _____ DATE: _____
PLANS----- YES ☒ NO ☐

4-18-97

BRIDGE NO.: 79100400059
ELEVEN DIGIT NUMBERBRIDGE LOC. NO.: 79-140-5.09
CO. RTE. L.M.I-40-Rt Lane OVER Wolf River
CO. RTE. L.M.

STRUCTURE NAME (if named)

YEAR CONSTRUCTED 1970 COUNTY Shelby MAINTENANCE DISTRICT NO. 45
(ESTIMATED OR ACTUAL)[X] []
YEAR WIDENED
ESTIMATED OR ACTUAL
[] []YEAR REHABILITATED
ESTIMATED OR ACTUAL
[] []

FEATURES

WEARING SURFACE---CONCRETE ☐ TIMBER ☐ ASPHALT ☐ (DEPTH = ____")
FLARED WIDTH-----YES ☐ NO ☐
NAVIGATIONAL CONTROL---YES ☐ NO ☐
MEDIAN TYPE-----OPEN ☐ NONE ☐ CLOSED ☐BRIDGE SKEW 60°STRUCTURE TYPE CTB
Main SpanNO. SPANS 11
Main SpanSTRUCTURE TYPE
Approach SpansNO. SPANS
Approach Spans

MAXIMUM SPAN LENGTH

TOTAL LENGTH

INSPECTORS

1. Jerrids
2. Sisk
3. Fisher
4. Stephenson
5. _____
6. _____
7. _____
8. _____
9. _____

WIDTHS

DECK OUT-TO-OUT _____
ROADWAY CURB-TO-CURB _____
SIDEWALK _____ RT _____ LT _____
*APPROACH ROADWAY _____
APPR. SHOULDER _____ RT _____ LT _____

CLEARANCES

MIN. VERTICAL OVER DECK _____
MIN. VERTICAL UNDER CL. _____
MIN. LATERAL UNDER CL. _____ RT _____ LT _____

*DOES NOT INCLUDE SHOULDERS

UNDERWATER INSPECTION

INSPECTION PERFORMED BY:
DOT FIELD TEAM ☐ DATE _____
CONTRACT DIVERS ☐ DATE _____
NONE REQUIRES ☐(< 25FT.)
NBIS BRIDGE
LENGTH _____
(FT.) (INCHES)CHANGE IN STRUCTURAL CONDITION YES ☐ NO ☒
MAJOR REPIARS MADE YES ☐ NO ☒FRACTURE CRITICAL
DETAILS: YES ☐ NO ☐
IF YES, INCLUDE BIR 3.9

COMMENTS:

BRIDGE RATING ☐ ☐ ☒ ☐
GOOD FAIR POOR CRITICAL
Kimeth Jerrids
SUPERVISING BRIDGE INSPECTOR

BRIDGE LOC. NO. 79 - I40 - 5.09 134
 CO. RTE. L.M.

DATE APR 28 1997

STREAM CHANNEL DATA AND CONDITIONS

STREAM CROSSING:

Wolf River

PIER & ABUTS PILE BENTS

I. SCOUR LOCATIONS:

	LOCATED IN CHANNEL	NOT IN CHANNEL	FAILED RIP-RAP	SOLID ROCK	COBBLE	GRAVEL	SAND	SILT	CLAY	NOT VISIBLE	SCOUR ABOVE FOOTING	FOOTING EXPOSED	PILES EXPOSED	UNDER SPREAD FOOTING	SLIGHT SCOUR (<= 1')	MODERATE SCOUR	SEVERE SCOUR (>= 1')
1. ABUT/PIER/BENT <u>3</u>	✓		X							X				X			
2. ABUT/PIER/BENT <u>4</u>	✓		X							X				X			
3. ABUT/PIER/BENT <u>5</u>	✓		X							X				X			
4. ABUT/PIER/BENT <u>6</u>	✓		X							X	✓			X			
5. ABUT/PIER/BENT <u>7</u>	✓		X							X				X			
6. ABUT/PIER/BENT			X							X				X			
7. ABUT/PIER/BENT			X							X				X			

II. HAS THE CHANNEL SHIFTED? (CHECK ANSWER)

LATERALLY

NO MOVEMENT [] NOT APPARENT []
 SLIGHT (<=1') [] MODERATE [✓]
 SEVERE (> 5') []

VERTICALLY

NO MOVEMENT [✓] NOT APPARENT []
 SLIGHT (<=1') [] MODERATE []
 SEVERE (> 5') []

III. ARE EMBANKMENT OR APPROACH SUBSTRUCTURE BEING THREATENED BY BANK FAILURE? YES [✓] NO []

IV. CHANNEL AND BANK STABILITY CONDITIONS: (CHECK IF APPLICABLE)

- (1) STEEP BANK - FAILURES UPSTREAM [] DOWNSTREAM []
 CONDITIONS
 (2) MODERATE BANK EROSION [✓]
 (3) BANK (A) LOW GROWTH [✓] (B) LARGE TIMBER [✓]
 VEGETATION (C) DEAD TREES [✓] (D) CLEAR BANKS [✓]
 (4) SEDIMENT OR GRAVEL ACCUMULATION: YES [] NO [✓] UNKNOWN []
 (5) CHANNEL ALTERED OR STRAIGHTENED: YES [] NO [✓] UNKNOWN []
 (6) STABLE CONDITIONS: (A) LIVE GROWTH [] (B) BEDROCK []
 (C) BOULDERS [] (D) FLAT SLOPES []
 (<= 2:1)

BRIDGE LOC. NO.: 79 - I40 - 509Rt.
CO. RTE. L.M.

DATE: APR 28 1997

STREAM CHANNEL DATA AND CONDITIONS (CON'T)

V. Waterway adequacy and debris characteristics: (check if applicable)

- (1) Bridge deck elevation:
(a) level with approach roadway ☒
(b) higher than approach roadway ☐
(c) roadway approach $\geq 2'$ above natural ground line . ☒
(2) Abutment encroaches into channel ☐
(3) Large scour (blowhole) under bridge ☐
(4) Indications that flood waters overtop bridge:
NO ☒ YES ☐ -> OCCASIONALLY ☐ FREQUENTLY ☐ UNKNOWN ☐
(5) Debris characteristics:
(a) debris present: YES ☒ NO ☐
(b) debris likely to accumulate: YES ☒ NO ☐
(c) dead trees upstream: ☒ or downstream ☒

VI. Condition of rip-rap? G F P C Est. % failed: _____ N/A: ☒

VII. Underwater diver inspection recommended? YES ☒ NO ☐

VIII. Overall condition of channel? G F ☒ C

IX. Item 61 - Code values 0 thru 9 according to the recording and coding guide currently in effect: 4

X. Comments: Channel moving toward Abut #2 with Bent #6 now
located in channel & channel edge within 12" of Bent #7.
Channel has moved 100' E from original position.

SPECIAL INSPECTION DATA
(FOR REASONS OTHER THAN FC OR SCOUR)

I. Does this bridge need a special inspection? YES ☐ NO ☒

II. Reason for special inspection: _____

APR 28 1997

REV. : 07/29/93

DATE : / /

PAGE NUMBER : OF

BRIDGE NUMBER : 79 - I-40 - 5.09 R
COUNTY ROUTE LOG MILE

ABUT/BENT PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO -- BOTTOM OF FOOTING (OR GROUND LINE / DATE FOR PILES)	(t) FOOTING THICKNESS	W/FTG @ H. = TOP OF CAP TO TOP OF FOOTING	EXPOSURE
A-1	4		4	
P-1	45.2'	4.25'	42.44 L 38.40 R 38.9'	
P-2	55.8'	4.5'	52.86 L 49.70 R 49.7'	
P-3	61.5'	4.5	58.10 L 55.90 R 55.9	35.9'
P-4	61.6'	3.75'	59.41 L 56.10 R 56.1	41.7'
P-5	61.6'	3.25'	60.08 L 56.70 R 56.7	44.4'
P-6	52.5'	3.25'	50.95 L 57.40 R 57.4	43.7'
P-7	53.3'	3.25'	51.78 L 49.20 R 49.2	33.2'
P-8	49.3'	3.25	47.62 L 44.58 R 44.6	
P-9	46.1'	3.25	43.00 L 42.50 R 42.5	
P-10	38.1'	3.25	35.86 L 34.66 R 34.6	
P-11				

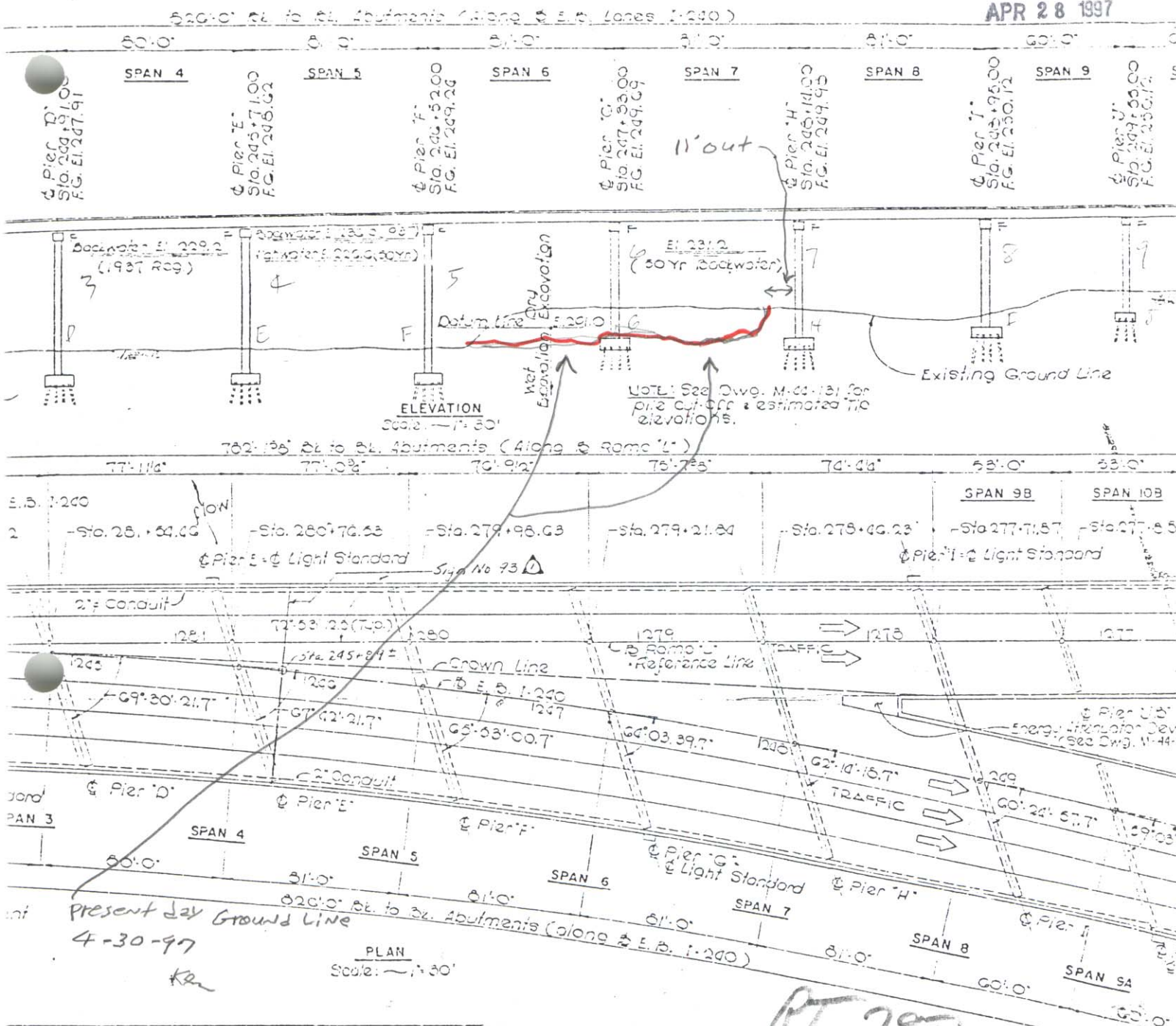
TOP OF CAP TO TOP OF WATER : 36.7' Bent #6RIP - RAP : YES [] NO : ☒
@ BENT / PIER NO. : 100.00' UPSTREAM : 43.7'THRU STRUCTURE : 47.7'100.00' DOWNSTREAM : 43.7'

COMMENTS :

the channel has moved 100' ± from original position
toward Abut #2 - Bent #6 located in channel & the channel
is within 12' of Bent #7.

79-I 40-509 Rt.

APR 28 1997

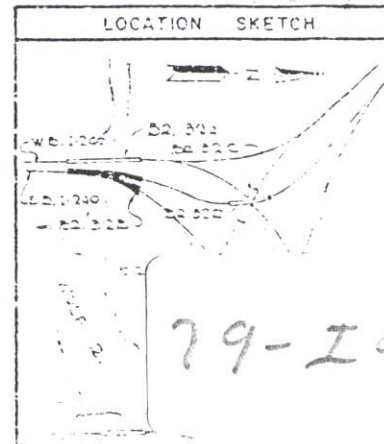


Dwg. No.	Title
M-44-122	Framing Plan - Spans 1 & 2
M-44-123	Framing Plan - Spans 3 & 4
M-44-124	Framing Plan - Spans 5 & 6
M-44-125	Framing Plan - Spans 7 & 8
M-44-126	Framing Plan - Spans 9A, 9B & 10A
M-44-127	Framing Plan - Spans 9B, 10B & 11B
M-44-128	Freshness & Beam Details
M-44-129	Sealing Details
M-44-130	Grade Elevations
M-44-131	Foundation Data
M-44-132	Light Standard Support Details
M-44-133	Structure to Pier Connections
M-44-134	Bridge Decking, Girders & Floorbeams
M-44-135	Roofs, Gutters & Drainage Details
M-44-136	Particulars of Bridge Components
M-44-137	Site and Pier Details
M-44-138	Waterway & Drainage Details
M-44-139	Notes

HYDROLOGICAL DATA
 Highwater (floodwater) Elev. 236.00 (1937 Flood)
 Highwater (floodwater) Elev. 226.00 (50 Yr)
 Mean Flood Elev. 226.00
 Discharge 66,000 CFS (50 Yr)
 Opening Required 15,000 Sq. Ft.
 Opening Provided (82.0W 225.0) 18,525 Sq. Ft.
 Opening Provided (82.0W 236.0) 21,755 Sq. Ft.

E.B. I-240 CURVE DATA

PI Sta. 260+92.67
 L 441.00' 33' (21)
 OA 21.15'
 P 1.245' 23'
 TA 11.15' 23'
 LA 22.30' 23'
 EA 31.45'
 LA 200.00'
 LA 100.00'
 LA 500.00'



BRIDGE NO. 79 I40 5109Rt

DATE APR 28 1997

TOTAL HEIGHT:
(TOP OF CAP TO BOTTOM OF FTG.)

61.5

BENT/PIER NO. 3

FOOTING THICKNESS (t):

4.5

SCOUR: ☒ F ☐ P ☐ C

DRIFT: ☒ F ☐ P ☐ C

TOP OF CAP TO TOP OF FTG.:

55.90 Rt end

MAXIMUM EXPOSURE

TOP OF CAP TO TOP OF WATER:

34.9

35.9

WATER DEPTH TO TOP OF FTG.:

DEPTH
DISTANCE

←
DIR. OF FLOW

<u>5.0'</u>	<u>4.0'</u>	<u>4.0'</u>
	40	
<u>5.0'</u>	<u>4.0'</u>	<u>5.0'</u>
	30	
<u>3.0'</u>	<u>3.0'</u>	<u>3.0'</u>
	20	
<u>1.0</u>	<u>1.0'</u>	<u>2.0'</u>
	10	
<u>0.0</u>	<u>1.0'</u>	<u>1.0'</u>
	1	

<u>1.0'</u>	<u>1.0'</u>	<u>1.0'</u>
	1	
<u>0.0'</u>	<u>0.0'</u>	<u>0.0'</u>
	10	
	20	
	30	

DATE APR 28 1997BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILEBENT/PIER NO. 4TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 61.6 ²SCOUR : Q F P C
DRIFT : Q F P CMAXIMUM EXPOSURE 41.7'FOOTING THICKNESS (t) : 3'9"(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 57.8 ²TOP OF CAP TO TOP OF WATER : 35.7WATER DEPTH TO TOP FOOTING :

<u>8</u>	<u>8</u> <u>90</u>	<u>7</u>
<u>4</u>	<u>6</u> <u>30</u>	<u>6</u>
<u>5</u>	<u>6</u> <u>20</u>	<u>6</u>
<u>4</u>	<u>5</u> <u>10</u>	<u>6</u>
<u>4</u>	<u>5</u> <u>1</u>	<u>6</u>

DIRECTION OF ROUTE ↑

30/4 20/4 10/4 1/5

4
40
4
50

6/1 5/10 5/20 6/30
6
40
6
50

<u>5</u>	<u>3</u> <u>1</u>	<u>5</u>
<u>4</u>	<u>4</u> <u>10</u>	<u>4</u>
<u>4</u>	<u>5</u> <u>20</u>	<u>4</u>
<u>5</u>	<u>5</u>	<u>4</u>
<u>5</u>	<u>30</u> <u>4</u> <u>40</u>	<u>4</u>

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW ←

DATE APR 28 1997BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILEBENT/PIER NO. 5TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 46.62SCOUR : G 1 P CDRIFT : G F 1 C see 11

MAXIMUM EXPOSURE

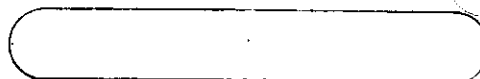
44.4'FOOTING THICKNESS (t) : 3'3"(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 584.2TOP OF CAP TO TOP OF WATER : 36.4WATER DEPTH TO TOP FOOTING :

① Large under water drift

DIRECTION OF ROUTE

<u>3</u>	<u>7</u> 40	<u>6</u>
<u>2</u>	<u>7</u> 30	<u>6</u>
<u>2</u>	<u>3</u> 20	<u>6</u>
<u>5</u>	<u>2</u> 10	<u>7</u>
<u>8</u>	<u>3</u> 1	

30/10 20/10 10/10 1/8

10
90
9
50


①

11 5/10 5/20 5/30

5
90
5
50

<u>6</u>	<u>5</u> 1	
<u>8</u>	<u>8</u> 10	<u>9</u>
<u>8</u>	<u>10</u> 20	<u>8</u>
<u>7</u>	<u>8</u> 30	<u>11</u>
<u>8</u>	<u>8</u> 40	<u>10</u>

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILE

BENT/PIER NO. 6

TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 52.5 ft

SCOUR : G F see ③DRIFT : G F see ① ②MAXIMUM EXPOSURE 43.7

FOOTING THICKNESS (t) : 3'-3"

(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 49.2 ft

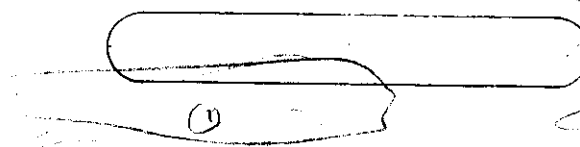
TOP OF CAP TO TOP OF WATER : 36.7

WATER DEPTH TO TOP FOOTING : 4 ft

③ Scour Rated poor due
to movement of channel

① drift, Large Log
② drift

DIRECTION OF ROUTE



30/8 20/8 10/8 1/1

8
40
8
50

3/1 3/10 3/20 3/30
2
40
0.0
50

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

4.2'

REV. 07-01-91

DATE : APR 28 1997

BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILE

BENT/PIER NO. _____

TOTAL HEIGHT : _____
(TOP OF CAP TO BOTTOM OF FOOTING)

SCOUR : G F C see D
DRIFT : G F C see C
MAXIMUM EXPOSURE : 41.7'

FOOTING THICKNESS (t) : _____

(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : _____

① channel has moved from original
Position 100'± toward Abut #2.

TOP OF CAP TO TOP OF WATER : 36.7' Bant #6

② drift @ Bent #5

WATER DEPTH TO TOP FOOTING : _____

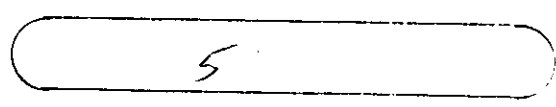
$\frac{7'}{3/4}$



edge water ↑

$\frac{4'}{3/4}$

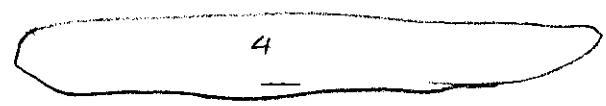
$\frac{3'}{3/4}$



$\frac{6'}{1/2}$

$\frac{7}{2}$

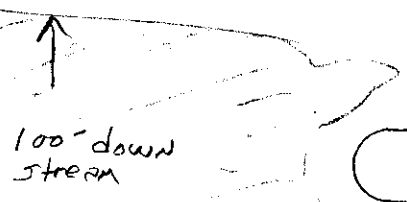
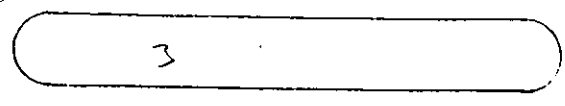
$\frac{8'}{1/2}$



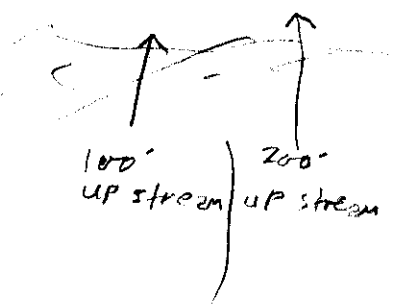
$\frac{5}{1/4}$

$\frac{6'}{1/4}$

$\frac{10'}{1/4}$



DEPTH
DISTANCE

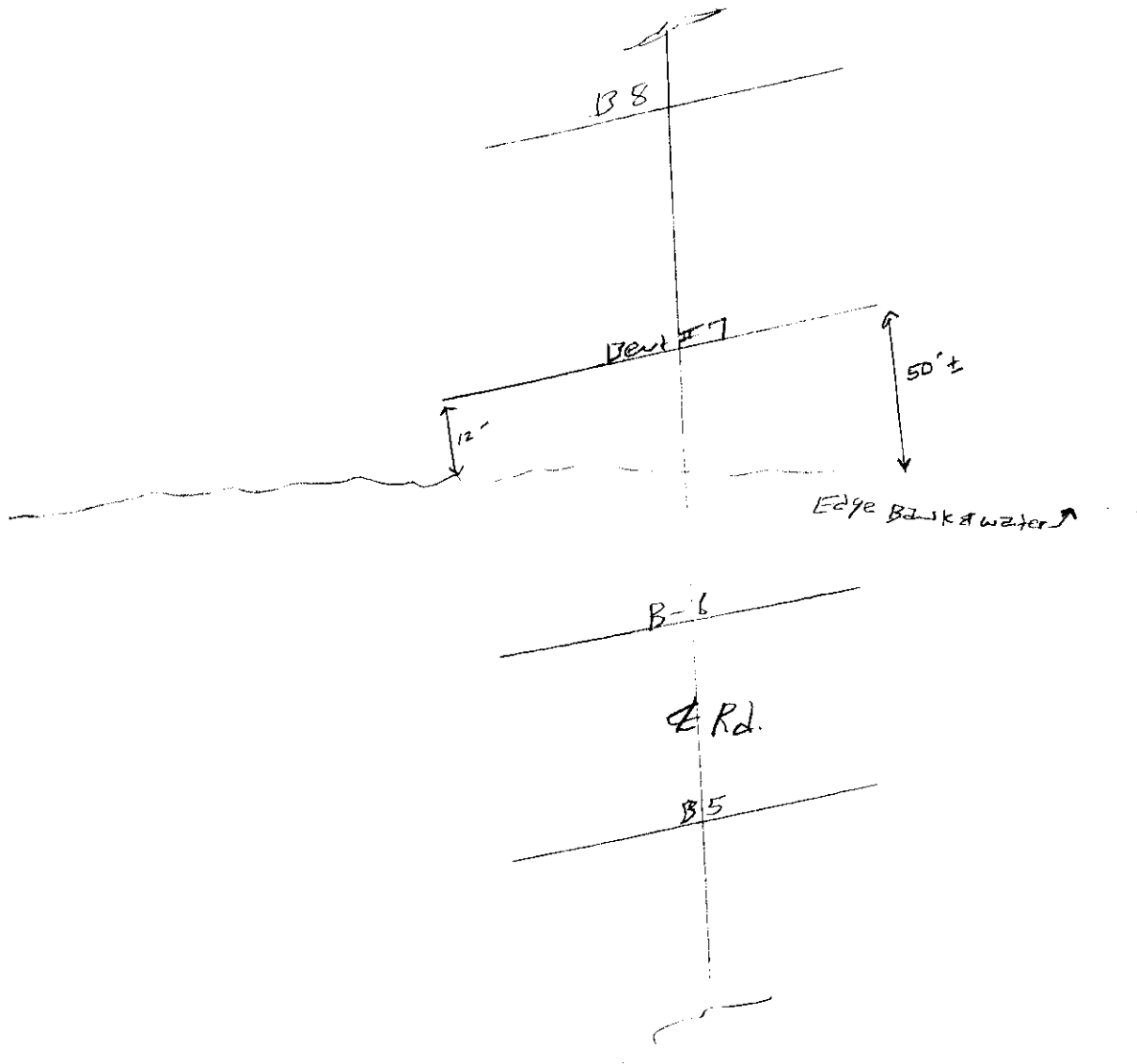


SHOW DIRECTION OF FLOW ←

DIRECTION OF ROUTE ↑

79-F46-5109R

APR 28 1951



BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILEBENT/PIER NO. 7TOTAL HEIGHT : 53.3
(TOP OF CAP TO BOTTOM OF FOOTING)

SCOUR : G F P C

DRIFT : G F P C

MAXIMUM EXPOSURE 33.2FOOTING THICKNESS (t) : 3'3"(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 50.0TOP OF CAP TO TOP OF WATER : Not in waterWATER DEPTH TO TOP FOOTING :

DIRECTION OF ROUTE

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

BRIDGE NO. 79 I-40 5.09 R
 COUNTY ROUTE LOG MILE

BENT/PIER NO. 8

TOTAL HEIGHT : 49.3
 (TOP OF CAP TO BOTTOM OF FOOTING)

SCOUR : G F P C

DRIFT : G F P C

MAXIMUM EXPOSURE _____

FOOTING THICKNESS (t) : 3'-3"

(TOP OF CAP TO TOP OF FOOTING)
 W/FTG @ H = : 46.1

TOP OF CAP TO TOP OF WATER : _____

WATER DEPTH TO TOP FOOTING : _____

DIRECTION OF ROUTE

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW



RECEIVED

AUG 29 1996

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

P. O. BOX 429
JACKSON, TENNESSEE 38302-0429

D.O.T.
BRIDGE INSPECTION & REPAIR

August 27, 1996

M E M O R A N D U M

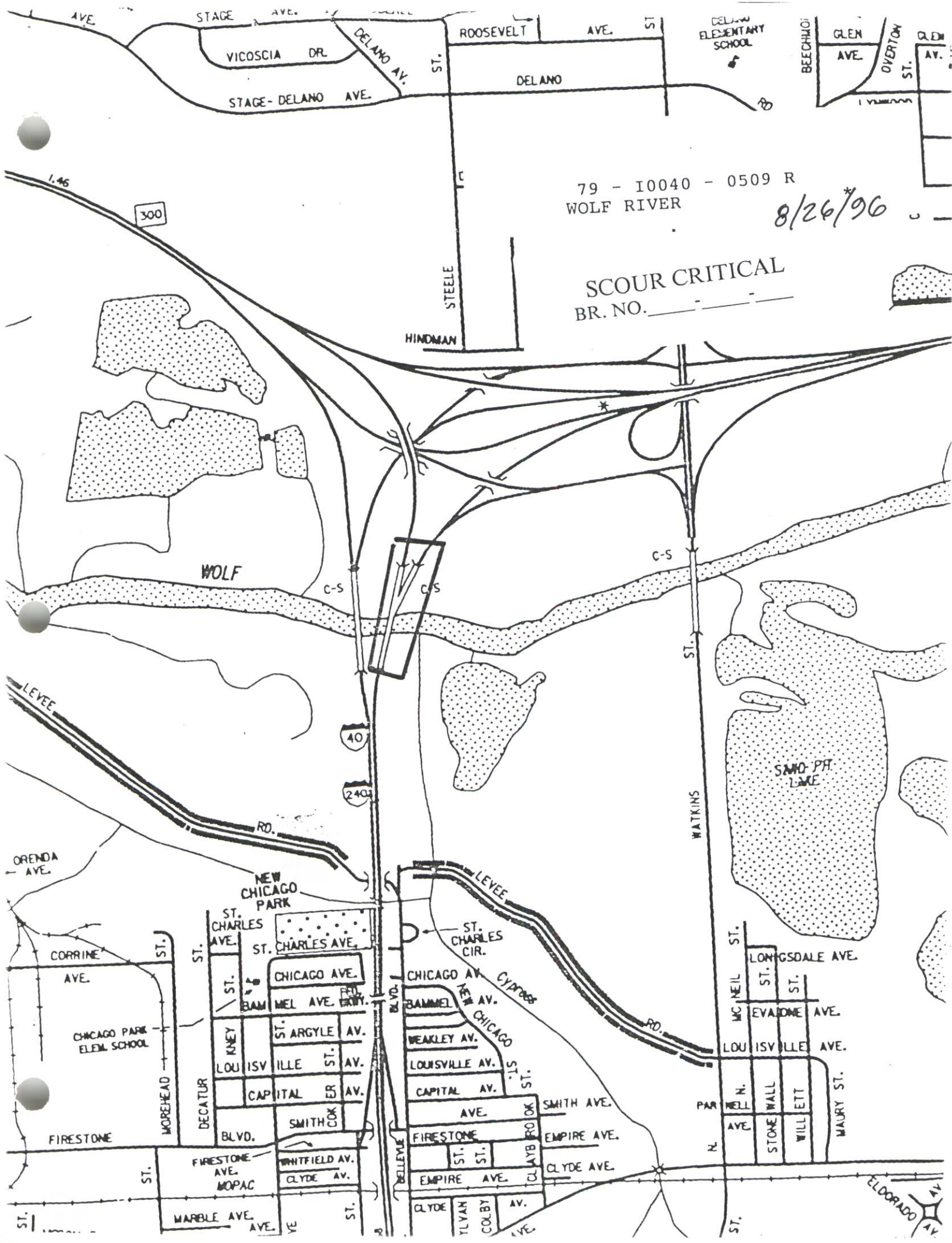
TO: Mr. Paul Sharp, Civil Engineering Manager I
FROM: *Phillip Hardin* Mr. Bill Hazlerig, Regional Bridge Engineer

SUBJECT: Scour Information (Bridge #79 - I0040- 05.09R)

Please find enclosed Scour Information you requested on the above referenced bridge.

BWH: cw

cc: File



79 - 10040 - 0509 R
WOLF RIVER

8/26/96

SCOUR CRITICAL
BR. NO. _____

WOLF

SAND PT. LAKE

NEW CHICAGO PARK

CHICAGO PARK ELEM. SCHOOL

DELANO ELEMENTARY SCHOOL

RECEIVED

INSPECTION TEAMS' SUMMARY

AUG 29 1996

BRIDGE # 79 140 509R
8/26/96

D.O.T.
BRIDGE INSPECTION & REPAIR

This inspection is for a
scour check on piers 4, 5
6 and 7.

The major problem is
at pier #7 with the
embankment encroaching into
the pier. There is no major
change since the last inspection.

The scour is still poor
with no repairs having been
made.

Ken Sil

BRIDGE INSPECTION REPORT

FORM BIR 3.0
Rev. 6-9-92
DT-0069

FIELD REPORT NO.: 8-2676 DATE: 8-26-96
PREVIOUS REPORT NO.: 8-2676 DATE: 8-26-96
PLANS-----YES ☒ NO ☐

BRIDGE NO.: 79100400059
ELEVEN DIGIT NUMBER

BRIDGE LOG. NO.: 79-140-5.05
CO. 79 RTE. 140 L.M. 5.05

I-40 Rt Lane OVER Wolf River
CO. RTE. L.M.

STRUCTURE NAME (if named)

YEAR CONSTRUCTED 1970 COUNTY shelby MAINTENANCE DISTRICT NO. 45
(ESTIMATED OR ACTUAL) ☒ []

YEAR WIDENED
ESTIMATED OR ACTUAL ☐ []

YEAR REHABILITATED
ESTIMATED OR ACTUAL ☐ []

FEATURES

WEARING SURFACE---CONCRETE ☐ TIMBER ☐ ASPHALT ☐ (DEPTH = ")
FLARED WIDTH-----YES ☐ NO ☐
NAVIGATIONAL CONTROL---YES ☐ NO ☐
MEDIAN TYPE-----OPEN ☐ NONE ☐ CLOSED ☐

BRIDGE SKEW 60° Rt

STRUCTURE TYPE CIB
Main Span

NO. SPANS 11
Main Span

STRUCTURE TYPE
Approach Spans

NO. SPANS
Approach Spans

MAXIMUM SPAN LENGTH

TOTAL LENGTH

WIDTHS

DECK OUT-TO-OUT
ROADWAY CURB-TO-CURB
SIDEWALK RT LT
*APPROACH ROADWAY
APPR. SHOULDER RT LT

CLEARANCES

MIN. VERTICAL OVER DECK
MIN. VERTICAL UNDER CL.
MIN. LATERAL UNDER CL. RT LT

*DOES NOT INCLUDE SHOULDERS

INSPECTORS

1. Tennold
2. Sick
3. Coxin
4. Fisher
5. Stephenson
6.
7.
8.
9.

UNDERWATER INSPECTION

INSPECTION PERFORMED BY:
DOT FIELD TEAM ☐ DATE
CONTRACT DIVERS ☐ DATE
NONE REQUIRES ☐

(< 25FT.)
NBIS BRIDGE
LENGTH (FT.) (INCHES)

CHANGE IN STRUCTURAL CONDITION YES ☐ NO ☒
MAJOR REPIARS MADE YES ☐ NO ☒

FRACTURE CRITICAL
DETAILS: YES ☐ NO ☐
IF YES, INCLUDE BIR 3.9

COMMENTS:

Scan check only

BRIDGE RATING ☐ ☐ ☒ ☐

GOOD FAIR POOR CRITICAL
Kenneth Tennold
SUPERVISING BRIDGE INSPECTOR

BRIDGE LOC. NO. 79 - 10040-0509 R
 CO. RTE. L.M.

DATE 8-26-96

STREAM CHANNEL DATA AND CONDITIONS

STREAM CROSSING:

PIER & ABUTS PIER BEN

I. SCOUR LOCATIONS:

	LOCATED IN CHANNEL	NOT IN CHANNEL	FAILED RIP-RAP	SOLID ROCK	COBBLE	GRAVEL	SAND	SILT	CLAY	NOT VISIBLE	SCOUR ABOVE FOOTING	FOOTING EXPOSED	PILES EXPOSED	UNDER SPREAD FOOTING	SLIGHT SCOUR (<= 1')	MODERATE SCOUR (<= 1')	SEVERE SCOUR (<= 1')
1. ABUT/PIER/BENT <u>5/4</u>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. ABUT/PIER/BENT <u>7</u>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. ABUT/PIER/BENT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. ABUT/PIER/BENT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. ABUT/PIER/BENT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. ABUT/PIER/BENT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. ABUT/PIER/BENT	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

II. HAS THE CHANNEL SHIFTED? (CHECK ANSWER)

LATERALLY

NO MOVEMENT ☐ NOT APPARENT ☐
 SLIGHT (<= 1') ☐ MODERATE ☐
 SEVERE (> 5') ☒

VERTICALLY

NO MOVEMENT ☐ NOT APPARENT ☐
 SLIGHT (<= 1') ☐ MODERATE ☐
 SEVERE (> 5') ☒

III. ARE EMBANKMENT OR APPROACH SUBSTRUCTURE BEING THREATENED BY BANK FAILURE? YES ☒ NO ☐

IV. CHANNEL AND BANK STABILITY CONDITIONS: (CHECK IF APPLICABLE)

- (1) STEEP BANK - FAILURES UPSTREAM ☐ DOWNSTREAM ☐
 CONDITIONS
 (2) MODERATE BANK EROSION ☒
 (3) BANK (A) LOW GROWTH ☒ (B) LARGE TIMBER ☒
 VEGETATION (C) DEAD TREES ☐ (D) CLEAR BANKS ☐
 (4) SEDIMENT OR GRAVEL ACCUMULATION: YES ☐ NO ☒ UNKNOWN ☐
 (5) CHANNEL ALTERED OR STRAIGHTENED: YES ☐ NO ☒ UNKNOWN ☐
 (6) STABLE CONDITIONS: (A) LIVE GROWTH ☒ (B) BEDROCK ☐
 (C) BOULDERS ☐ (D) FLAT SLOPES ☐ (< = 2:1)

BRIDGE LOC. NO.: 79 - 10040-0509R
CO. RTE. L.M.

DATE: 8-26-96

STREAM CHANNEL DATA AND CONDITIONS (CON'T)

V. Waterway adequacy and debris characteristics: (check if applicable)

- (1) Bridge deck elevation:
(a) level with approach roadway ☒
(b) higher than approach roadway ☐
(c) roadway approach $\geq 2'$ above natural ground line ☒
(2) Abutment encroaches into channel ☐
(3) Large scour (blowhole) under bridge ☐
(4) Indications that flood waters overtop bridge:
NO ☒ YES ☐ - OCCASIONALLY ☐ FREQUENTLY ☐ UNKNOWN ☐
(5) Debris characteristics:
(a) debris present: YES ☒ NO ☐
(b) debris likely to accumulate: YES ☒ NO ☐
(c) down trees upstream: ☐ or downstream ☐

VI. Condition of rip-rap: G ☒ P ☐ C ☐ Est. % failed: N/A: ☐

VII. Underwater diver inspection recommended? YES ☒ NO ☐

VIII. Overall condition of channel? G ☒ P ☐ C ☐

IX. Item 61 - Code values 0 thru 9 according to the recording and coding guide currently in effect: 3

X. Comments: Beet # 5 & 6 have been drilled
Beet # 4, 5 & 6 located in channel
Beet # 7 located on edge of channel

SPECIAL INSPECTION DATA
(FOR REASONS OTHER THAN FC OR SCOUR)

- I. Does this bridge need a special inspection? YES ☐ NO ☒
II. Reason for special inspection: _____

REV. : 07/29/93

DATE : 8 / 26 / 86PAGE NUMBER : OF BRIDGE NUMBER : 77 - I-40 - 5.09R
COUNTY ROUTE LOG MILE

ABUT/BENT PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO -- BOTTOM OF FOOTING (OR GROUND LINE / DATE FOR PILES)	(t) FOOTING THICKNESS	W/FTG @ H. = TOP OF CAP TO TOP OF FOOTING	EXPOSURE Rt. end Bent
A-1	4		4	
P-1	45.2'	4.25'	47.442 58.00R 38.9'	
P-2	55.8'	4.5'	58.062 58.00R 49.7'	
P-3	61.5'	4.5'	58.004 58.00R 55.0'	
P-4	61.6'	3.75'	58.012 58.00R 56.1'	42.4'
P-5	61.6'	3.25'	58.082 58.00R 55.7'	46.2'
P-6	58.5'	3.25'	58.082 58.00R 57.0'	44.0'
P-7	53.3'	3.25'	58.752 58.00R 57.0'	
P-8	49.3'	3.25'	47.824 47.85R 57.0'	
P-9	46.0'	3.25'	47.824 47.85R 57.0'	
P-10	38.1'	3.25'	35.932 35.96R 35.9'	
P-11				

Bent II 6
TOP OF CAP TO TOP OF WATER : 43.0'100.00' UPSTREAM : 45.6'THRU STRUCTURE : 47.8'100.00' DOWNSTREAM : 45.0'

COMMENTS :

RIP - RAP : YES [] NO : []
@ BENT / PIER NO. :Unable to top due to
drift & sand bar

DATE 8/26/96

BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILE

BENT/PIER NO. 4

TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 61.6

SCOUR : ~~G~~ F P C
 DRIFT : G F ~~P~~ C → see ①

FOOTING THICKNESS (t) : 3' 2"

MAXIMUM EXPOSURE 42.4
Corbin

(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 57.8 L

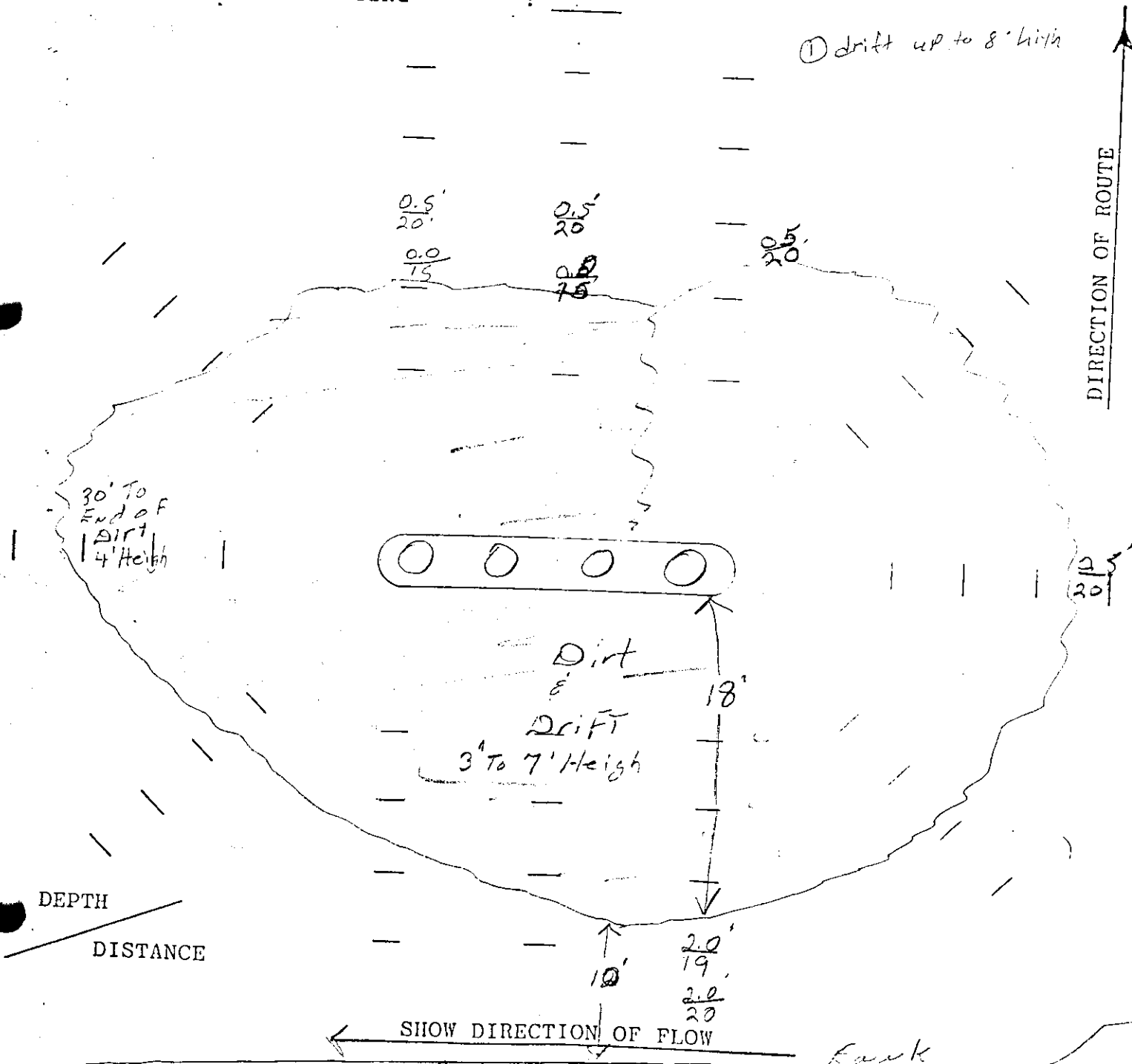
Corbin
Ken Sisk

TOP OF CAP TO TOP OF WATER : 41.9

WATER DEPTH TO TOP FOOTING : _____

① drift up to 8' high

DIRECTION OF ROUTE



DATE 8/26/96BRIDGE NO. 79 COUNTY I-40 ROUTE 5.09 R LOG MILEBENT/PIER NO. 5TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 6.16SCOUR : G (E) P C
DRIFT : G F (E) C 50.00FOOTING THICKNESS (t) : 3.3MAXIMUM EXPOSURE 46.2(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H =: 584.7

Corbin - note

Sisk - motor

Tennolds - Red

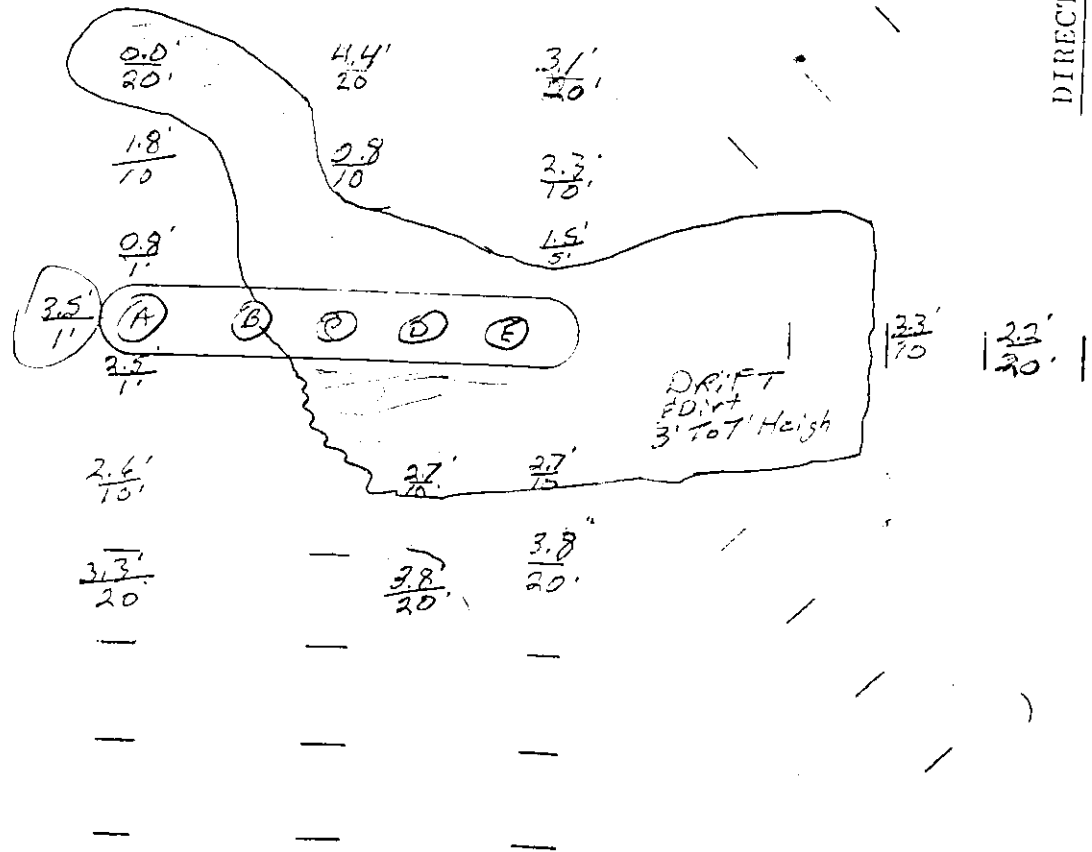
TOP OF CAP TO TOP OF WATER

: 42.7

WATER DEPTH TO TOP FOOTING

: ① = drift up to 10'
high

DIRECTION OF ROUTE



DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

DATE 8/26/96

BRIDGE NO. 79 COUNTY I-40 ROUTE 5.09 R LOG MILE

BENT/PIER NO. 6

TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 52.5 ft

SCOUR : G F ~~B~~ C
 DRIFT : G F ~~B~~ C *see D*

FOOTING THICKNESS (t) : 3'-3"

MAXIMUM EXPOSURE 47.0

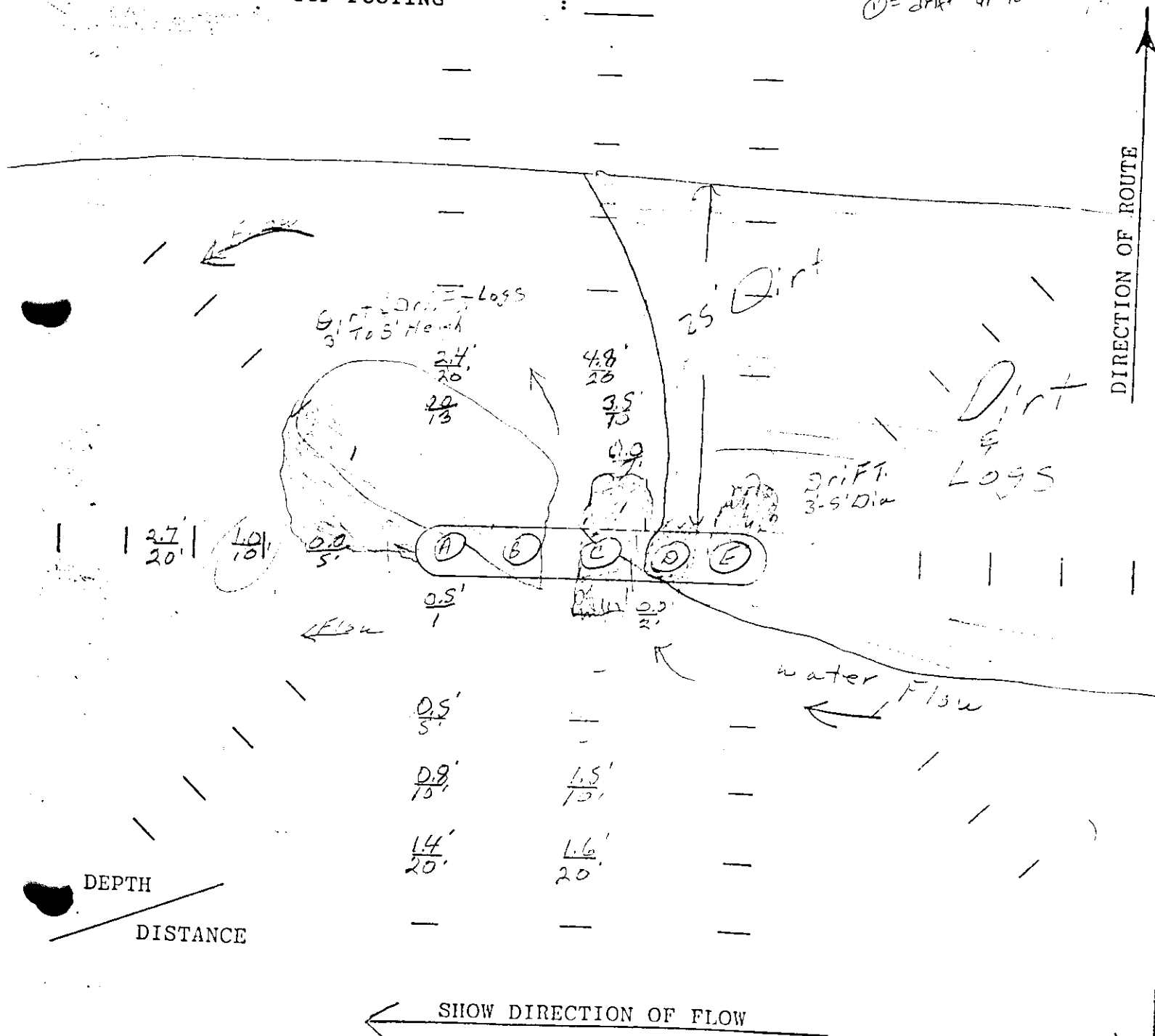
(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 49.2 \angle

Corbin - note
Sisk - Motor
Terrold - Rod

TOP OF CAP TO TOP OF WATER : 43.2'

WATER DEPTH TO TOP FOOTING : _____

① = drift up to 5° high

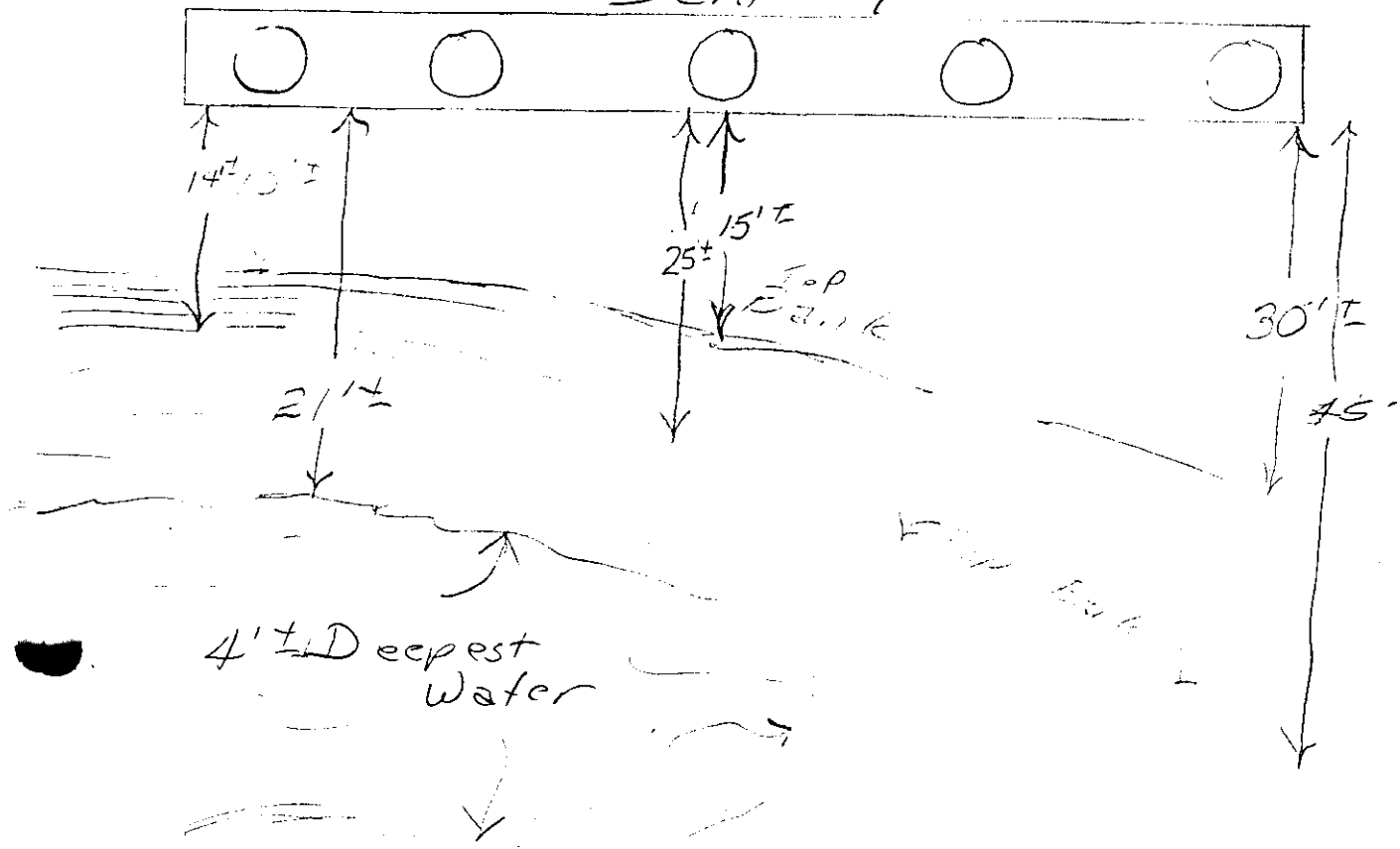


Br. No. 79 I 40 5.09 R

8-26-96

K.R.S.

Bent 7



Note: 12.5 ± From Water
to Top Bank

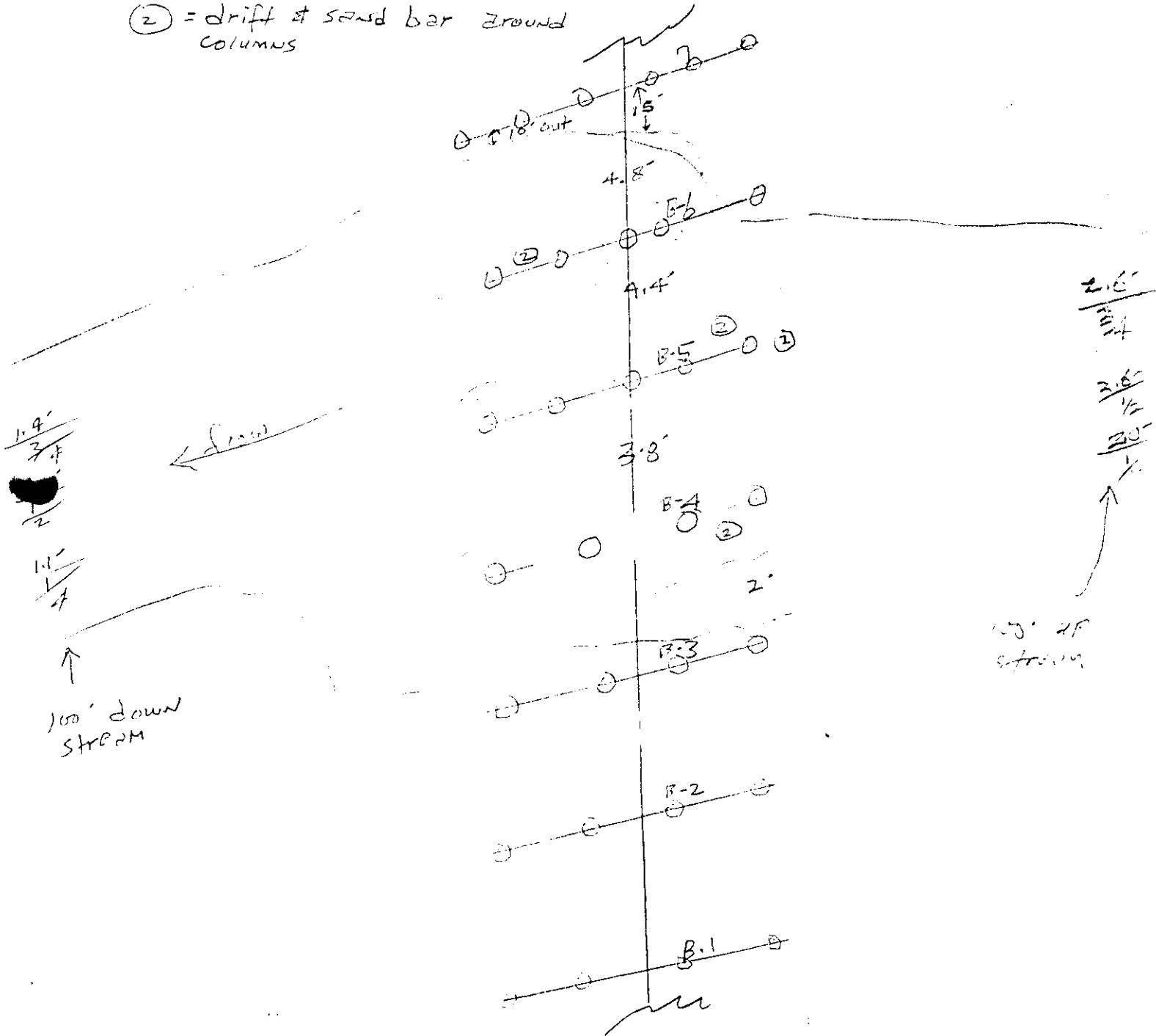
34.2 from top cap
to ground Line

79-140-5.09R

8/26/96

15N

(2) = drift & sand bar ground columns



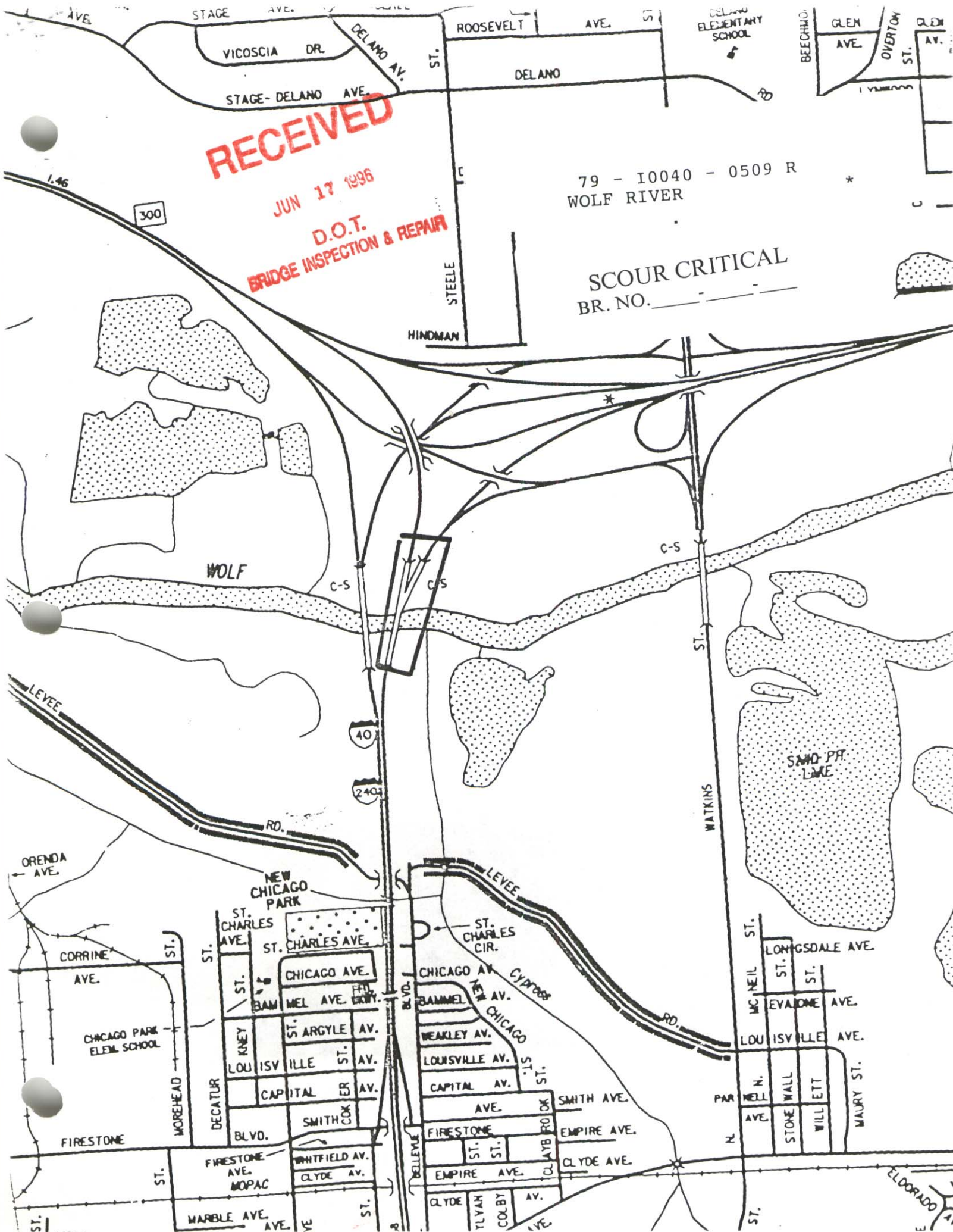
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JUN 17 1996

D.O.T.
BRIDGE INSPECTION & REPAIR

79 - 10040 - 0509 R
WOLF RIVER

SCOUR CRITICAL
BR. NO. _____



RECEIVED

103.0
6-9-92

JUN 17 1996

INSPECTION TEAMS' SUMMARY

BRIDGE #

D.O.T.
BRIDGE INSPECTION & REPAIR

BRIDGE # 79 F 40 5.09 R.

This scout check of Bents 4-7 made during flood stage of Wolf River shows very little change in the basic channel ground line, vertically. The channel Banks are hard to define due to high water (25') but they appear close to being the same as when the scout was checked in Aug. of 95, at which time the channel had moved north $170^{\circ}(\pm)$ from original position & moving into Bent #7.

This is a scout critical Bridge with the scout rated Poor this date (6-13-96)

Kenneth J. Smith

RECEIVED

BRIDGE INSPECTION REPORT

FORM BIR 3.0
Rev. 6-9-92
DT-0069

JUN 17 1996

6-13-96
FIELD REPORT NO.: _____ DATE: _____
PREVIOUS REPORT NO.: _____ DATE: _____
PLANS----- YES ☒ NO ☐

D.O.T.
BRIDGE NO.: 708 INSPECTION & REPAIR
ELEVEN DIGIT NUMBER

BRIDGE LOC. NO.: 79 - I40 - 5.09
CO. RTE. L.M.

I-40 Rt Lane OVER Wolf River
CO. RTE. L.M.

STRUCTURE NAME (if named)

YEAR CONSTRUCTED 1970 COUNTY shelby MAINTENANCE DISTRICT NO. 45
(ESTIMATED OR ACTUAL)

[X] []
YEAR WIDENED
ESTIMATED OR ACTUAL
[] []

YEAR REHABILITATED
ESTIMATED OR ACTUAL
[] []

FEATURES

WEARING SURFACE---CONCRETE [] TIMBER [] ASPHALT [] (DEPTH = ____")
FLARED WIDTH-----YES [] NO []
NAVIGATIONAL CONTROL---YES [] NO []
MEDIAN TYPE-----OPEN [] NONE [] CLOSED []

BRIDGE SKEW 60° R+

STRUCTURE TYPE CIB
Main Span

NO. SPANS 11
Main Span

STRUCTURE TYPE
Approach Spans

NO. SPANS
Approach Spans

MAXIMUM SPAN LENGTH

TOTAL LENGTH

INSPECTORS

1. Jerrids
2. Sisk
3. Lane
4. Fisher
5. Stephanson
6. _____
7. _____
8. _____
9. _____

WIDTHS

DECK OUT-TO-OUT _____
ROADWAY CURB-TO-CURB _____
SIDEWALK _____ RT _____ LT _____
*APPROACH ROADWAY _____
APPR. SHOULDER _____ RT _____ LT _____

CLEARANCES

MIN. VERTICAL OVER DECK _____
MIN. VERTICAL UNDER CL. _____
MIN. LATERAL UNDER CL. _____ RT _____ LT _____

*DOES NOT INCLUDE SHOULDERS

UNDERWATER INSPECTION

INSPECTION PERFORMED BY:
DOT FIELD TEAM [] DATE _____
CONTRACT DIVERS [] DATE _____
NONE REQUIRES []

(< 25FT.)
NBIS BRIDGE
LENGTH _____
(FT.) (INCHES)

CHANGE IN STRUCTURAL CONDITION YES [] NO []
MAJOR REPIARS MADE YES [] NO []

FRACTURE CRITICAL
DETAILS: YES [] NO []
IF YES, INCLUDE BIR 3.9

COMMENTS:

BRIDGE RATING [] [] [] []
GOOD FAIR POOR CRITICAL

SUPERVISING BRIDGE INSPECTOR

RECEIVED

REV. : 07/29/93

JUN 17 1996

DATE : ____ / ____ / ____

PAGE NUMBER : ____ OF ____

D.O.T.

BRIDGE NUMBER : 17 - F40 - 5.0912

COUNTY

ROUTE

LOG MILE

ABUT/BENT PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO BOTTOM OF FOOTING (OF GROUND LINE, DATE FOR FILES)	(t) FOOTING THICKNESS	W/FTG @ H = TOP OF CAP TO TOP OF FOOTING	EXPOSURE
-----------------------------	---	-----------------------------	--	----------

Abut 1				
Bent 1				
2				
3				
4	61.6'	3.75'	56.40' Rt. end	41.1'
5	61.6'	3.25'	56.70' Rt. end	49.5'
6	52.5'	3.25'	57.40' Rt. end	44.8'
7	53.3'	3.25'	49.20' Rt. end	36.5'
8				
9				
10				
Abut 2				

TOP OF CAP TO TOP OF WATER : 22.1' Rt. end Bent #9100.00' UPSTREAM : 42.1'THRU STRUCTURE : 46.1'100.00' DOWNSTREAM : 42.1'

COMMENTS :

RIP - RAP : YES [] NO : []
@ BENT / PIER NO. : ____*R.R. not vis. due to High
water.

RECEIVED

RECEIVED

JUN 17 1996

JUN 17 1996

REV. 07-01-91

D.O.T.
BRIDGE INSPECTION & REPAIR

D.O.T.
DATE
BRIDGE INSPECTION & REPAIR

BRIDGE NO. 79
COUNTY I40 ROUTE 509 LOG MILE 17

BENT/PIER NO. 4-7

TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : _____

SCOUR : G F P C

DRIFT : G F P C

MAXIMUM EXPOSURE : _____

FOOTING THICKNESS (t) : _____

(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : _____

see individual scour sheets

TOP OF CAP TO TOP OF WATER : 22.1' Above Bent #4

WATER DEPTH TO TOP FOOTING : _____

← 100' ±

→ 100'

Bent # 7

$\frac{19}{3/4}$

$\frac{18}{3/4}$

Bent # 6

$\frac{20}{1/2}$

$\frac{20}{1/2}$

Bent # 5

$\frac{14}{1/4}$

Bent # 4

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

$\frac{15}{1/4}$

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REV. 07-01-91

JUN 17 1996

DATE _____

BRIDGE NO. 79 COUNTY I-40 ROUTE 529.2 LOG MILE 529.2

D.O.T.

BRIDGE INSPECTION & REPAIR

BENT/PIER NO. 4

TOTAL HEIGHT (TOP OF CAP TO BOTTOM OF FOOTING) : 61.6

SCOUR : G F P C
DRIFT : G F P C

FOOTING THICKNESS (t) : 3'7"

MAXIMUM EXPOSURE 46.1

(TOP OF CAP TO TOP OF FOOTING) W/FTG @ H = : 57.8

A.S

TOP OF CAP TO TOP OF WATER : 22.1

L.L

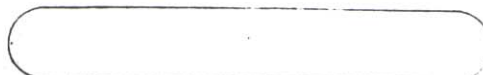
WATER DEPTH TO TOP FOOTING : 35.7

R.F

—	<u>19.0</u>	—
—	n.d	—
—	—	—
<u>18</u>	<u>18</u>	<u>22</u>
<u>20</u>	<u>20</u>	<u>20</u>
<u>14</u>	<u>15</u>	<u>20</u>
<u>10</u>	<u>10</u>	<u>10</u>
<u>17</u>	<u>16</u>	<u>16</u>
<u>1</u>	<u>1</u>	<u>1</u>

DIRECTION OF ROUTE

1- 18/20 | 18/10 | 19/1



14/1 | 13/10 | 18/20 —

<u>19</u>	<u>17</u>	<u>14</u>
<u>1</u>	<u>1</u>	<u>1</u>
<u>19</u>	<u>18</u>	<u>17</u>
<u>10</u>	<u>10</u>	<u>10</u>
<u>19</u>	<u>19</u>	<u>20</u>
<u>20</u>	<u>20</u>	<u>20</u>

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW

BRIDGE NO. 79 I-40 5.09 R
COUNTY ROUTE LOG MILE

BENT/PIER NO. 5

TOTAL HEIGHT : 61.62
(TOP OF CAP TO BOTTOM OF FOOTING)

SCOUR : G F P C
DRIFT : G F P C

FOOTING THICKNESS (t) : 3'3"

MAXIMUM EXPOSURE 49.5

(TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 524

TOP OF CAP TO TOP OF WATER : 22.9

WATER DEPTH TO TOP FOOTING : 35.5

RECEIVED

JUN 17 1996

D.O.T.
BRIDGE INSPECTION & REPAIR

—	23.0 mid	—
—	—	—
$\frac{20}{20}$	$\frac{19}{20}$	$\frac{22}{20}$
$\frac{22}{10}$	$\frac{18}{10}$	$\frac{23}{10}$
$\frac{20}{1}$	$\frac{15}{1}$	$\frac{20}{1}$

DIRECTION OF ROUTE ↑

$\frac{23}{20}$ | $\frac{23}{10}$ | $\frac{24}{1}$

$\frac{23}{1}$ | $\frac{21}{10}$ | $\frac{20}{20}$

$\frac{24}{1}$	$\frac{22}{1}$	$\frac{23}{1}$
$\frac{23}{20}$	$\frac{22}{20}$	$\frac{22}{10}$
$\frac{23}{20}$	$\frac{23}{20}$	$\frac{20}{20}$

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW ←

DATE _____

 BRIDGE NO. 79 COUNTY I-40 ROUTE 5.09R LOG MILE
BENT/PIER NO. 6
 TOTAL HEIGHT
 (TOP OF CAP TO BOTTOM OF FOOTING) : 52.5 L

SCOUR : G F P C

DRIFT : G F P C

 FOOTING THICKNESS (t) : 3'-3"

 MAXIMUM EXPOSURE 44.8
RECEIVED

 (TOP OF CAP TO TOP OF FOOTING)
 W/FTG @ H = : 49.2 L

JUN 17 1996

 TOP OF CAP TO TOP OF WATER : 24.4

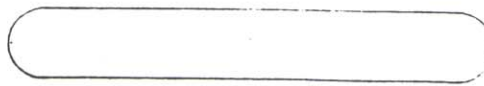
D.O.T.

 WATER DEPTH TO TOP FOOTING : 24.8

BRIDGE INSPECTION & REPAIR

—	<u>20</u> Mid	—
—	—	—
$\frac{18}{20}$	$\frac{20}{20}$	$\frac{12}{20}$
$\frac{17}{10}$	$\frac{20}{10}$	$\frac{14}{10}$
$\frac{19}{1}$	$\frac{20}{1}$	$\frac{18}{1}$

DIRECTION OF ROUTE ↑

 $\frac{24}{20} \mid \frac{20}{10} \mid \frac{19}{1}$

 $\frac{18}{1} \mid \frac{18}{10} \mid \frac{18}{20}$

<u>20</u> 1	$\frac{19}{1}$	$\frac{18}{1}$
$\frac{20}{10}$	$\frac{19}{10}$	$\frac{18}{10}$
$\frac{21}{20}$	$\frac{20}{20}$	$\frac{18}{20}$

DEPTH

DISTANCE

SHOW DIRECTION OF FLOW ←

BRIDGE NO. 79 COUNTY I-40 ROUTE 5.09 R LOG MILE
BENT/PIER NO. 7
 TOTAL HEIGHT
(TOP OF CAP TO BOTTOM OF FOOTING) : 53.3

 SCOUR : G F P C
DRIFT : G F P C

 FOOTING THICKNESS (t) : 3'3"

 MAXIMUM EXPOSURE 36.5
RECEIVED

 (TOP OF CAP TO TOP OF FOOTING)
W/FTG @ H = : 50.0

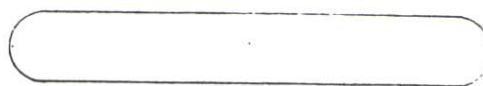
JUN 17 1996

 TOP OF CAP TO TOP OF WATER : 22.5 30.7

 WATER DEPTH TO TOP FOOTING : 27.5
D.O.T.
BRIDGE INSPECTION & REPAIR

—	<u>8.0</u>	—
—	<u>mid</u>	—
—	—	—
$\frac{8}{20}$	$\frac{8}{20}$	$\frac{8}{20}$
$\frac{7}{10}$	$\frac{9}{10}$	$\frac{9}{10}$
$\frac{7}{1}$	$\frac{8}{1}$	$\frac{9}{1}$

DIRECTION OF ROUTE ↑

 $\frac{6}{20}$ | $\frac{6}{10}$ | $\frac{6}{1}$

 $\frac{9}{1}$ | $\frac{9}{10}$ | $\frac{9}{20}$

DEPTH

DISTANCE

$\frac{6}{1}$	$\frac{6}{1}$	$\frac{3}{1}$
$\frac{7}{10}$	$\frac{7}{10}$	$\frac{8}{10}$
$\frac{18}{20}$	$\frac{15}{20}$	$\frac{8}{20}$

← near bank

SHOW DIRECTION OF FLOW ←

08 23 1995

REV. : 07/29/93

DATE : / /

PAGE NUMBER : OF

BRIDGE NUMBER : 79 - I-40 - 5.09R
COUNTY ROUTE LOG MILE

ABUT/BENT PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO - BOTTOM OF FOOTING (OR GROUND LINE / DATE FOR PILES)	(t) FOOTING THICKNESS	W/FTG @ H. = TOP OF CAP TO TOP OF FOOTING	EXPOSURE
A-1	4		4	
P-1	45.2'	4.25'	42.44 L 38.90 R 38.9'	26.5'
P-2	55.8'	4.5'	52.86 L 49.70 R 49.7'	31.5'
P-3	61.5'	4.5	58.10 L 55.90 R 55.9'	36.0'
P-4	61.6'	3.75'	59.41 L 56.10 R 56.1'	45.5'
P-5	61.6'	3.25'	60.08 L 56.70 R 56.7'	48.0'
P-6	AVG PILE 31' 52.5'	3.25'	50.95 L 57.40 R 57.4'	47.3'
P-7	AVG. PILE 35' 53.3'	3.25'	51.78 L 49.20 R 49.2'	34.3'
P-8	49.3'	3.25	47.62 L 44.55 R 44.55	34.0'
P-9	46.1'	3.25	43.50 L 42.50 R 42.5	29.0'
P-10	38.1'	3.25	35.86 L 34.66 R 34.66	16.5'
A-2				

TOP OF CAP TO TOP OF WATER : 43.5'

100.00' UPSTREAM : 46.0'

THRU STRUCTURE : 48.5'

100.00' DOWNSTREAM : 46.0'

COMMENTS :

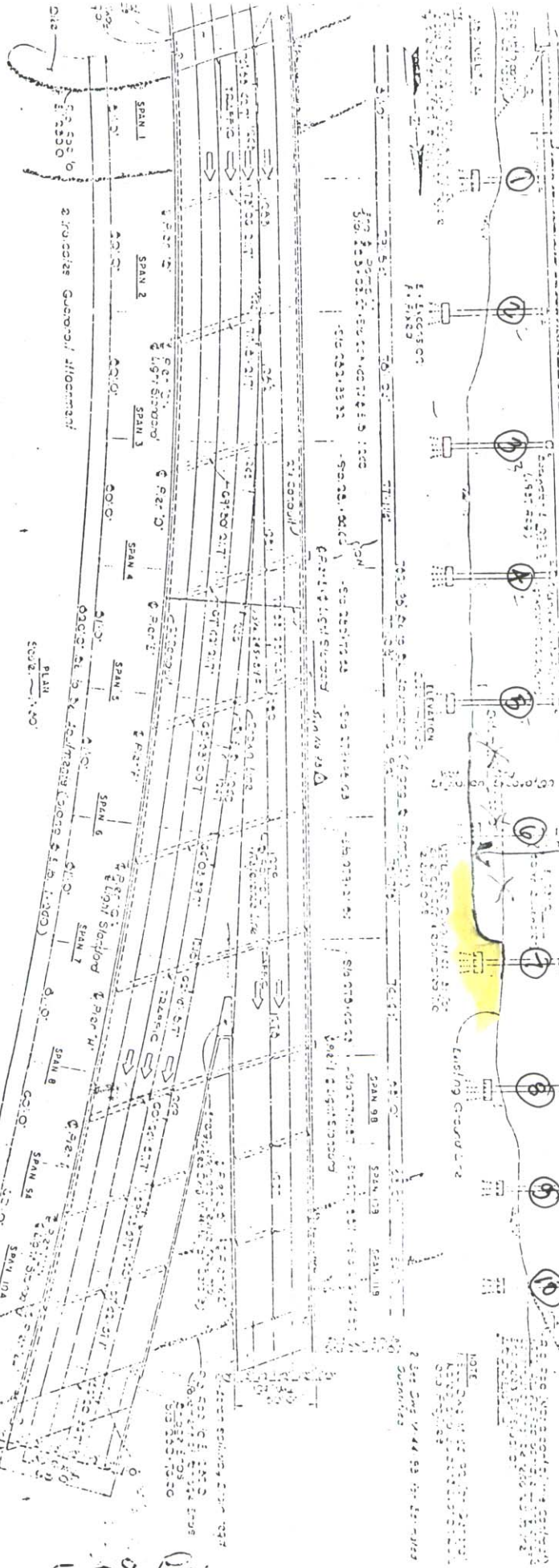
Ground line shown in log is wrong
River bed having been raised
Bent # 7 threatened by Bank failure.

CHANNEL BANK BETWEEN 6+

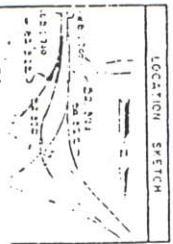
RIP - RAP : YES [] NO : ☒
@ BENT / PIER NO. :

new ground line 8-23-95

COUNT NO	68-601 A2	FILED	SHEET 20
----------	-----------	-------	----------



Year	1900	1901	1902	1903	1904	1905	1906	1907	1908	1909	1910	1911	1912	1913	1914	1915	1916	1917	1918	1919	1920	1921	1922	1923	1924	1925	1926	1927	1928	1929	1930	1931	1932	1933	1934	1935	1936	1937	1938	1939	1940	1941	1942	1943	1944	1945	1946	1947	1948	1949	1950	1951	1952	1953	1954	1955	1956	1957	1958	1959	1960	1961	1962	1963	1964	1965	1966	1967	1968	1969	1970	1971	1972	1973	1974	1975	1976	1977	1978	1979	1980	1981	1982	1983	1984	1985	1986	1987	1988	1989	1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038	2039	2040	2041	2042	2043	2044	2045	2046	2047	2048	2049	2050	2051	2052	2053	2054	2055	2056	2057	2058	2059	2060	2061	2062	2063	2064	2065	2066	2067	2068	2069	2070	2071	2072	2073	2074	2075	2076	2077	2078	2079	2080	2081	2082	2083	2084	2085	2086	2087	2088	2089	2090	2091	2092	2093	2094	2095	2096	2097	2098	2099	2100
1900	1901	1902	1903	1904	1905	1906	1907	1908	1909	1910	1911	1912	1913	1914	1915	1916	1917	1918	1919	1920	1921	1922	1923	1924	1925	1926	1927	1928	1929	1930	1931	1932	1933	1934	1935	1936	1937	1938	1939	1940	1941	1942	1943	1944	1945	1946	1947	1948	1949	1950	1951	1952	1953	1954	1955	1956	1957	1958	1959	1960	1961	1962	1963	1964	1965	1966	1967	1968	1969	1970	1971	1972	1973	1974	1975	1976	1977	1978	1979	1980	1981	1982	1983	1984	1985	1986	1987	1988	1989	1990	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038	2039	2040	2041	2042	2043	2044	2045	2046	2047	2048	2049	2050	2051	2052	2053	2054	2055	2056	2057	2058	2059	2060	2061	2062	2063	2064	2065	2066	2067	2068	2069	2070	2071	2072	2073	2074	2075	2076	2077	2078	2079	2080	2081	2082	2083	2084	2085	2086	2087	2088	2089	2090	2091	2092	2093	2094	2095	2096	2097	2098	2099	2100	

[illegible]

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

PAID 40 328
F B I-240 7167 WOLF 01V569

79-140-5.07K

[illegible]

REV. 07-01-91

BRIDGE NO.

79
COUNTYI-40
ROUTE5.09R
LOG MILE

DATE

JUN 07 1988
AUG 1 1988BENT/PIER NO.TOTAL HEIGHT
(TOP OF CAP TO
BOTTOM OF FOOTING)(t)
FOOTING
THICKNESSW/FTG @ H =
(TOP OF CAP TO
TOP OF FOOTING)EXPOSURE

Abut. #1

Bent #1

Bent #2

Bent #3

Bent #4

Bent #5

Bent #6

Bent #7

Bent #8

Bent #9

Bent #10

Abut. #2

20' Upstream

Through Bed
100' Down Stream

61.6'

3.8'

57.8'

47.1'

61.6'

3.2'

58.4'

52.0'

52.4'

3.2'

49.2'

49.0'

53.2'

3.2'

50.0

36.3'

49.3

3.2'

46.1'

33.6'

25.2'

25.5'

measurements From Top of Cap of Column Bent #6

48.9'

50.1'

49.3'

BRIDGE INSPECTION REPORT

MAY 15 1991

FORM BIR 3.0
3/30/78
REV. 01/01/90
DT-0069

Scour Only

FIELD REPORT NO. 10 DATE 5/14-91
PREVIOUS REPORT NO. _____ DATE _____
PLANS _____ YES ☒ NO ☐

BRIDGE NO. 79 --- I-240 --- 256 REV. SYS. NO. 79 --- I-40 --- 5.09R
County Route Log Mile

1-40 Rt Lane OVER Wolf River
Type Service

Structure Name (if named)

YEAR CONSTRUCTED _____ COUNTY Shelby MAINTENANCE DISTRICT NO. 45
(ESTIMATED OR ACTUAL) ☐ ☐

YEAR WIDENED _____ YEAR REHABILITATED _____
ESTIMATED OR ACTUAL ☐ ☐ ESTIMATED OR ACTUAL ☐ ☐

FEATURES

WEARING SURFACE --- CONCRETE ☐ TIMBER ☐ ASPHALT ☐ (DEPTH = _____")
FLARED WIDTH --- YES ☐ NO ☐
NAVIGATIONAL CONTROL --- YES ☐ NO ☐
MEDIAN TYPE --- OPEN ☐ NONE ☐ CLOSED ☐

BRIDGE SKEW 600 KT
STRUCTURE TYPE C.I.B NO. SPANS 11
Main Span Main Type
STRUCTURE TYPE _____ NO. SPANS _____
Approach Spans Approach Type
MAXIMUM SPAN LENGTH 82' TOTAL LENGTH 783

WIDTHS

DECK OUT-TO-OUT _____
ROADWAY CURB-TO-CURB _____
SIDEWALK RT _____ LT _____
* APPROACH ROADWAY _____
APPR. SHOULDER RT _____ LT _____

CLEARANCES

MIN. VERTICAL OVER DECK _____
MIN. VERTICAL UNDER CL. _____
MIN. LATERAL UNDER CL. _____ RT _____ LT _____

*DOES NOT INCLUDE SHOULDERS

INSPECTORS

1 Beene
2 Stewart
3 Rowland
4 Sisk
5 Fuller
6 Emerson
7 _____
8 _____
9 _____

UNDER WATER INSPECTION

INSPECTION PERFORMED BY:
DOT FIELD TEAM ☐ DATE _____
CONTRACT DIVERS ☐ DATE _____
NONE REQUIRED ☐

NBIS BRIDGE
LENGTH (FT.) (INCH.)

FRACTURE CRITICAL
DETAILS? YES ☐ NO ☐

CHANGE IN STRUCTURAL CONDITION YES ☐ NO ☐
MAJOR REPAIRS MADE YES ☐ NO ☐

IF YES, INCLUDE BIR 3.9

COMMENTS: Scour Only

LATERAL SHIFT TO NORTH MAY UNDERMINE
PIER 6 WHICH HAS P.T.C. EXPOSED
only

BRIDGE RATING

☐ ☐ ☐ ☐
GOOD FAIR POOR CRITICAL

SUPERVISING BRIDGE INSPECTOR P.E.

MAY 15 1991 41

BIR 3.8 A - Use for any bridge with observed or potential scour.
01/01/90

BRIDGE NO: 79 I-40 5.09R DATE: 5/14-91

STREAM CHANNEL DATA & CONDITIONS

STREAM CROSSING:

Wolf River

I. SCOUR LOCATION:

1. ABUT/PIER/BENT
2. ABUT/PIER/BENT
3. ABUT/PIER/BENT
4. ABUT/PIER/BENT
5. ABUT/PIER/BENT
6. ABUT/PIER/BENT
7. ABUT/PIER/BENT

	CHANNEL BED MATERIAL										PIERS & ABUTS	PII BEN
	LOCATED IN CHANNEL	NOT IN CHANNEL	FAILED RIP-RAP	SOLID ROCK	COBBLE	GRAVEL	SAND	SILT	CLAY	NOT VISIBLE		
1. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
2. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
3. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
4. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
5. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
6. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
7. ABUT/PIER/BENT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

II. Has the channel shifted?

(check answer)

LATERALLY

VERTICALLY

- no movement ☐ not apparent ☐ no movement ☐ not apparent ☐
 slight ($\leq 1'$) ☐ moderate ☒ slight ($\leq 1'$) ☐ moderate ☐
 severe ($> 5'$) ☐ severe ($> 5'$) ☐ severe ($> 5'$) ☐ Check previous report

III. Are embankment or approach substructures being threatened by bank failure?
 Indicate River Trying to move North from point on bank

YES ☐ NO ☒

IV. Channel & bank stability conditions:

(Check if applicable)

- (1) Steep bank - Failures upstream: ☒ conditions downstream: ☐
- (2) Moderate bank erosion: ☐
- (3) Bank vegetation: (a) low growth: ☒ (b) large timber: ☒
(c) dead trees: ☐ (d) clear banks: ☐
- (4) Sediment or gravel accumulation: YES ☐ NO ☐ UNKNOWN ☐
- (5) Channel altered or straightened: YES ☐ NO ☐ UNKNOWN ☐
- (6) Stable conditions: (a) live growth: ☒ (b) bedrock: ☐
(c) boulders: ☐ (d) flat slopes: ☐ ($\leq 2:1$)

MAY 15 1991 42

BIR 3.8 B - Use for any bridge with observed or potential scour.
01/01/90

BRIDGE NO: 79 I-40 5.09R DATE: 5/15/91

STREAM CHANNEL DATA & CONDITIONS (CON'T)

V. Waterway adequacy and debris characteristics: (check if applicable)

- (1) Bridge deck elevation:
 - (a) level with approach roadway: ☒
 - (b) higher than approach roadway: ☐
 - (c) roadway approach \geq 2' above natural ground line: ☒
- (2) Abutment encroaches into channel: no
- (3) Large scour (blowhole) under bridge: see soundings
- (4) Indications that flood waters overtop bridge:
NO ☒ YES ☐ --Occasionally ☐ Frequently ☐ Unknown ☐
- (5) Debris characteristics:
 - (a) debris present: YES ☒ NO ☐ Drift on Pier #5
 - (b) debris likely to accumulate: YES ☒ NO ☐
 - (c) dead trees upstream: ☒ or downstream: ☐

VI. Condition of rip-rap? G ☒ F ☐ P ☐ C ☐ Est. % failed: on Abutts only N/A: ☐

VII. Underwater diver inspection recommended? YES ☒ NO ☐

VIII. Overall condition of channel? G ☐ F ☐ ☒ C ☐ drift & Scour

IX. Item 61 - Code values 0 thru 9 according to the recording and coding guide currently in effect: 5

X. Comments: footing Above ground in Channel
Pier #5 & 6 see soundings
Large Drift on pier #5
Deepest water next to North bank 17.0
Top footing Arc 16.0' Beene

SPECIAL INSPECTION DATA

(for reasons other than FC or scour)

I. Does this bridge need a special inspection? YES ☐ NO ☒

II. Reason for special inspection:

MAY 15 1991

INSPECTION TEAMS SUMMARY

BRIDGE NO. 79 I-40 5.09R

Scour on Rt. Lane I-40 Piers 5 & 6
a large drift caught on upstream side
of pier #5

footing Exposed on pier 5 & 6 measured
with rod and touched footing at 16.0'.

Depth are 17.0' deepest around piers.

Depth do not indicate scour below
bottoms of footings at this date.

Ground line are below top of footings.

James Beine

79 - 140 - 5.09 RT

PIER		FTG "t"	(TOP OF CAP - TOP OF FOOTING)	LT.	RT.
				42.44	
B	1	4' - 3"		42.44	38.5
C	2	4' - 6"		52.86	49.7
D	3	4' - 6"		58.10	55.9
E	4	3' - 9"		59.41	56.1
F	5	3' - 3"		60.68	56.7
G	6	3' - 3"		59.95	47.4
H	7	3' - 3"		51.78	49.26
I	8	3' - 3"		47.62	44.58
J	9	3' - 3"		43.20	42.54
K	10	3' - 3"		35.24	34.66

BRIDGE NO.

79 COUNTY I-40 ROUTE

5.09 R
LOG MILE

DATE 7-13-91

BENT/PIER NO.

TOTAL HEIGHT
(TOP OF CAP TO
BOTTOM OF FOOTING)

(t)
FOOTING
THICKNESS

W/FTG @ H =
(TOP OF CAR TO
TOP OF FOOTING)

EXPOSURE

Pier 2
and 5.

PLANS

[illegible]

DATE 7-13-91

EXPOSURE

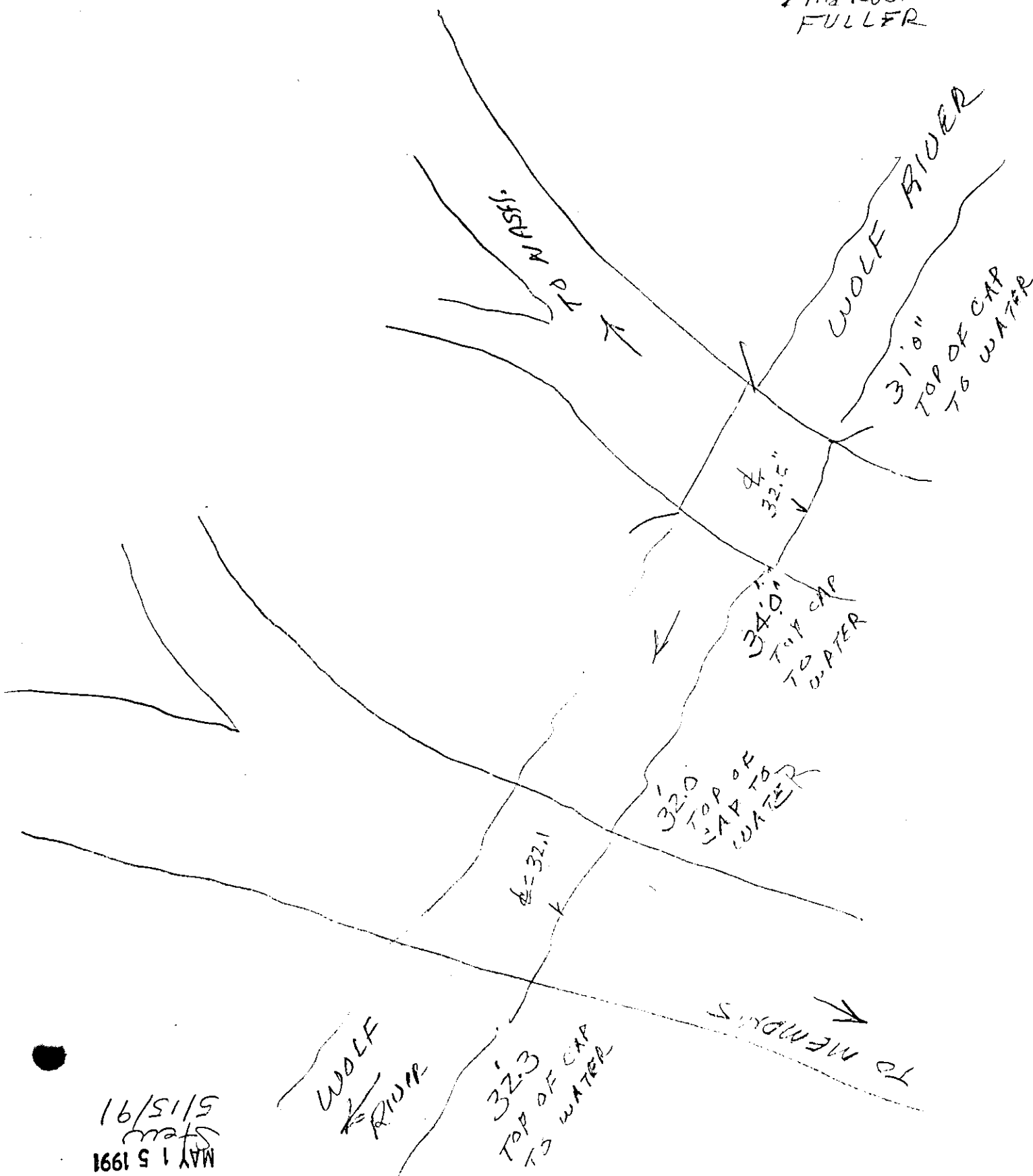
Pier #1
under.

PLANS

Construction

[illegible]

5-15-91
STOWART
EMERSON
FULLER

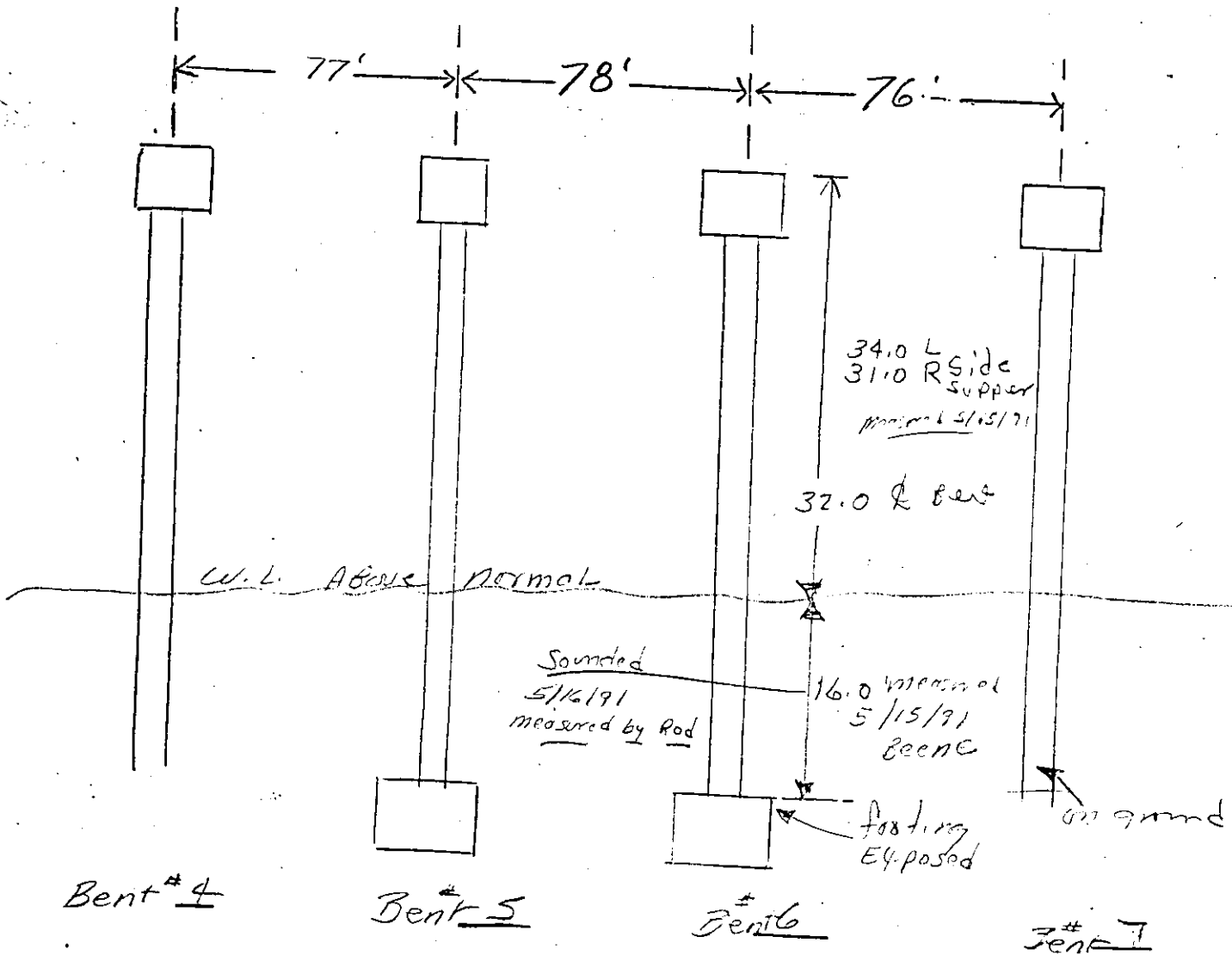


1661 S I VW
MAY 15 1991
STOWART
5/15/91

BR. No. 79 I-40 5.09Rt

MAY 15 1991

45



4

BENT/PIER NO.

5/15/91

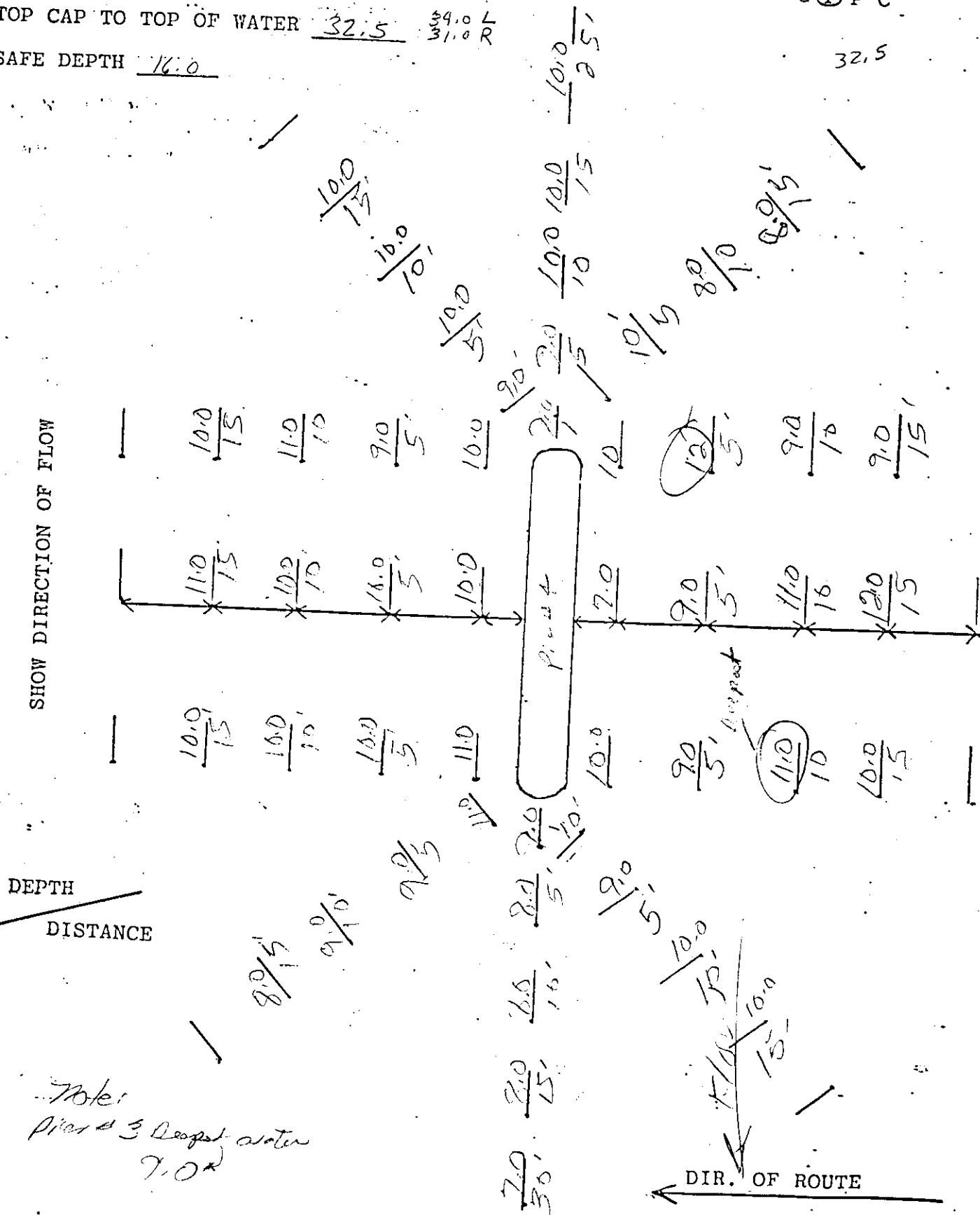
$$\text{SCOUR} = G \textcircled{F} P C$$
$$\text{DRIFT} = G \textcircled{F} P \cdot C$$

48.0

32.5

SAFE DEPTH 16:0

32.5



Note:
Piles @ 3 Deep water
7.0*

BRIDGE NO. 79 I-70 529 Rt.

MAY 15 1991
BENT/PIER NO. 5

PIER HEIGHT

5/15/91

SCOUR = G E P C

DRIFT = G F P C

Large Drift

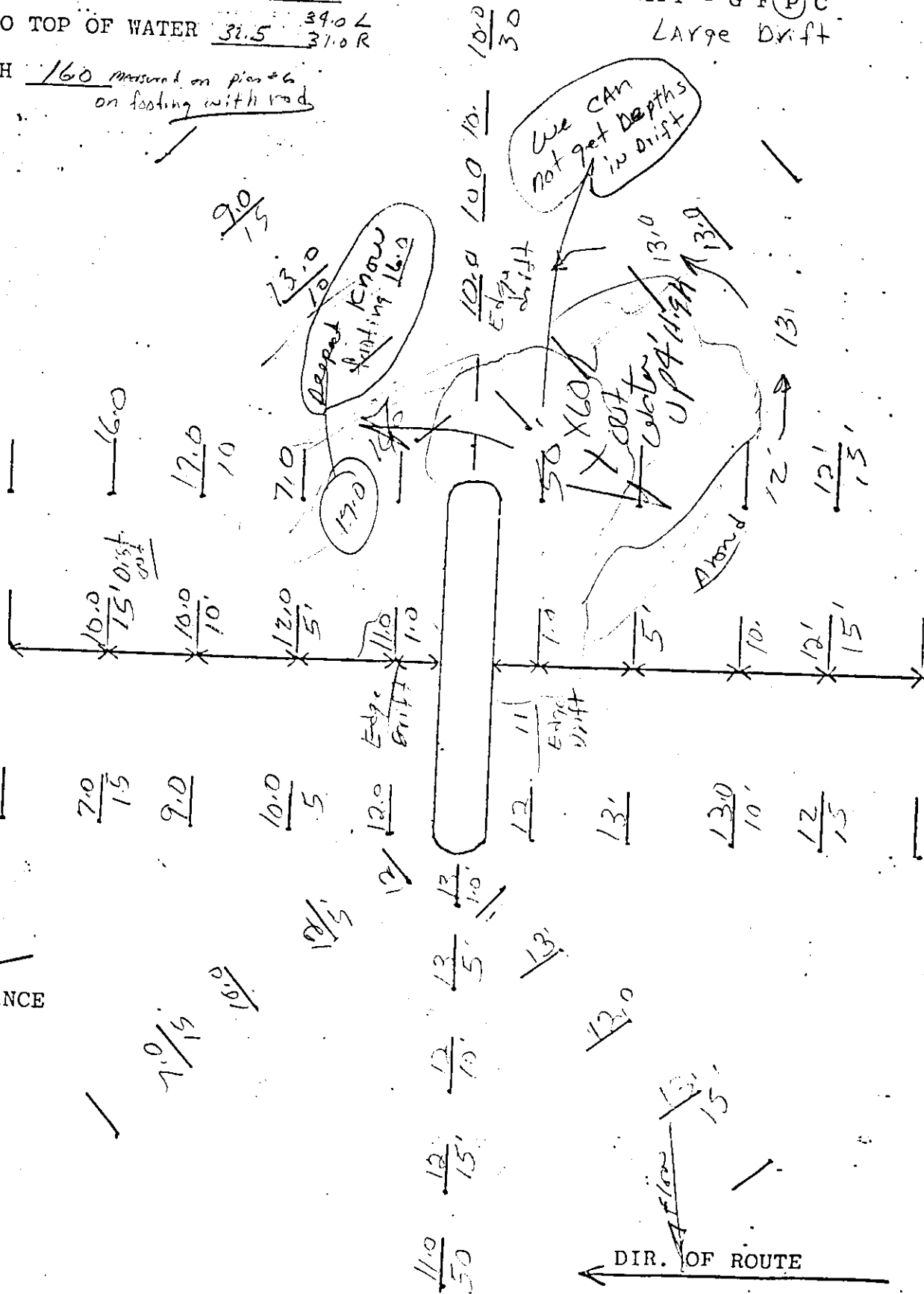
TOP CAP TO TOP OF FOOTING

TOP CAP TO TOP OF WATER 32.5 $\frac{39.0}{31.0}$ L

SAFE DEPTH 16.0 measured on pier #6
on footing with rod

SHOW DIRECTION OF FLOW

DEPTH
DISTANCE



BRIDGE NO. 79

I-90

5.09 Rd.

MAY 15 1991
BENT PIER NO. 55C

PIER HEIGHT _____

TOP CAP TO TOP OF FOOTING 48.0

TOP CAP TO TOP OF WATER 32.5

SAFE DEPTH 16.0

measured 5/15/91
deena

Top water

To top

footing

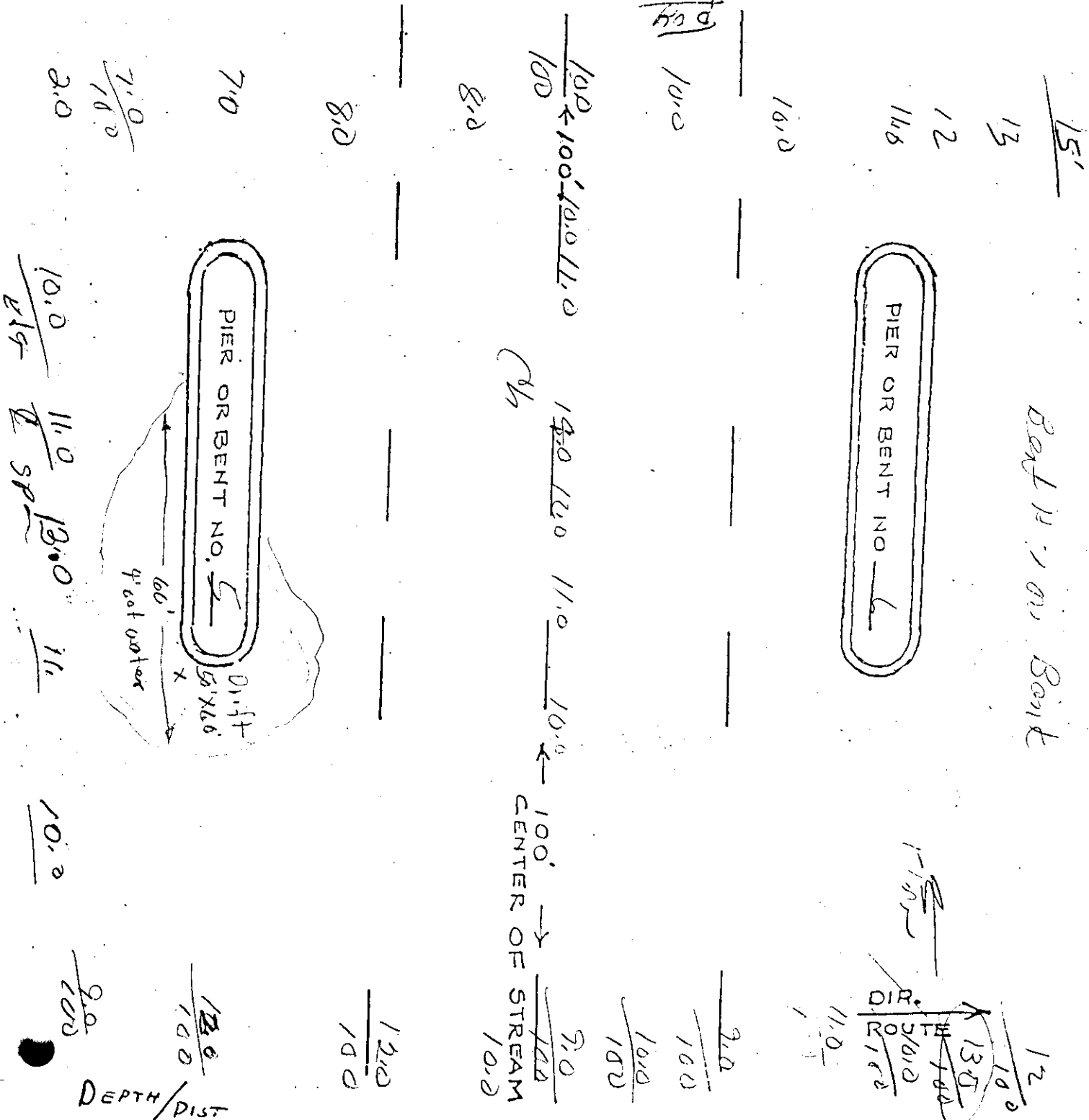
16.0'

10.0'

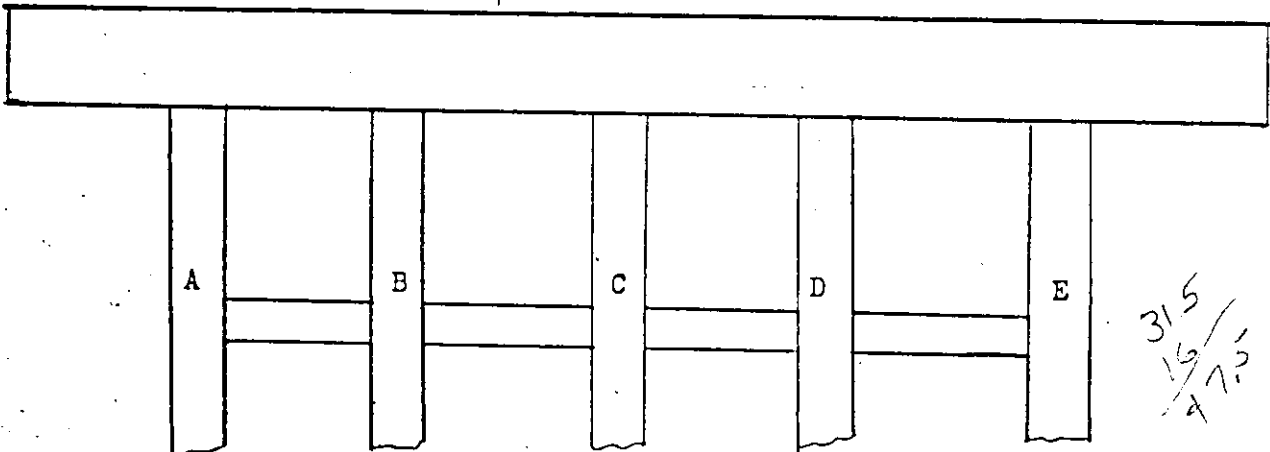
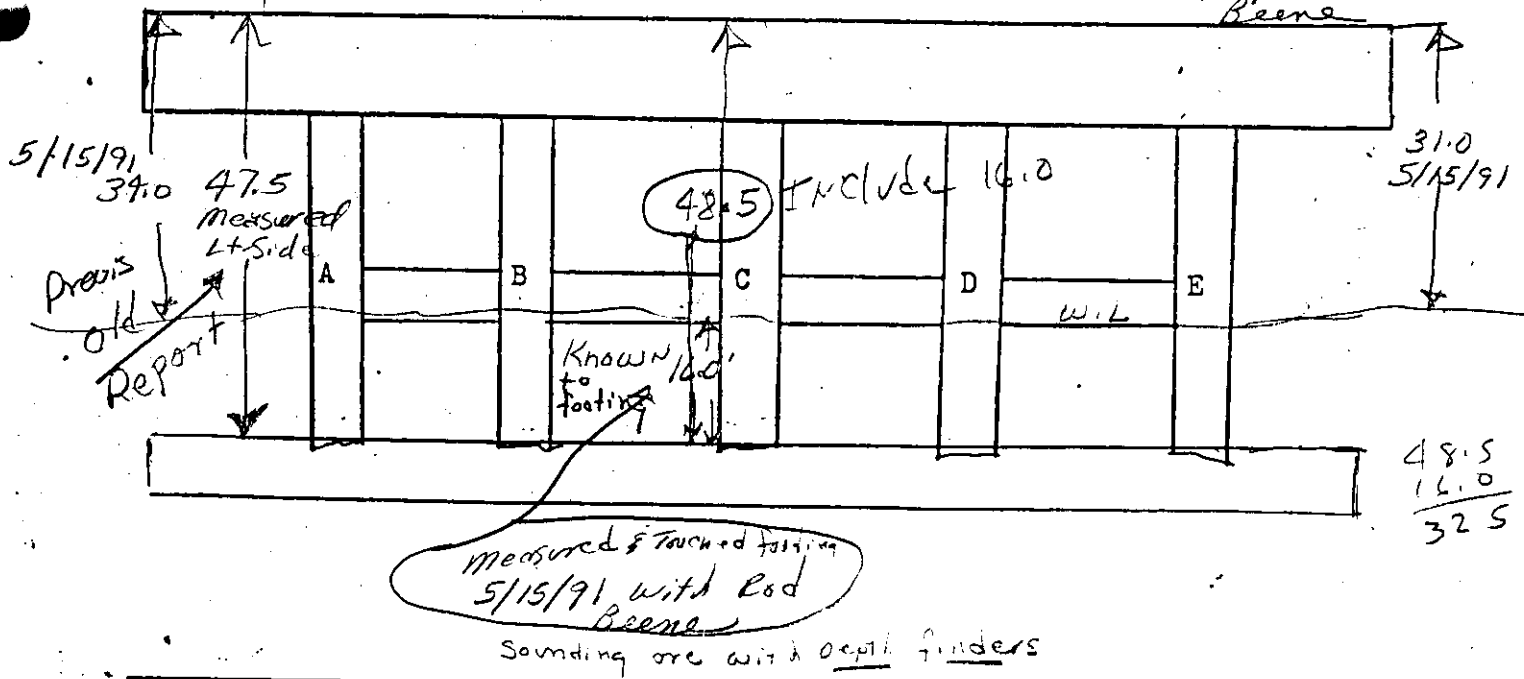
SCOUR = G F (P) C

DRIFT = G F (P) C

5/15/91



MAY 15 1991

BRIDGE NO. 79 I-40 5.09 RTBENT NO. 65/15/91
Beene

ELEMENT	RATINGS	COMMENT
CAP	G F P C	
COLUMN A-	G F P C	
	G F P C	
	G F P C	
	G F P C	
BEARINGS	G F P C	
STRUTS	G F P C	

BRIDGE NO. 29 I-40 5.09.14

MAY 15 1991
BENT/PIER NO. 9

PIER HEIGHT _____
TOP CAP TO TOP OF FOOTING _____
TOP CAP TO TOP OF WATER _____
SAFE DEPTH 16.0 Top of footing
measured 5/15/91
Beene

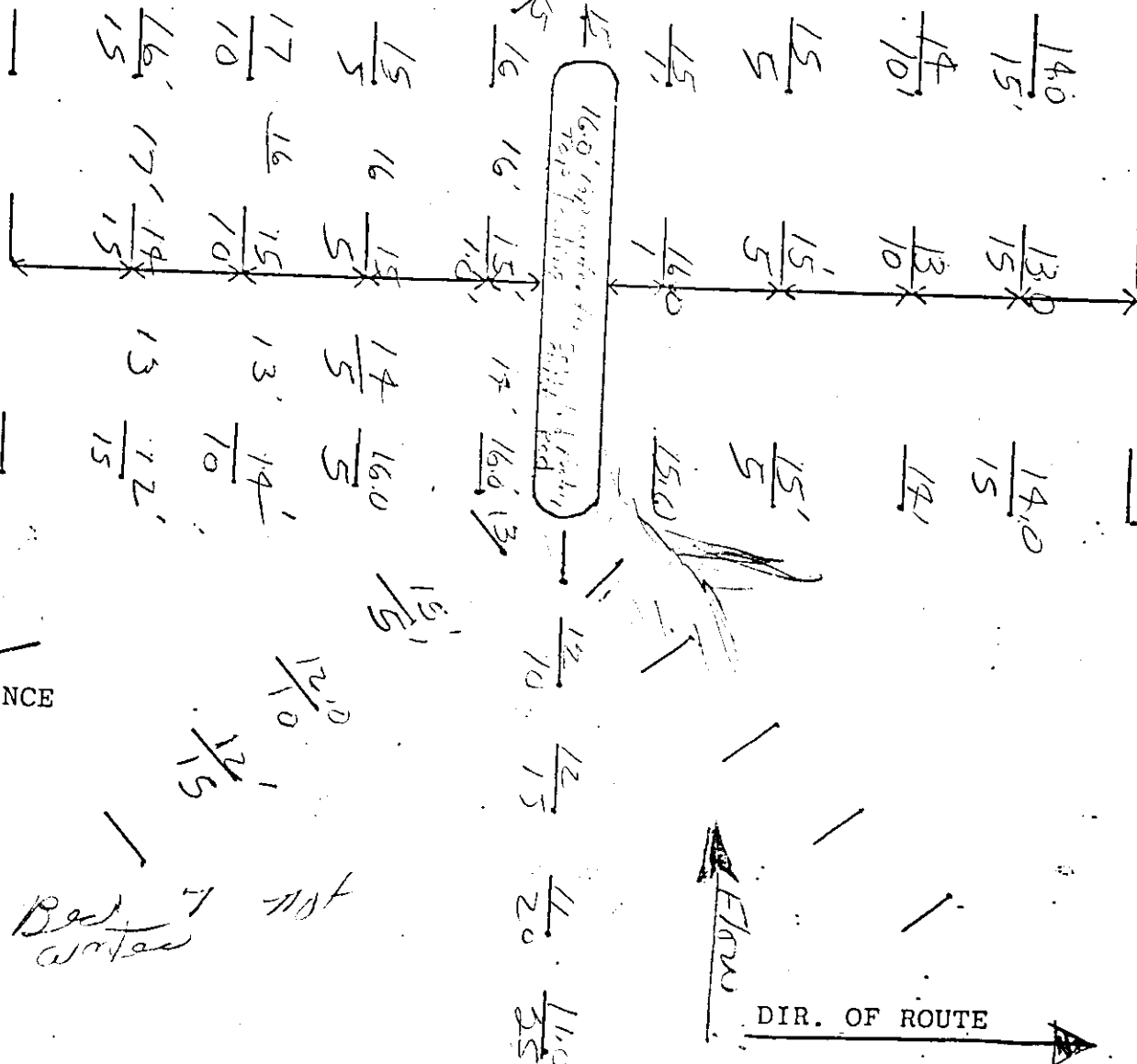
5/15/91
Top water
to top
of footing

SCOUR = G F (P) C
DRIFT = G F (P) C
Small drift on (6)
under water can't
determine size

SHOW DIRECTION OF FLOW

DEPTH
DISTANCE

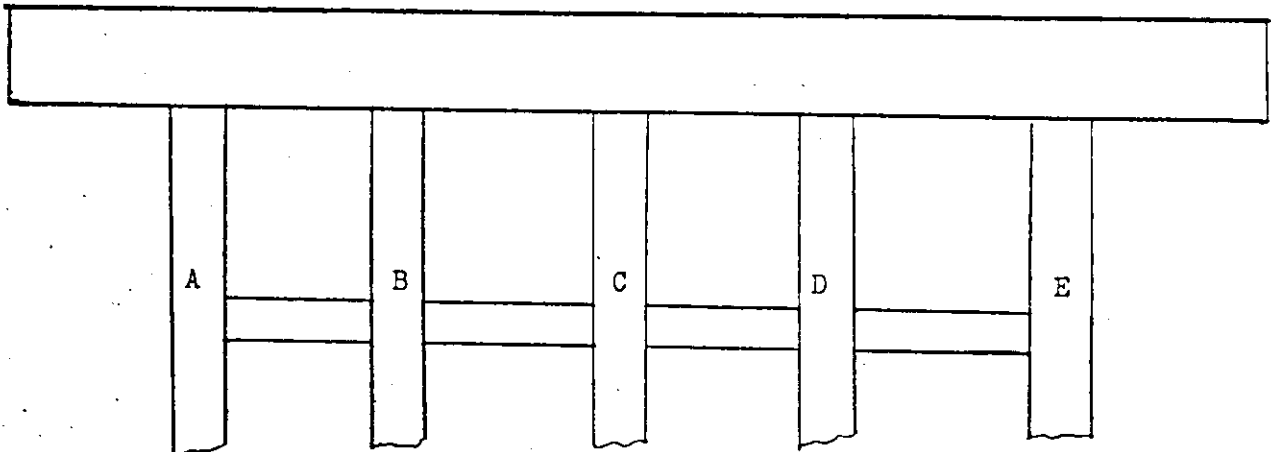
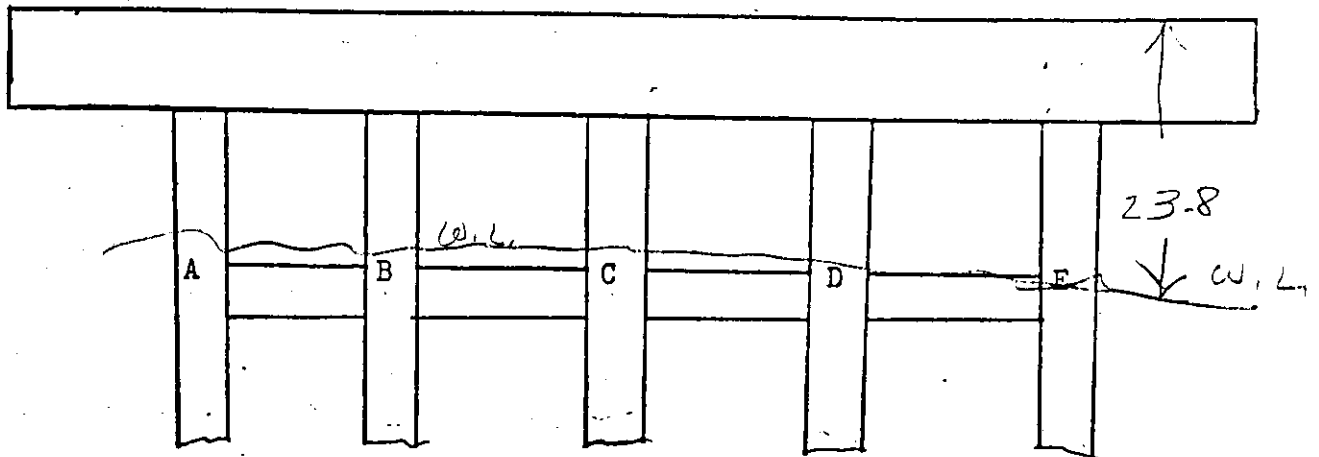
Note Bed is not
in water



MAY 16 1991

BRIDGE NO. 79 I-40 5.09 RT

BENT NO. 5
5-16-91



ELEMENT	RATINGS	COMMENT
CAP	G F P C	
COLUMN A-	G F P C	
	G F P C	
	G F P C	
	G F P C	
BEARINGS	G F P C	
STRUTS	G F P C	

AUG 3 1988

BRIDGE NO. 79 I-40 5.09 RT

~~PIER~~ BENT NO. 5

PIER ELEV. _____
WATER ELEV. _____
BOTTOM FOOTING ELEV. _____
MAX. SAFE DEPTH _____
HIEGTH PIER _____

SOUNDING SKETCH

Deepest water
is 3.8' deep -
NO sign of
SCOUR

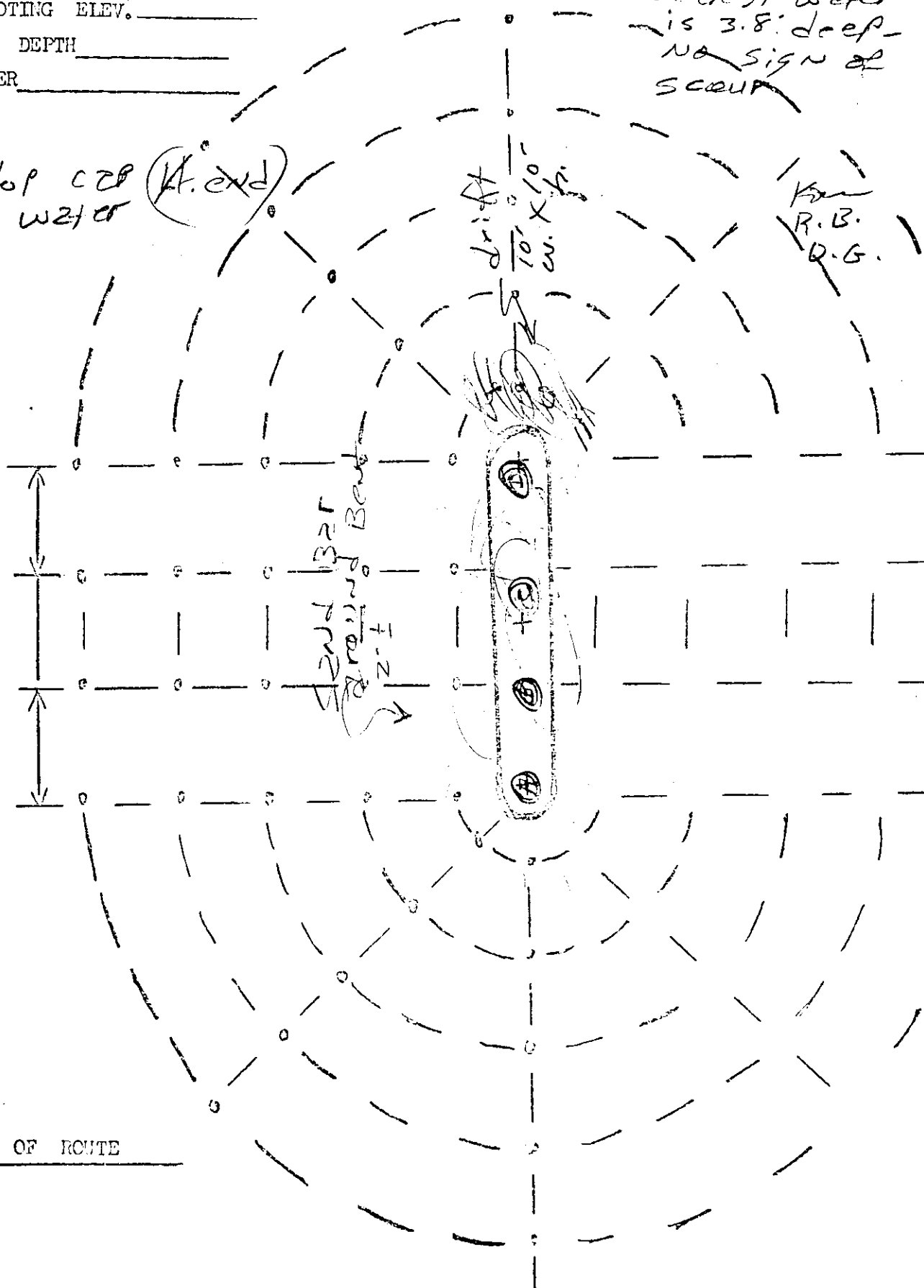
49.3' top cap (A. end)
to top water

10' x 10' x 10'

R.B.
D.G.

FLOW

DIR. OF ROUTE



AUG 3 " 1988

BRIDGE NO. 79 I-40 509

RT

PIER
BENT

NO.

6 120

PIER ELEV. _____

WATER ELEV. _____

BOTTOM FOOTING ELEV. _____

MAX. SAFE DEPTH _____

HIEGTH PIER _____

SOUNDING SKETCH

W. R. B. D. G.

49.3' top C/P
to water (Lt. end)

NOTE:

Footings ABOVE
Ground columns
"A" "B" + "C"

* 5 shots

2.5' - 2.6' & 2.1'

are on top

of footing

around

column

A, B & C

Column

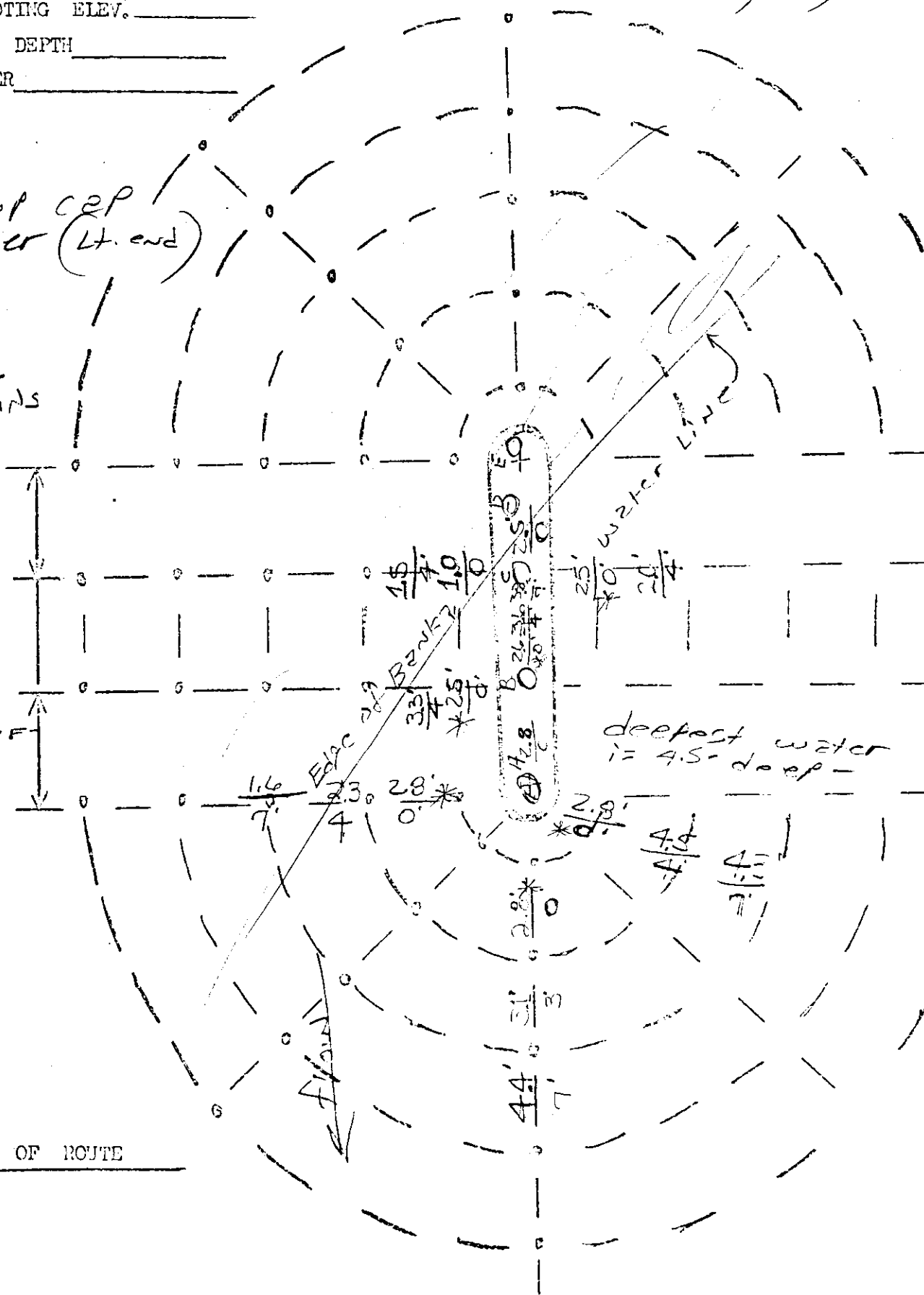
D & E

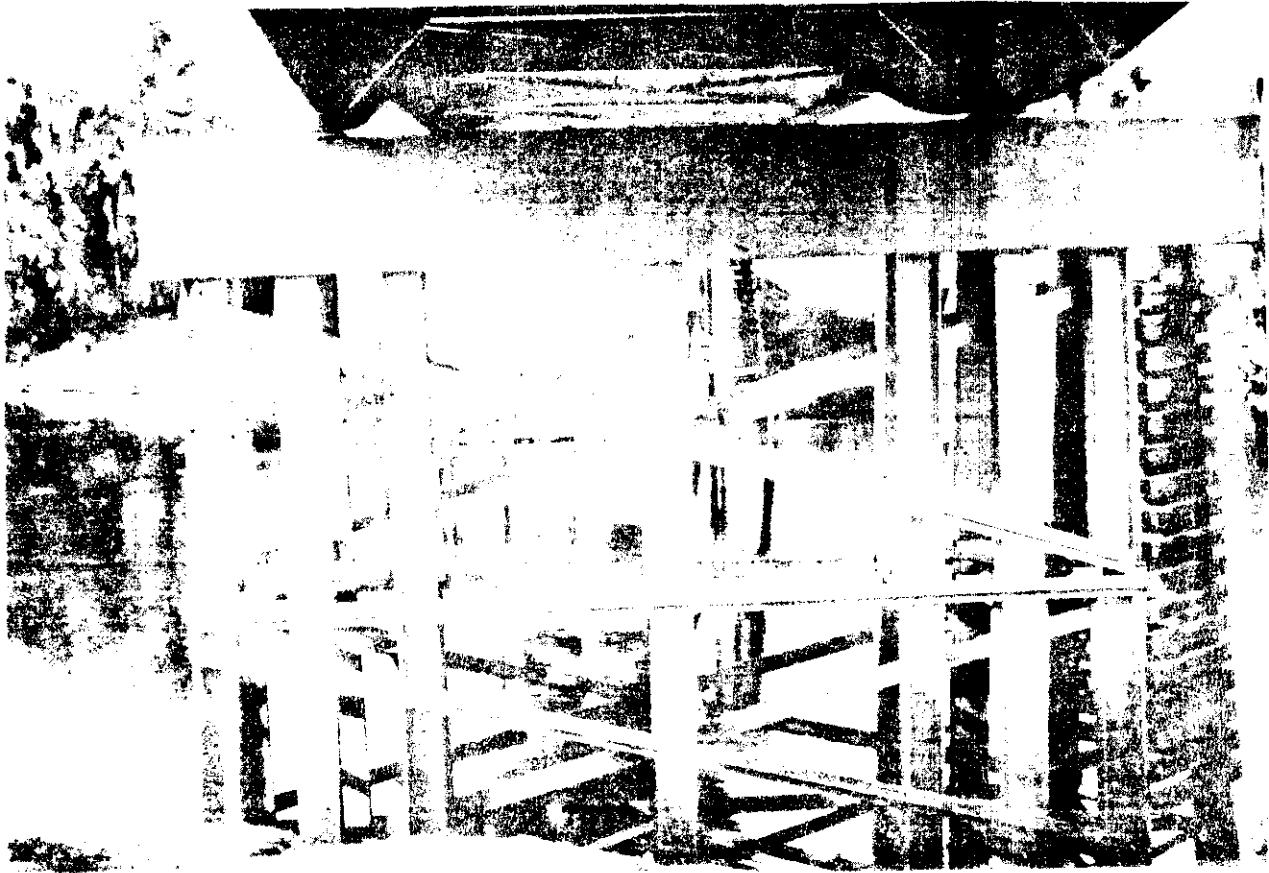
are out

of water

FLOW

DIR. OF ROUTE





Steel Pile Pier Rated Good

1983
PROFILE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



BRIDGE NO.



VIEW ACROSS TOP OF DECK

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



APPROACH #1, BROKEN DRAIN CURBS



SINGLE
RAIL
TRANSITION
I36B=0
SEE PHOTO
I36D=0
ALSO

LOOKING AHEAD ON ROUTE

Bridge Loc. No: 79 - I0040 - 05.09 - R

Date: 09-16-99



RIGHT SIDE VIEW OF BRIDGE



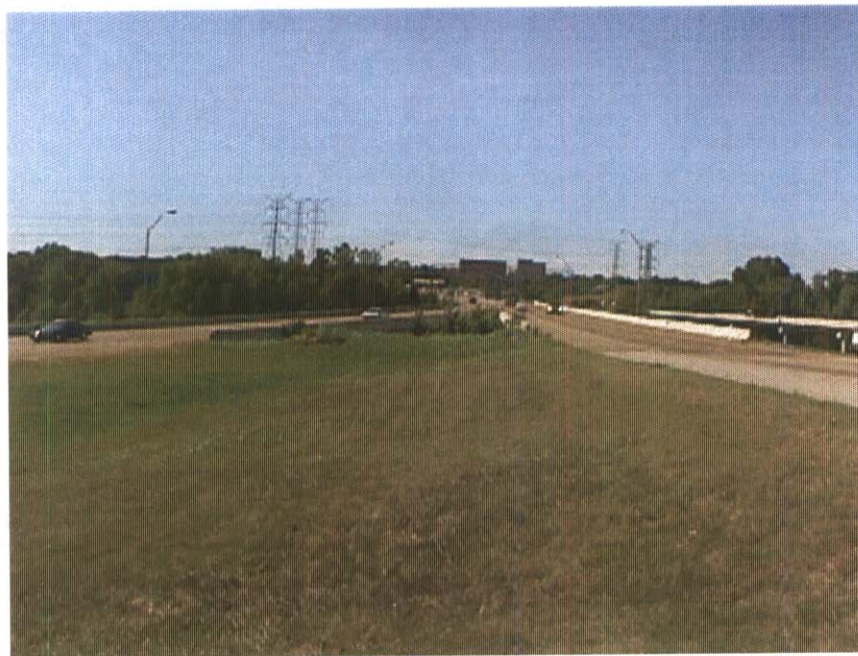
OLD COFFER DAM
REMOVED

LOOKING UPSTREAM, RIGHT SIDE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99

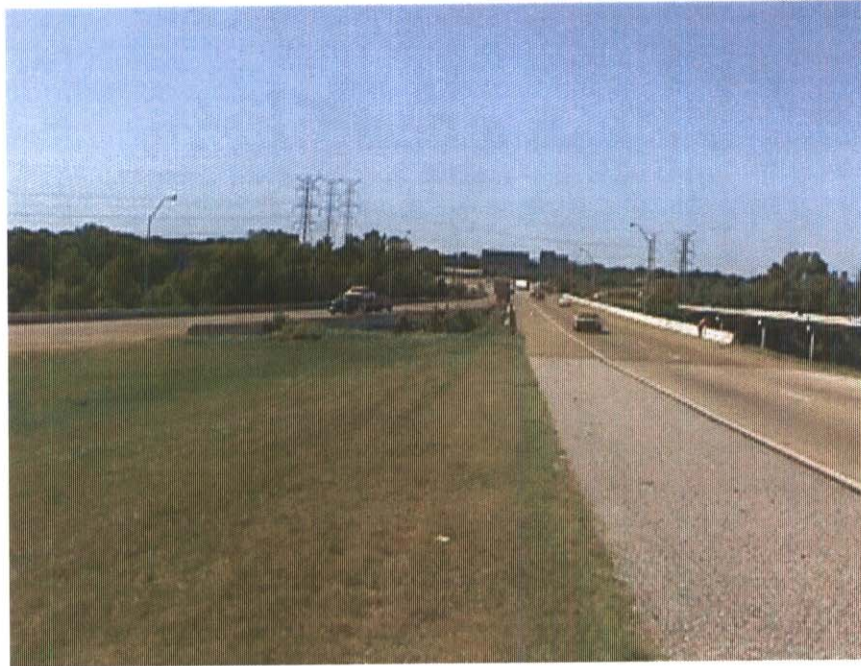


RIGHT SIDE VIEW OF RUBBLESTONE



LOOKING BACK ON ROUTE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



LOOKING BACK ON ROUTE



LEFT SIDE VIEW OF BRIDGE

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



BOTTOM OF DECK, PART "A"

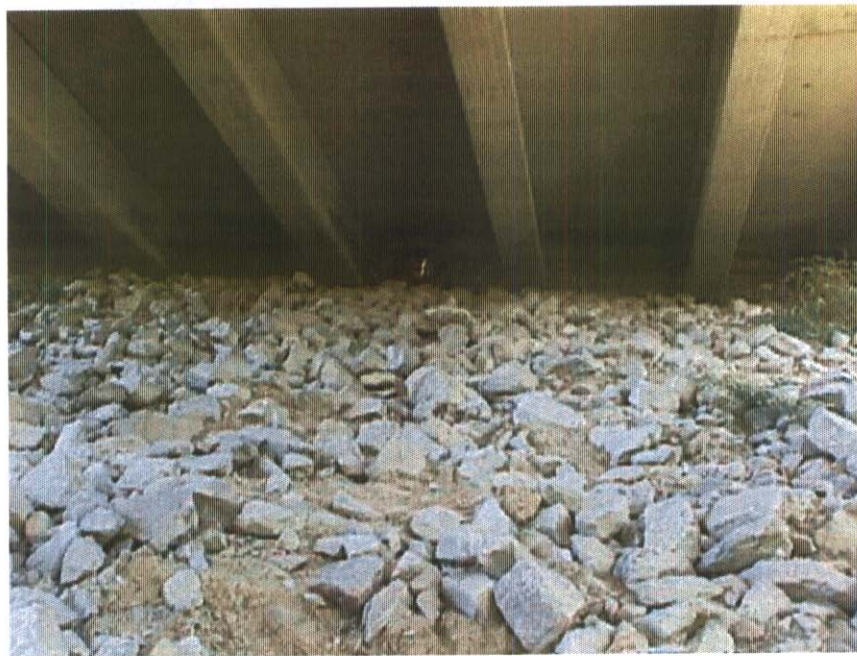


ABUTMENT #2, PART "B"

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



BOTTOM OF DECK CONNECTION PART "A" & PART "B"



ABUTMENT #2, PART "A"

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



BOTTOM OF DECK, PART "B"



BENT #8

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



RIGHT SIDE VIEW OF CHANNEL



BENT #5

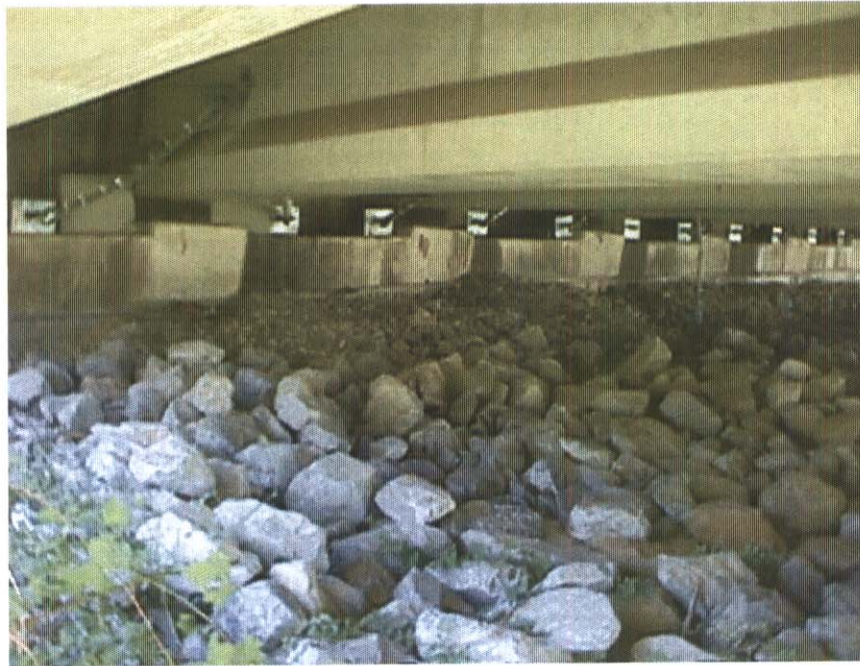
Bridge Loc. No: 79 - I0040 - 05.09 - R

Date: 09-16-99

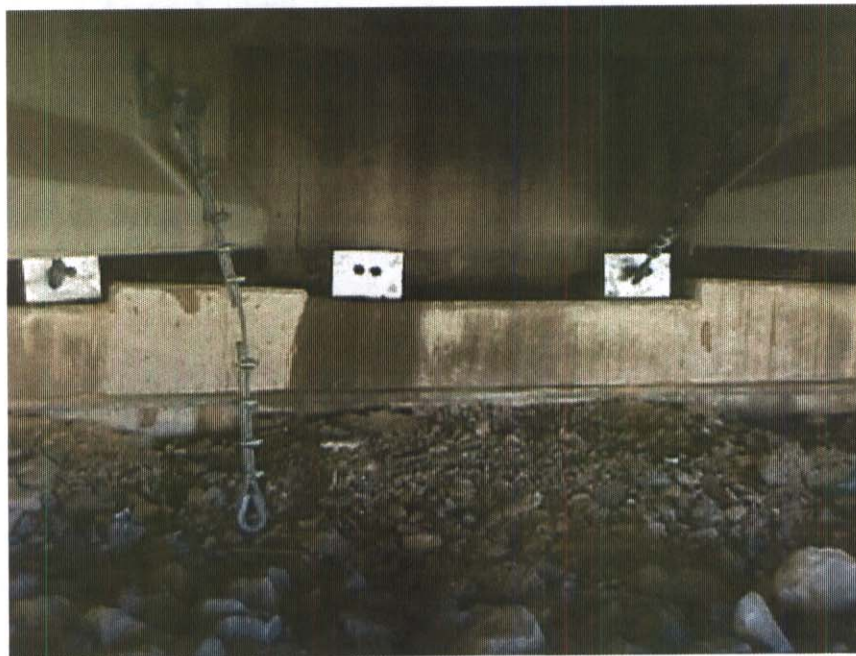


BENT #6

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



ABUTMENT #1



**ABUTMENT #1, EARTHQUAKE CABLE CONNECTED TO GIRDER "G",
BROKE LOOSE**

Bridge Loc. No: 79 - I0040 - 05.09 - R Date: 09-16-99



**ABUTMENT #1 EARTHQUAKE CABLE CONNECTED TO GIRDER "D",
BROKE LOOSE**



RIGHT SIDE VIEW OF CHANNEL

Bridge Loc. No: 79 - I0040 - 05.09 - R

Date: 09-16-99



LOOKING DOWNSTREAM, LEFT SIDE

SEP 16 1999

FIELD REPORT NO. : 14 DATE : 9-16-99
PREVIOUS REPORT NO. : DATE : 08/18/95
PLANS----YES[X] NO[]

CREEK NAME : WOLF RIVER

ROAD NAME : 000

ROAD NAME :

STRUCTURE NAME (IF NAMED) : _____
COUNTY : Shelby MAINTENANCE DIST. NO.: 45

```
WEARING SURFACE---CONCRETE [ ]
FLARED WIDTH-----YES [X]
NAVIGATIONAL CONTROL---YES [ ]
MEDIAN TYPE-----OPEN [X]
```

TIMBER [] ASPHALT [X] (DEPTH = 6)
 NO [] CLOSED []
 NO [X] BRIDGE SKEW : 60 LT [] RT [X]
 NONE []

NO. SPANS : 11
MAIN SPAN

NO. SPANS : _____
APPROACH SPANS

TOTAL LENGTH : 830.1

CLEARANCES (*.*m)

MIN. VERT. OVER DECK : _____
 MIN. VERT. UNDER CL. : _____
 MIN. LAT. UNDER CL. : _____ RT
 LT

*DOES NOT INCLUDE SHOULDERS

1. Bomar
2. Toshi
3. Scott
4. Greer
5. Ellison
6. Evans
7. Newton
8. _____

(< 7 . 6 2 m)
NBIS BRIDGE
LENGTH
(* . ** m)

DOT FIELD TEAM [☒] DATE : 9-16-99
CONTRACT DIVERS [☐] DATE : _____
NONE REQUIRED [☐]

CHANGE IN STRUCTURAL CONDITION YES [] NO [X]
MAJOR REPAIRS MADE YES [X] NO [X]

FRACTURE CRITICAL
DETAILS: YES[] NO[☒]
IF YES, INCLUDE BIR.3.9

\$ 200,000 REPAIR PROJ

BRIDGE RATING [] [~~X~~] [] []
GOOD FAIR POOR CRITICAL

PERFORMANCE EVALUATION

Time of day inspected 10:00 Weather conditions Clear 76°

Vehicles observed All Types

<u>LIVE LOAD BEHAVIOR</u>	<u>YES</u>	<u>NO</u>	<u>COMMENTS</u>
Substructure			
Horiz. & Vert. Defl. - - - []		[X]	
Vibration - - - - - []		[X]	
Superstructure			
Horiz. & Vert. Defl. - - - []		[X]	
Vibration - - - - - []		[X]	

APPROACH

Alignment	(G) F P C	
Slab	G F P C	<u>NIV</u>
Joints	G (F) P C	
Pavement	G (F) P C	
Embankment	(G) F P C	
Drains	G F (P) C	<u>Broke Rt Approach</u>

TRAFFIC SAFETY FEATURES

		STANDARD	SUB-STANDARD
Bridgerailing	G (F) P C	[X]	[]
Transitions	(G) F P C	[X]	[]
Guardrail	(G) F P C	[X]	[]
Guardrail Terminal	(G) F P C	[X]	[]
<u>Cushion Barrier</u>	<u>(2) - Span #8</u>		

SIGNING

	YES	NO	NEEDED	
Paddleboard - - - - -	[]	[X]	[]	WEIGHT LIMIT POSTED
				YES [] NO [X]
Vertical Clearance (< 4.4 m) - - -	[]	[X]	[]	GROSS-- TONS
Narrow [] One Lane Bridge [] - []	[]	[X]	[]	2 AXLE - TONS
				3 OR MORE
				AXLES--- TONS

Other Signs or Plaques Span #4 over Sign

Comments Regarding Any Problems With Signing _____

RECOMMENDATIONS

Bridgerail Is Substandard []	Install Post Load Limit Signs []
Approach Rail Is Substandard []	Level Approach []
Install Paddleboard Signs []	

Other Recommendations _____

SEP 16 1999

FORM BIR 3.2

P. 3-1-97

0081

BRIDGE LOC. NO. 79-140-509R ()
CO. ROUTE L.M. (LOG km)

DATE: _____

DECKCOMMENTS

WEARING SURFACE	G	(F)	P	C
DECK - STRUCTURAL	G	(F)	P	C
CONDITION				
CURBS	G	F	P	C
MEDIAN	G	F	P	C
SIDEWALKS	G	F	P	C
PARAPET	G	(F)	P	C
RAILING	G	F	P	C
PAINT	G	F	P	C
DRAINS	G	F	(P)	C
LIGHTING STD'S	G	F	P	C
UTILITIES	G	F	P	C
JOINT LEAKAGE	G	(F)	P	C
EXPANSION JOINTS	G	F	P	C

Stopped-up Span 1, 3, 4, 5 10-A

Ⓢ Abuts

SUPERSTRUCTURECOMMENTS

BEARING DEVICES	G	F	P	C
GRIDERS OR BEAMS	G	(F)	P	C
FLOOR BEAMS	G	F	P	C
STRINGERS	G	F	P	C
DIAPHRAGMS	G	(F)	P	C
BRACING	G	F	P	C
TRUSSES - GENERAL	G	F	P	C
- PORTALS	G	F	P	C
- BRACING	G	F	P	C
PAINT	G	F	P	C
ALIGNMENT OF	(G)	F	P	C
MEMBERS				

TEXTURE COAT

CONDITION RATING	G	F	(P)	C
OVERALL APPEARANCE	G	F	(P)	C
STAINING	G	F	(P)	C

FADING G (F) P C

NEEDS SPOT PAINTING? YES [X] NO []

NEEDS REPAINTING? YES [] NO []

COMMENTS: 20% Sealing Parapets

SCALING G (F) P C

RECOMMENDATIONS

CLEAN & SEAL JOINTS []

CLEAN DRAINS [X]

(069) Texture Coat Parapets All

NOTE: UNLESS OTHERWISE NOTED, MEASUREMENTS ARE TO BE TAKEN TO TWO (2) DECIMAL PLACES IN METERS.

FORM BIR 3.3
Rev. 3-1-97
-0082

BRIDGE LOC. NO. 79 - I-40 - 509R ()
CO. ROUTE L.M. (LOG km)

SEP 16 1999

DATE: _____

SUBSTRUCTURE

ABUTMENTS

COMMENTS

PILES TO BE REPLACED

	G	F	P	C		PILE(S)	BENT
CAPS	G	F	P	C			
BREASTWALL	G	F	P	C			
WINGS	G	F	P	C			
BACKWALL	G	F	P	C			
PLUMB	G	F	P	C			
FOOTING	G	F	P	C			
PILES	G	F	P	C	N/V		
EMBANKMENT	G	F	P	C			
BEARINGS	G	F	P	C			
SLOPE PAVING	G	F	P	C			
RIP RAP	G	F	P	C	Rock		
Earthquake Devices							
PIERS					D & C Span #1 Broke Loos		

CAPS	G	F	P	C			
COLUMNS	G	F	P	C			
PLUMB	G	F	P	C			
FOOTINGS	G	F	P	C			
PILES	G	F	P	C			
BEARINGS	G	F	P	C			
WEB	G	F	P	C			

BENTS

CAPS	G	F	P	C			
COLUMNS	G	F	P	C			
PLUMB	G	F	P	C			
FOOTINGS	G	F	P	C			
PILES	G	F	P	C			
BEARINGS	G	F	P	C			
BRACING	G	F	P	C			

Struts

PILES NEED REPLACEMENT NO [X] YES []

CUT VEGETATION NO [X] YES []

CLEAR DRIFT NO [] YES [X]

RECOMMENDATIONS: Repair Earthquake devices Span #1 @ Abut 1

NOTE: UNLESS OTHERWISE NOTED, MEASUREMENTS ARE TO BE TAKEN TO TWO (2) DECIMAL PLACES IN METERS.

SEP 16 1999

FORM BIR 3.8
P.V. 3-1-97
1508

BRIDGE LOC. NO. 79-140-509R ()
CO. ROUTE L.M. (LOG km)

DATE: 9-16-99

STREAM CHANNEL DATA AND CONDITIONS

STREAM CROSSING: WOLF RIVER

- I. 1. Type of bed material? Sand-Silt And Clay
2. Has the channel shifted? YES [] NO [] NOT APPARENT [X]
3. Condition of rip-rap? G (F) P C Est. % failed N/A []
4. Overall condition of channel? G (F) P C
5. Item 61 - Code values 0 thru 9 according to the recording and coding guide currently in effect: 6
6. Underwater diver inspection recommended? YES [] NO [X]
If yes, why? _____

- II. Channel and bank stability conditions: (check if applicable)
1. Steep bank - Failures upstream [] downstream [] conditions
2. Moderate bank erosion [X]
3. Bank (a) low growth [X] (b) large timber [X]
Vegetation (c) dead trees [X] (d) clear banks []
4. Sediment or gravel accumulation: YES [] NO [X] UNKNOWN []
5. Channel altered or straightened: YES [] NO [X] UNKNOWN []
6. Stable conditions: (a) live growth [X] (b) bedrock []
(c) boulders [] (d) flat slopes []
(=2:1)

- III. Waterway adequacy and debris characteristics: (check if applicable)
1. Bridge deck elevations:
(a) level with approach roadway [X]
(b) higher than approach roadway. []
(c) roadway approach ≥ 0.6 m above natural ground line. . [X]
2. Abutment encroaches into channel []
3. Large scour (blowhole) under bridge. []
4. Indications that flood waters overtop bridge:
NO [X] YES [] OCCASIONALLY [] FREQUENTLY [] UNKNOWN []
5. Debris characteristics:
(a) debris present YES [X] NO []
(b) debris likely to accumulate: YES [X] NO []
(c) dead trees upstream: [X] or downstream: [X]

IV. Comments: Large Drift on AT End of Bent #4 & 5

SPECIAL INSPECTION DATA - FOR REASONS OTHER THAN FC OR SCOUR

- I. Does this bridge need a special inspection? YES [] NO [X]
II. Reason for special inspection: _____

NOTE: UNLESS OTHERWISE NOTED, MEASUREMENTS ARE TO BE TAKEN TO TWO (2) DECIMAL PLACES IN METERS.

INSPECTION TEAMS SUMMARY

SEP 16 1999

BRIDGE NO. 79 10040 0509 R
CO. ROUTE LM R/L

This Bridge is in Fair Condition Approaches A.C. All Safety features in place. Approach #1 Drain Stopped-up & Concrete Broke. Top Deck & Concrete Parapets no major problem. Drains Stopped-up Spans 1, 3, 4, 5 & 10-A. Substructure Concrete with no major problems. Earthquake devices are broke loose at Abut #1 @ Beam D & G. Superstructure Concrete with no major problems. Large Drift Right Side Bents 4 & 5. no problem with Scour at this time Can not hit Footing

Bumak

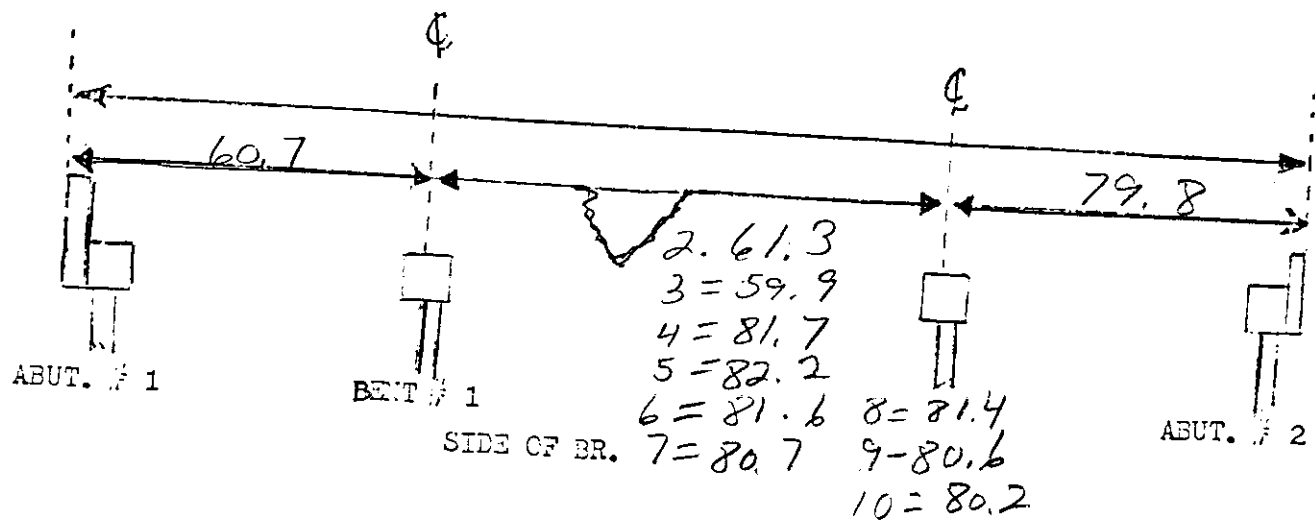
☒ CROSS SECTION☒ PONTIS

BR. NO. 79-I40-509R

SEP 16 1999

DIR. OF ROUTE →

F = FIXED
E = EXPANSION
S = SIMPLE
C = CONTINUOUS



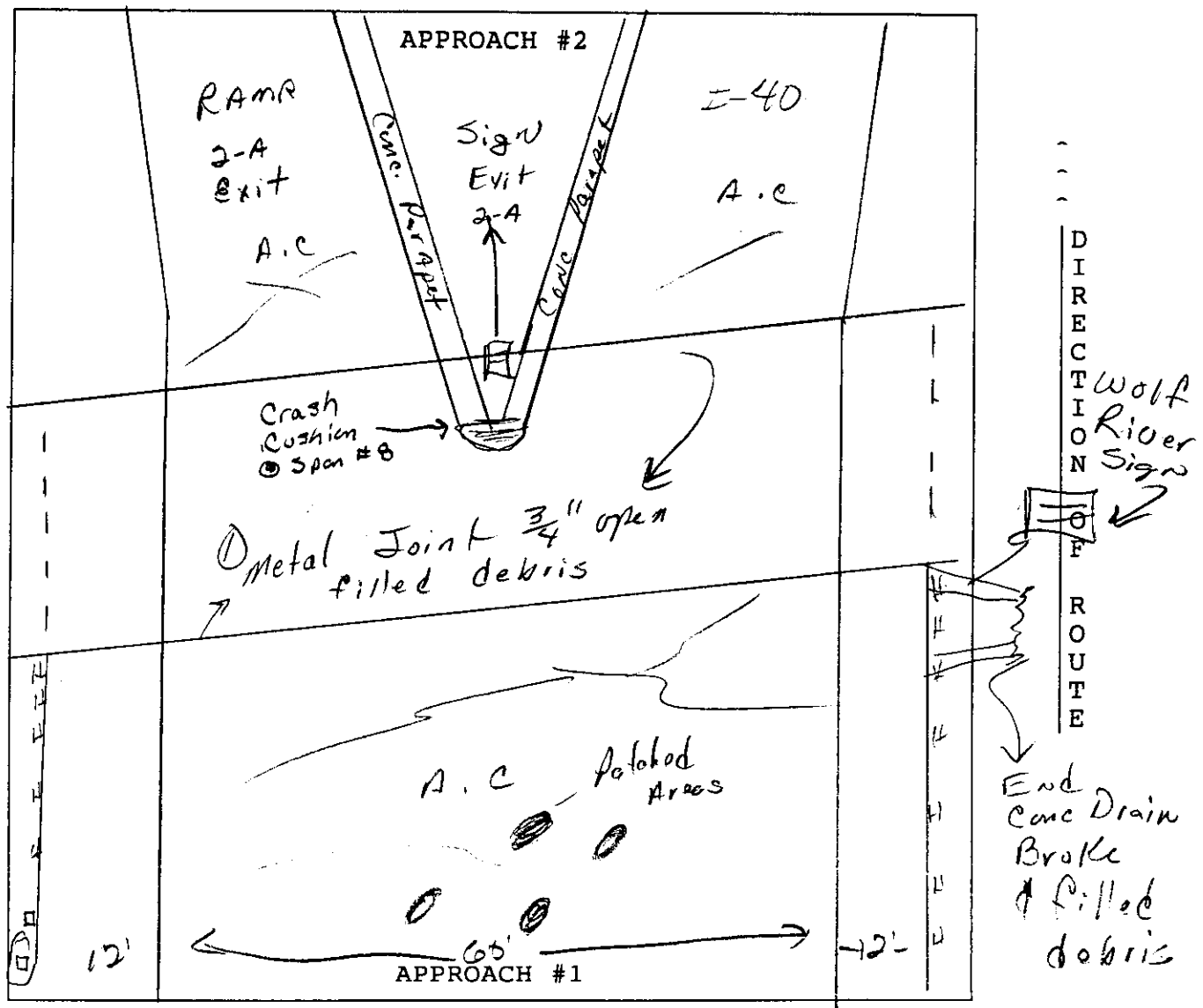
BRIDGE NO. 79 - 10040
CO. ROUTE

0509
L.M.

R
L/R

60
SKEW

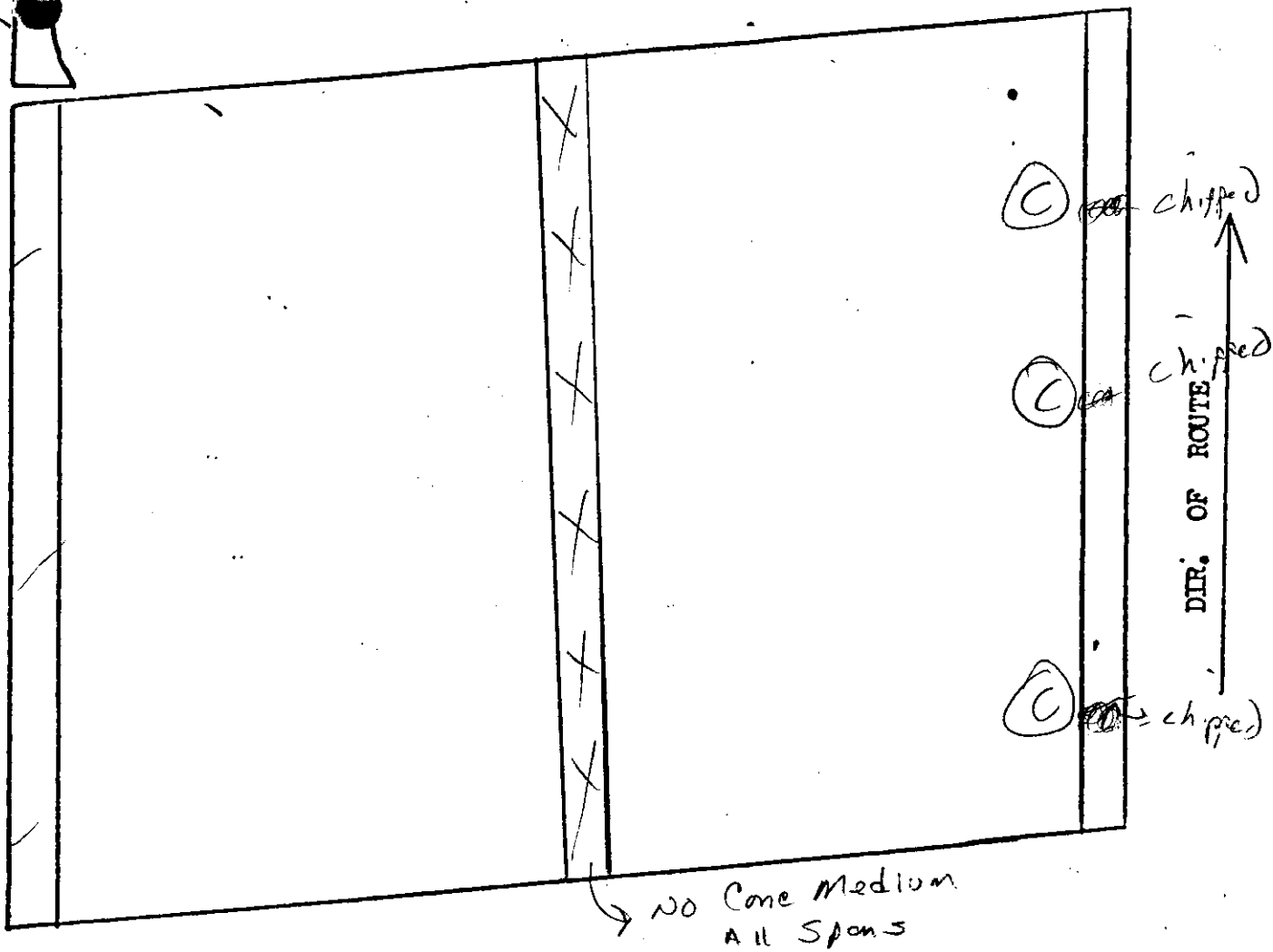
DATE: SEP 16 1999



ELEMENT	RATING				COMMENTS
ALIGNMENT	(G)	F	P	C	<u>patched Areas</u>
APPROACH PAVEMENT	G	(F)	P	C	<u>Fine Cracks/Traffic Abrasion</u>
APPROACH SLAB	G	F	P	C	<u>N/V</u>
APPROACH GUARDRAIL	(G)	F	P	C	<u>fine Cracks Approach</u>
Cone Parapet	(E)				
EMBANKMENT	G	F	P	C	
DRAINS	G	F	(P)	C	<u>Broke/Stopped-up Rt Approach</u>
APPROACH JOINT	G	(F)	P	C	<u>See (1)</u>
SIGNS	(G)	F	P	C	<u>Wolf River Sign</u>

BRIDGE NO. 79 I-40 5.09 SK. 60 RT. SPAN NO. 1
RT

SEP 16 1999



CK	G	(F)	P	C	Top Deck - Traffic Abre
PET	G	(F)	P	C	Parapet - Chipped places (C)
NS	G	F	(P)	C	Drain - No Drain
T	G	F	P	C	
ED	G	F	P	C	

RT

SEP 16 1999

BRIDGE NO. 79 I-40 5.09BENT NO. _____ SPAN NO. 1 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	(G) F P C	LOW PONDING
DIA.	G F P C	NONE
CONC. I. B.		
A B	(G) F P C	
C D	(G) F P C	
E F	(G) F P C	
G H	(G) F P C	
I J	(G) F P C	SEE #2
BACKWALLS	(G) F P C	FINE CRACKS
Exterior Concrete	(G) F P C	SEE #1

BR. NO. 29 T-40 309 SK. RT

SPAN NO. 1

SEP 16 1999

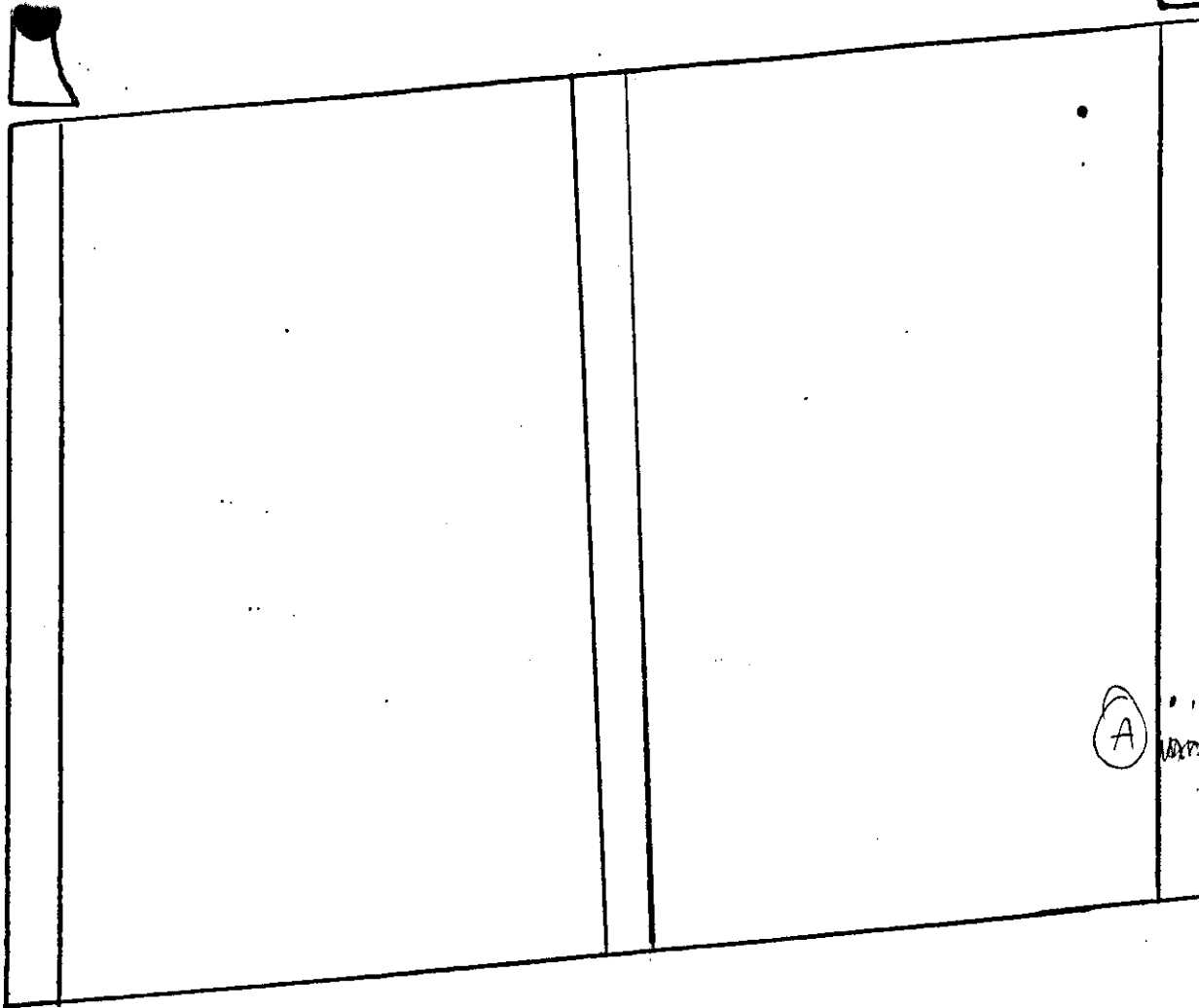
	A
	B
	C
	D
	E
	F
	G
	H
	I
	J

① Edith Quirk
Devices broke loose
NUT connected to
CIRCUIT About #1

②
FIREWORKS

BRIDGE NO. 79 I-40 5.09 SK. 60 RT. SPAN NO. 2
RT

SEP 16 1999



DIR. OF ROUTE

(A) Spalled

CK	G (F) P C	Normal Traffic Abr.
PET	G (F) P C	Five Cracks
NS	G (F) P C	Light Debris
T	G F P C	
RD	G F P C	
L		

SEP 16 1999

BENT NO. _____ SPAN NO. 2 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	(G) F P C	
DIA.	G F P C	
CONC. I. B.		
A B	(G) F P C	
C D	(G) F P C	File CD-12
E F	(G) F P C	
G H	(G) F P C	
I J	(G) F P C	
BACKWALLS	(G) F P C	

BR. NO. 79 T-40 5.09 SK.

RT

SPAN NO. 2
[Signature]

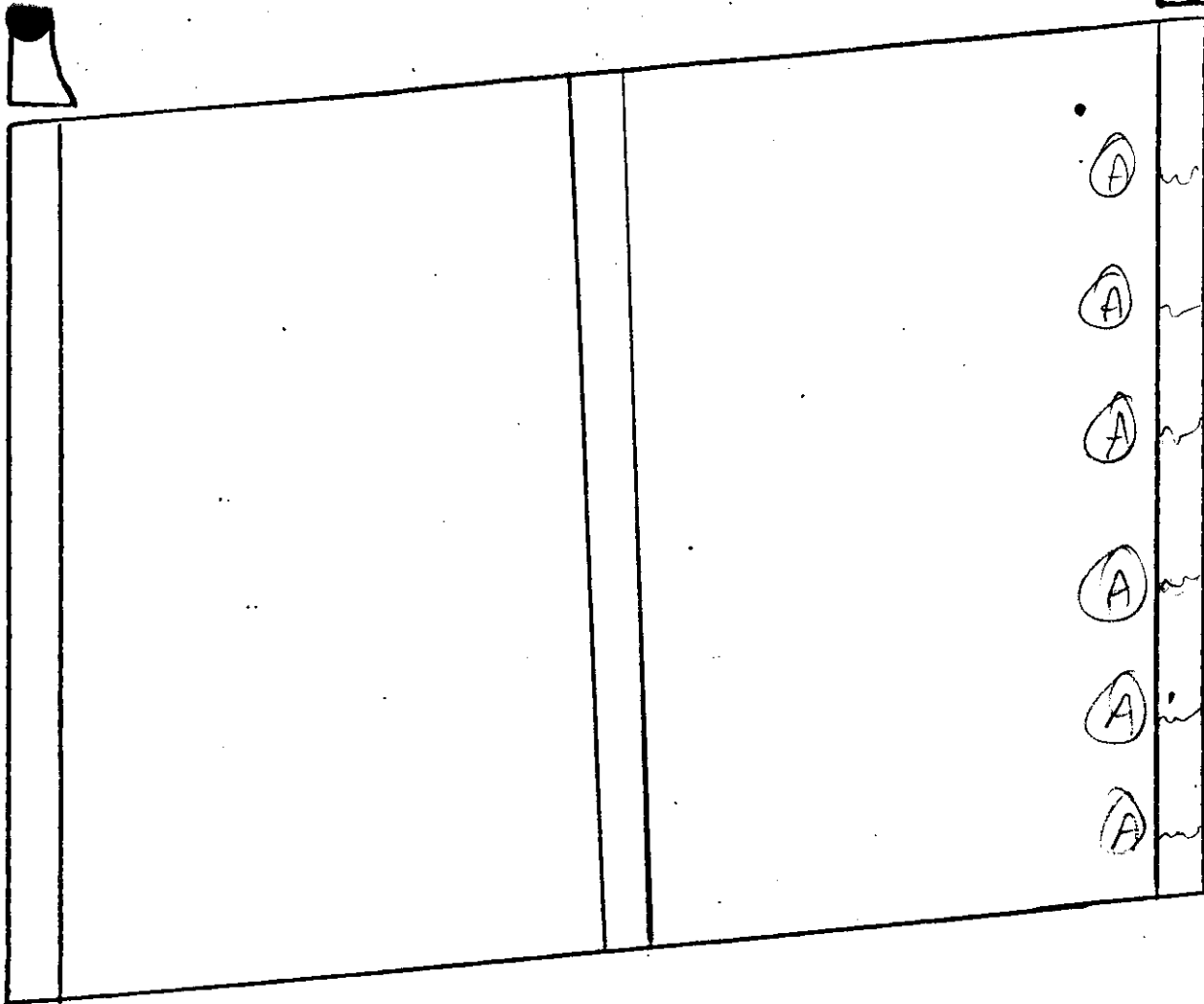
SEP 16 1999

A
B
C
<i>Five men</i>
D
E
F
G
H
I
J

BRIDGE NO. 29 I-410 5.09 SK. 60 RT. SPAN NO. 3

RT

SEP 16 1999



CK	G (F) P C	Normal Traffic Abra.
PET	G (F) P C	Fine cracks (A)
NS	G F (P) C	Stopped up
IT	G F P C	
RD	G F P C	
L		

R1

BENT NO. _____ SPAN NO. 3 ABT. NO. _____ PIER NO. _____

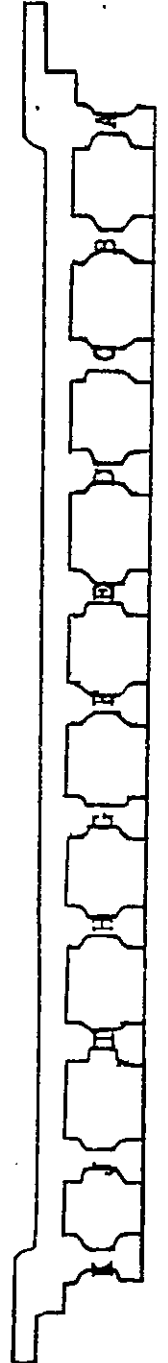
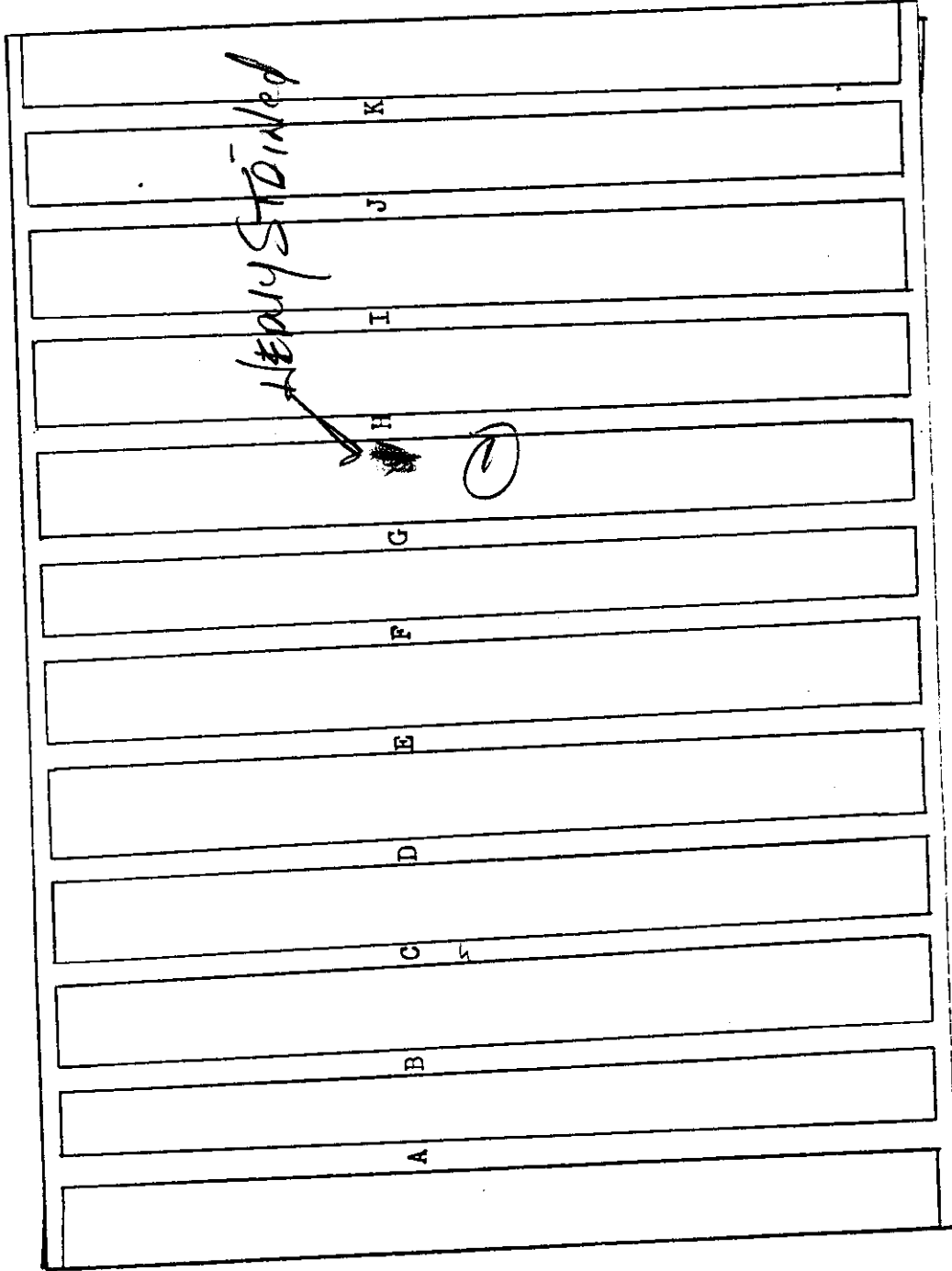
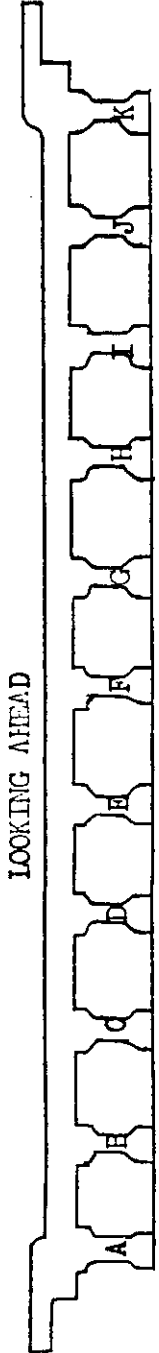
ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	SEE #1
CONC. I. BEAMS		
A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
J	G F P C	
K	G F P C	
DIA.	G F P C	
BACKWALLS	G F P C	

BR. NO. 79 I-40 5.09 SK. 60°^{RT}
RT

SPAN NO. 3

SEP 16 1999

MA

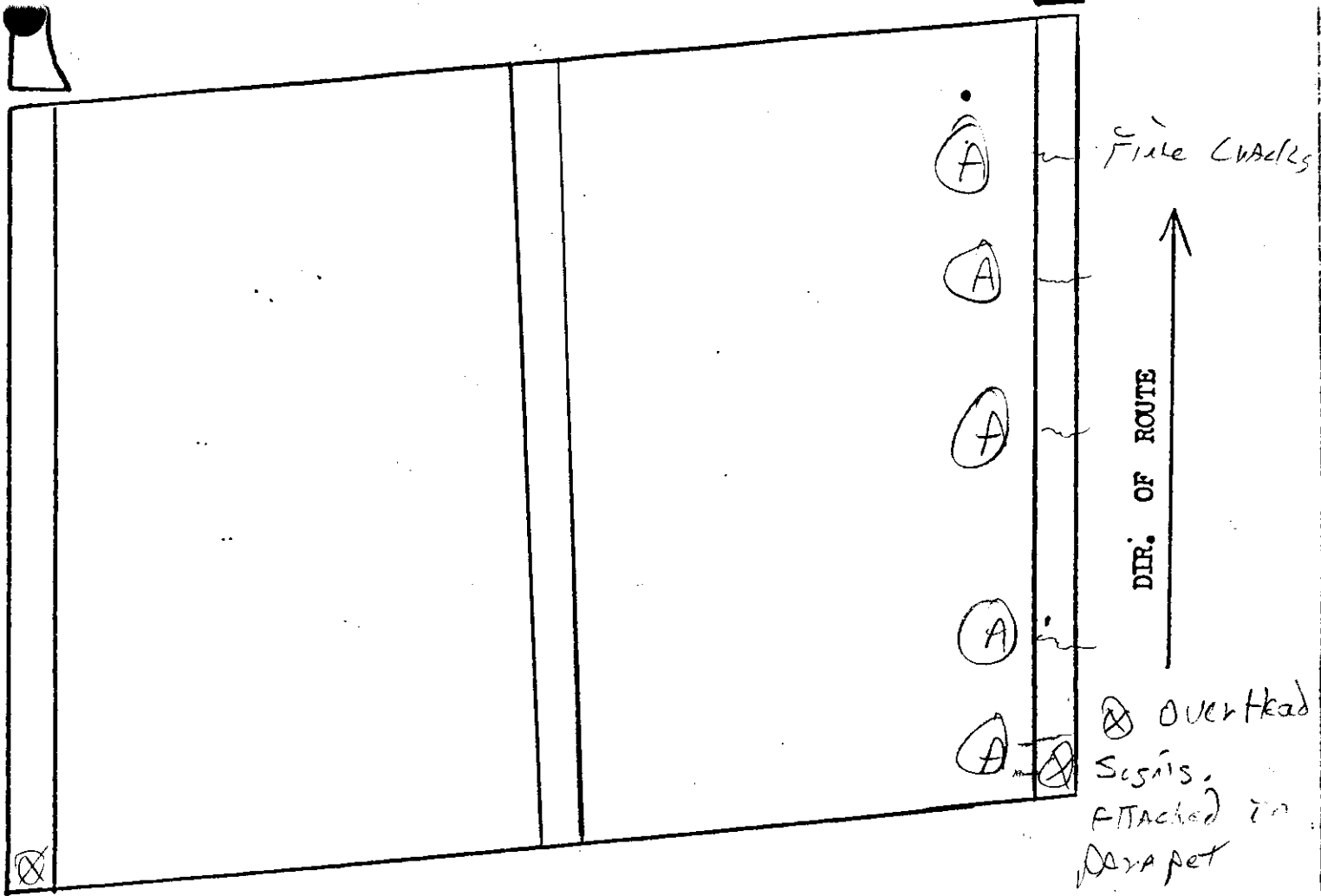


DIR. OF ROUTE

BRIDGE NO. 79 I-40 5.09 SK. 60° RT. SPAN NO. 4

RT

SEP 16 1999



CK G (F) P C Normal Traffic Abra.

PET G (F) P C Fine Cracks (A)

NS G F (P) C Stopped up

T G F P C

RD G F P C

L

RT

BRIDGE NO. 79 T-40 5.09

BENT NO. _____ SPAN NO. 7 ABT. NO. _____ PIER NO. _____

[illegible]

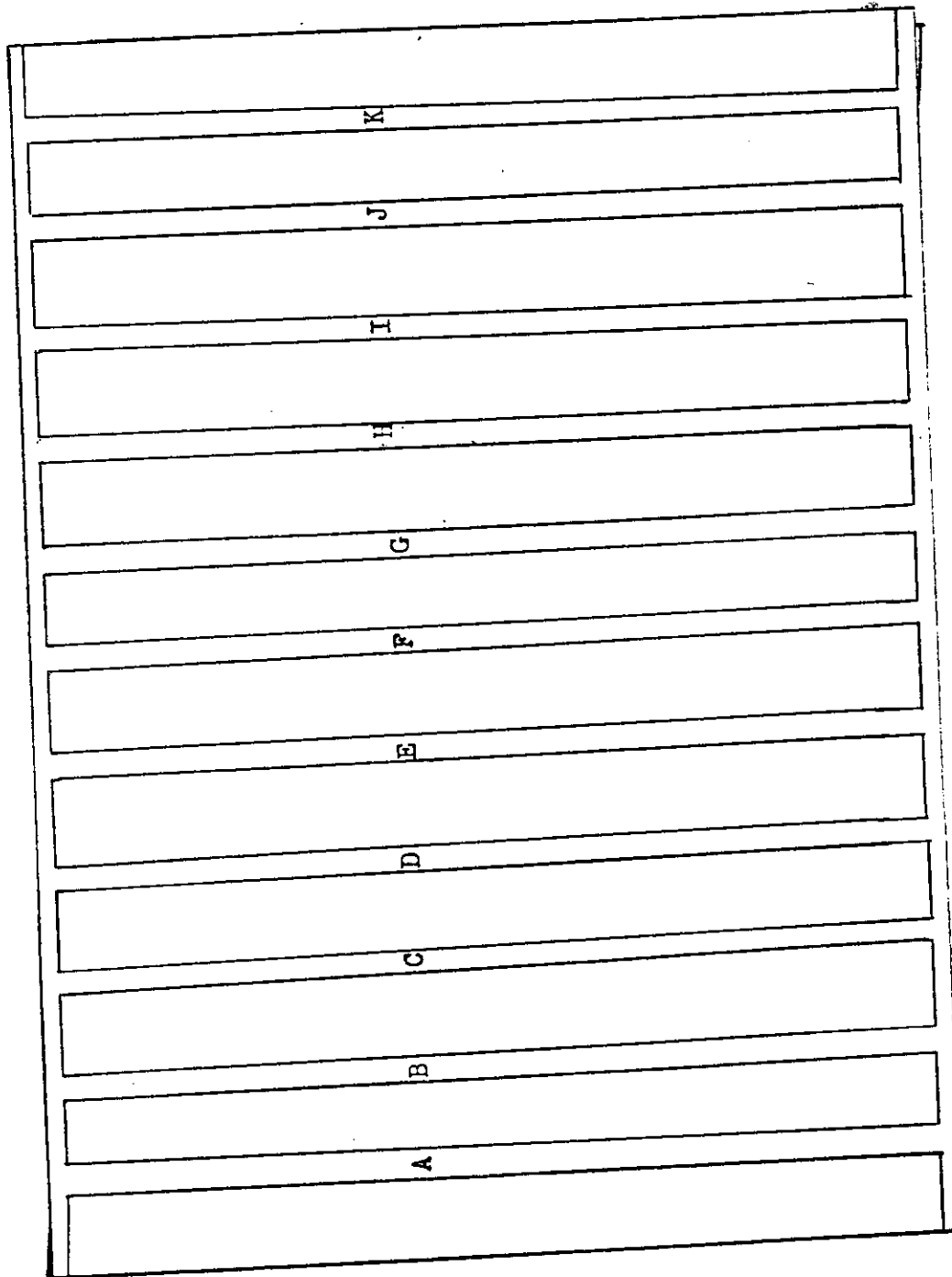
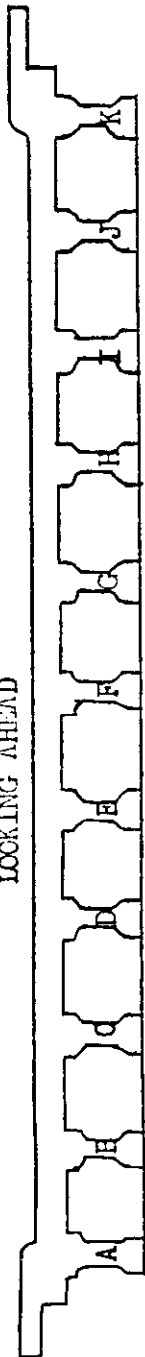
BR. NO. 79 T-48 5.09 SK. 60² 14

SPAN NO. 4

RT

SEP 16 1999

LOCKING AHEAD



DIR. OF ROUTE

LOCKING AHEAD



BRIDGE NO.

29T-HC509

SK.

60°

RT.

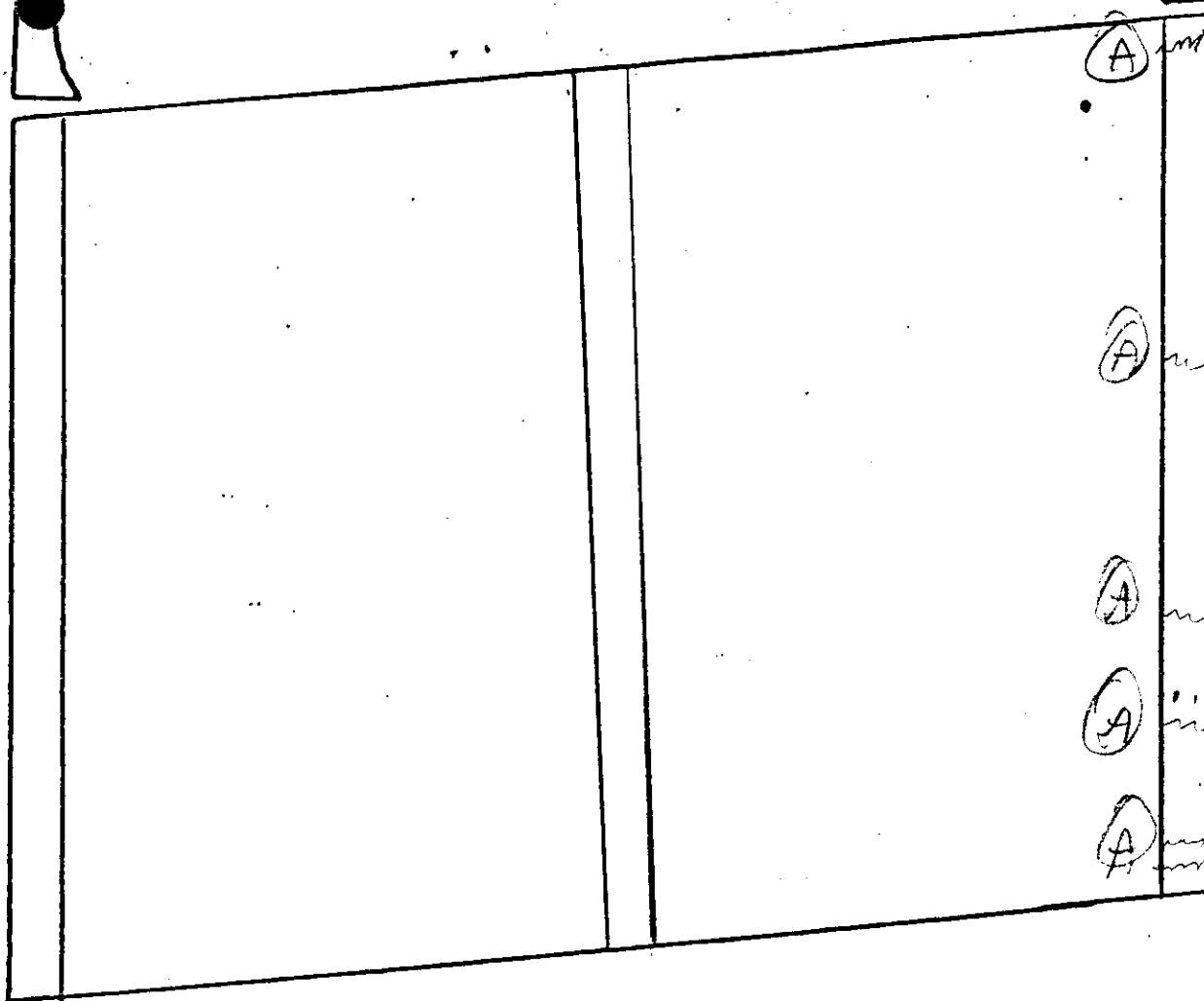
SPAN

NO.

5

RT

SEP 16 1999



FINE CRACKS

DIR. OF ROUTE

CK G (F) P C Normal Traffic Abon.

PET G (F) P C Fine Cracks (A)

NS G F (P) C Stopped up.

T G F P C

RD G F P C

L

RT

SEP 16 1999

BRIDGE NO. 79 T-40 5.09BENT NO. _____ SPAN NO. 5 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	
CONC. I. BEAMS	G F P C	
A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
J	G F P C	
K	G F P C	
L	G F P C	
M	G F P C	
DIA.	G F P C	
BACKWALLS	G F P C	

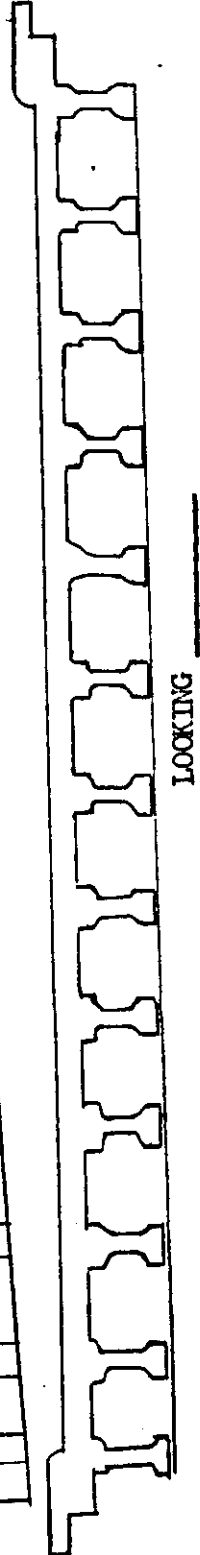
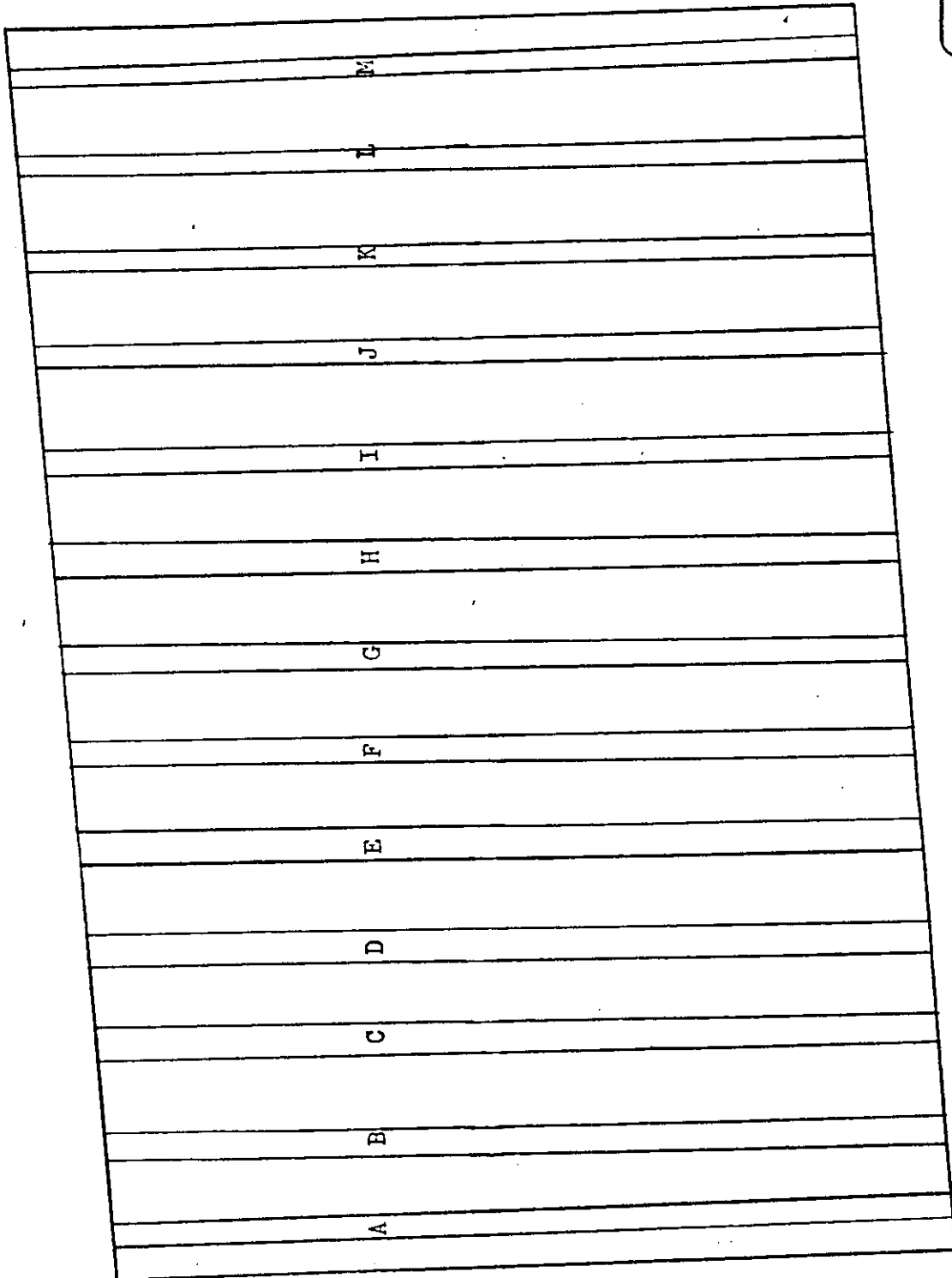
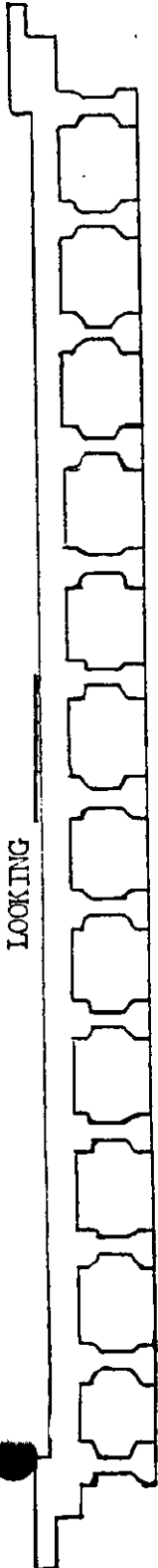
BR. NO. 79 140 5.09 SK

SPAN NO. 5

RT

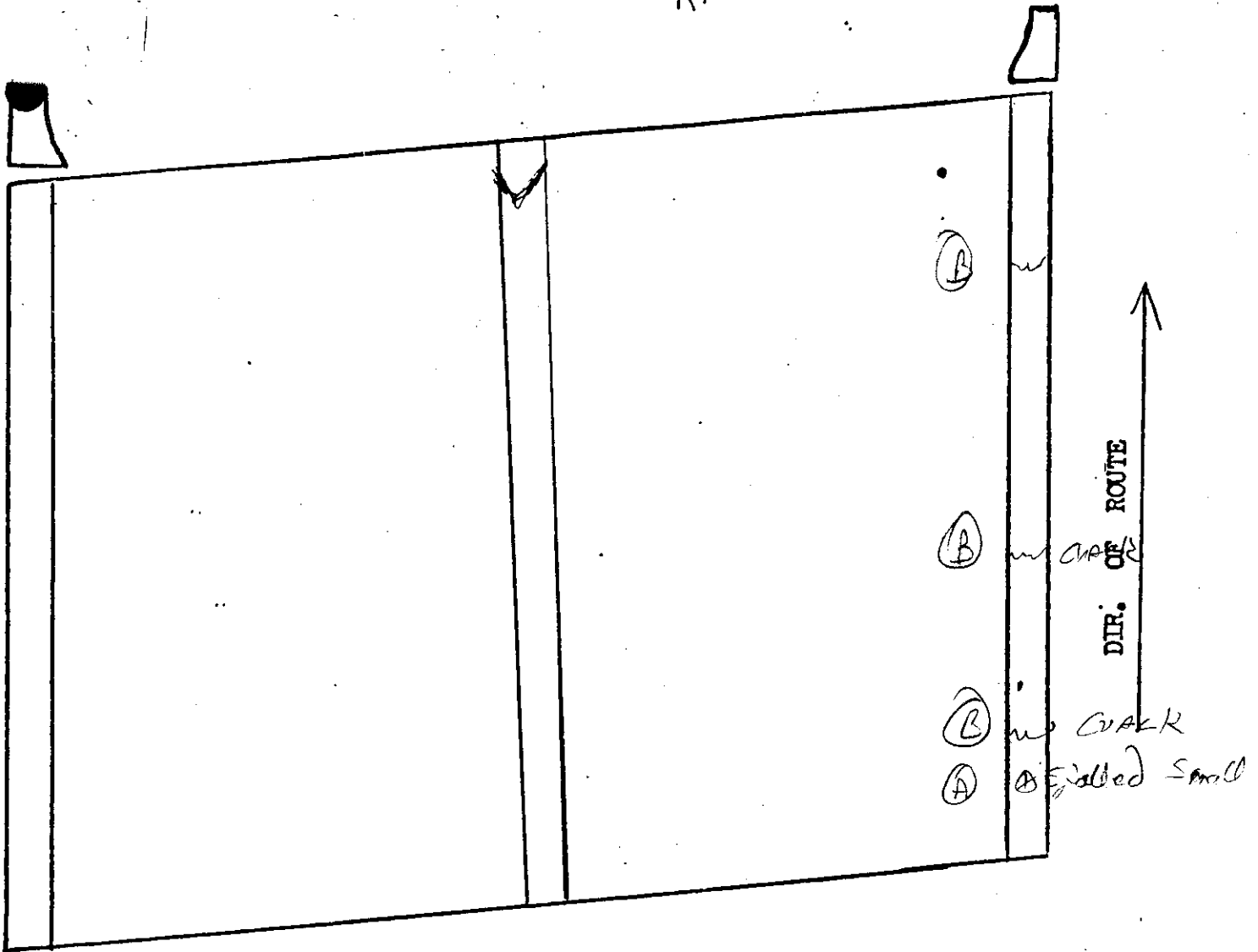
SEP 16 1999

← DIR. OF ROUTE



SEP 16 1999

BRIDGE NO. 79 T-110 5.09 SK. 60° RT. SPAN NO. 6
RT



CK	G	(F)	P	C	Normal Traffic Abre
PET	G	(F)	P	C	Fine Cracks & Spalled (B)
NS	G	(F)	P	C	
IT	G	F	P	C	
RD	G	F	P	C	
L					

RT

SEP 16 1999

BRIDGE NO. 79 I-40 5.09BENT NO. _____ SPAN NO. 6 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	(G) F P C	
CONC. I. BEAM		
A	(G) F P C	
B	(G) F P C	
C	(G) F P C	
D	(G) F P C	
E	(G) F P C	
F	(G) F P C	
G	(G) F P C	
H	(G) F P C	
I	(G) F P C	
J	(G) F P C	
K	(G) F P C	
L	(G) F P C	
M	(G) F P C	
N	(G) F P C	
BACKWALLS	(G) F P C	
DIA.	(G) F P C	

BRIDGE NO. 79 E-40 5.09

RT
SPAN NO. 6

SKEW 60° RT.

SEP 16 1999

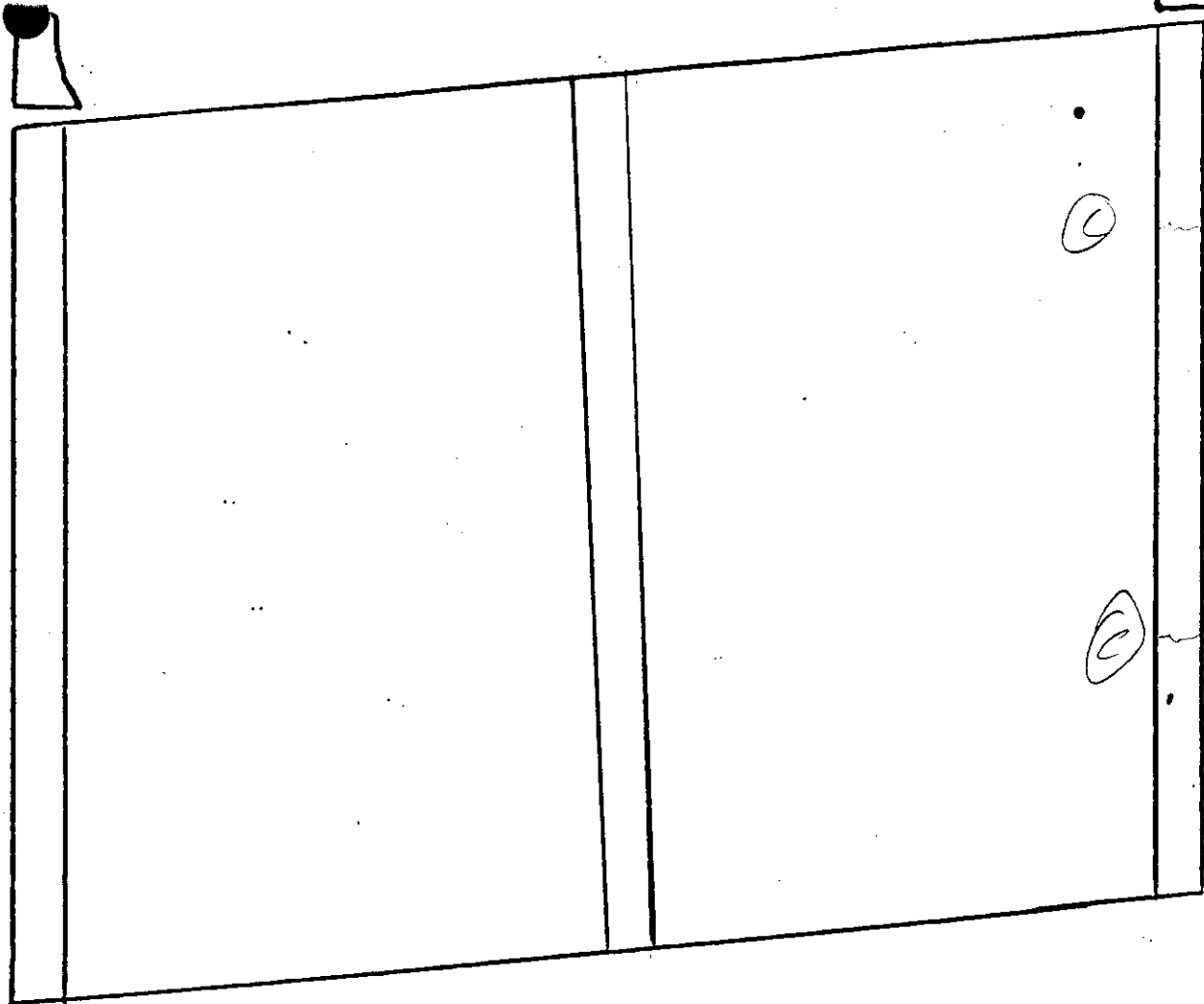
← DIR. OF ROUTE

LOOKING AHEAD

LOOKING BACK

BRIDGE NO. 7.9 I-40 5.09 SK. 60° RT. SPAN NO. 7
RT

SEP 16 1999



DIR. OF ROUTE

DECK	G (F) P C	Normal Traffic Abn.
APET	G (F) P C	Small Cracks (C)
INS	G (F) P C	
WT	G F P C	
RD	G F P C	
L		

RT

SEP 16 1999

BRIDGE NO. 79 I-40 S.09BENT NO. _____ SPAN NO. 7 ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	<u>G</u> F P C	
CONC. I. BEAMS		
A B C	<u>G</u> F P C	
_____	G F P C	
D E F	<u>G</u> F P C	
_____	G F P C	
G H I	<u>G</u> F P C	
_____	G F P C	
J K L	<u>G</u> F P C	
_____	G F P C	
M N O	<u>G</u> F P C	
_____	G F P C	
P	<u>G</u> F P C	
BACKWALLS	<u>G</u> F P C	
DIA.	G F P C	

BR. NO. 79 T-40 5.09 SPAN NO. 7 SEP 16 1999
RT SK. 60 RT.

	A	
	B	
	C	
	D	
	E	
	F	
	G	
	H	
	I	
	J	
	K	
	L	
	M	
	N	
	O	
	P	

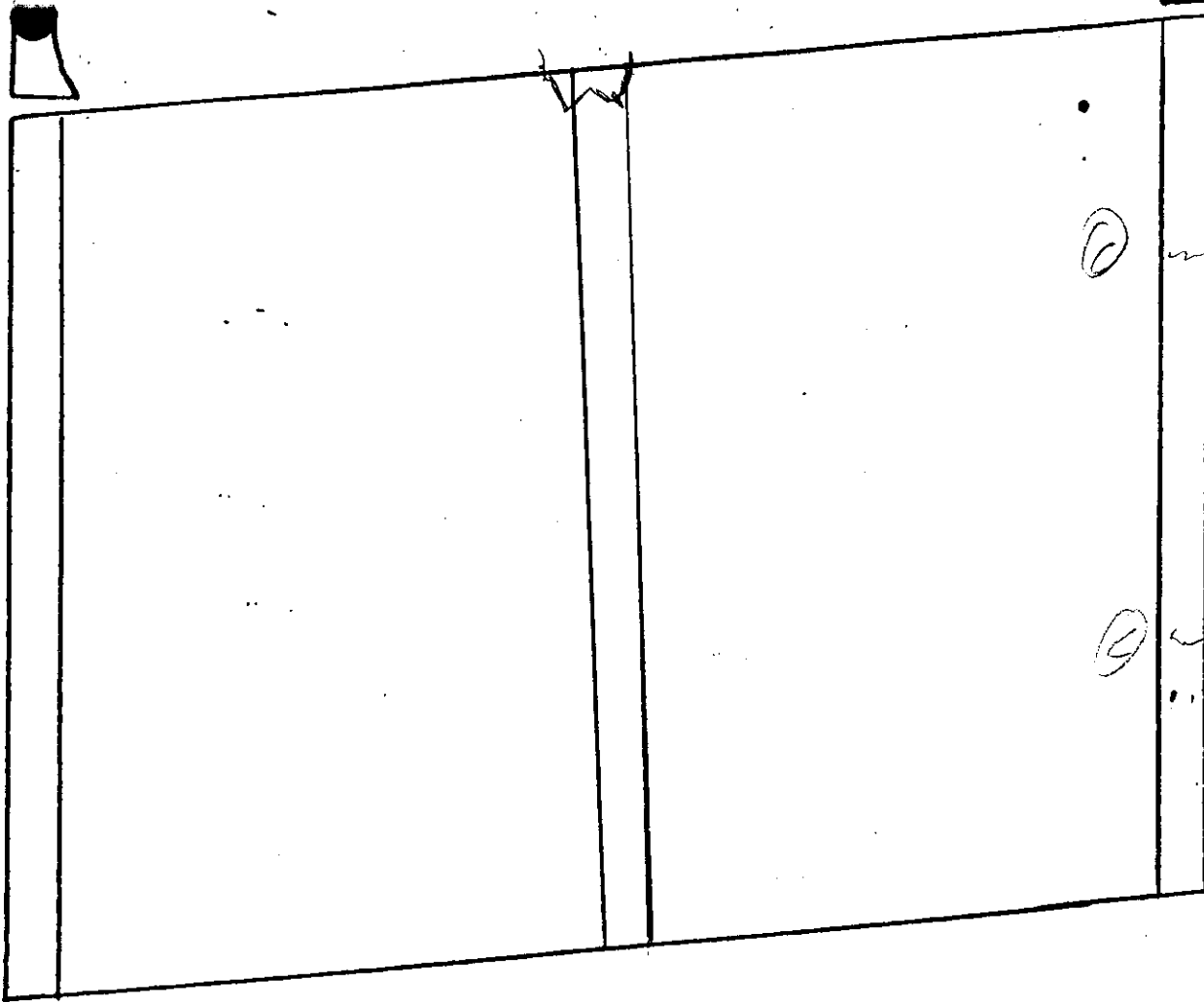
LOOKING

LOOKING

DIR. OF ROUTE

BRIDGE NO. 79 I-40 5.09 SK. 60 RT. SPAN NO. 8

SEP 16 1999



DIR. OF ROUTE

CK	G	(F)	P	C	Normal Traffic Abra
PET	G	(F)	P	C	Small Cracks
NS	G	(F)	P	C	
T	G	F	P	C	
RD	G	F	P	C	
L					

RT

SEP 16 1999

BRIDGE NO. 79 I-40 5.09

BENT NO. _____ SPAN NO. _____ ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	<u>G</u> F P C	
CONC. I. BEAMS		
A B C	<u>G</u> F P C	
_____	G F P C	
D E F	<u>G</u> F P C	
_____	G F P C	
G H I	<u>G</u> F P C	
_____	G F P C	
J K L	<u>G</u> F P C	
_____	G F P C	
M N O	<u>G</u> F P C	
_____	G F P C	
P - Q	<u>G</u> F P C	
BACKWALLS	<u>G</u> F P C	
DIA.	G F P C	

BH. NO. 79 I-40 5.09 SK. 60°Rt SPAN NO. 8
SEP 16 1999 RT

M

LOOKING

LOOKING

A

B

C

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E

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N

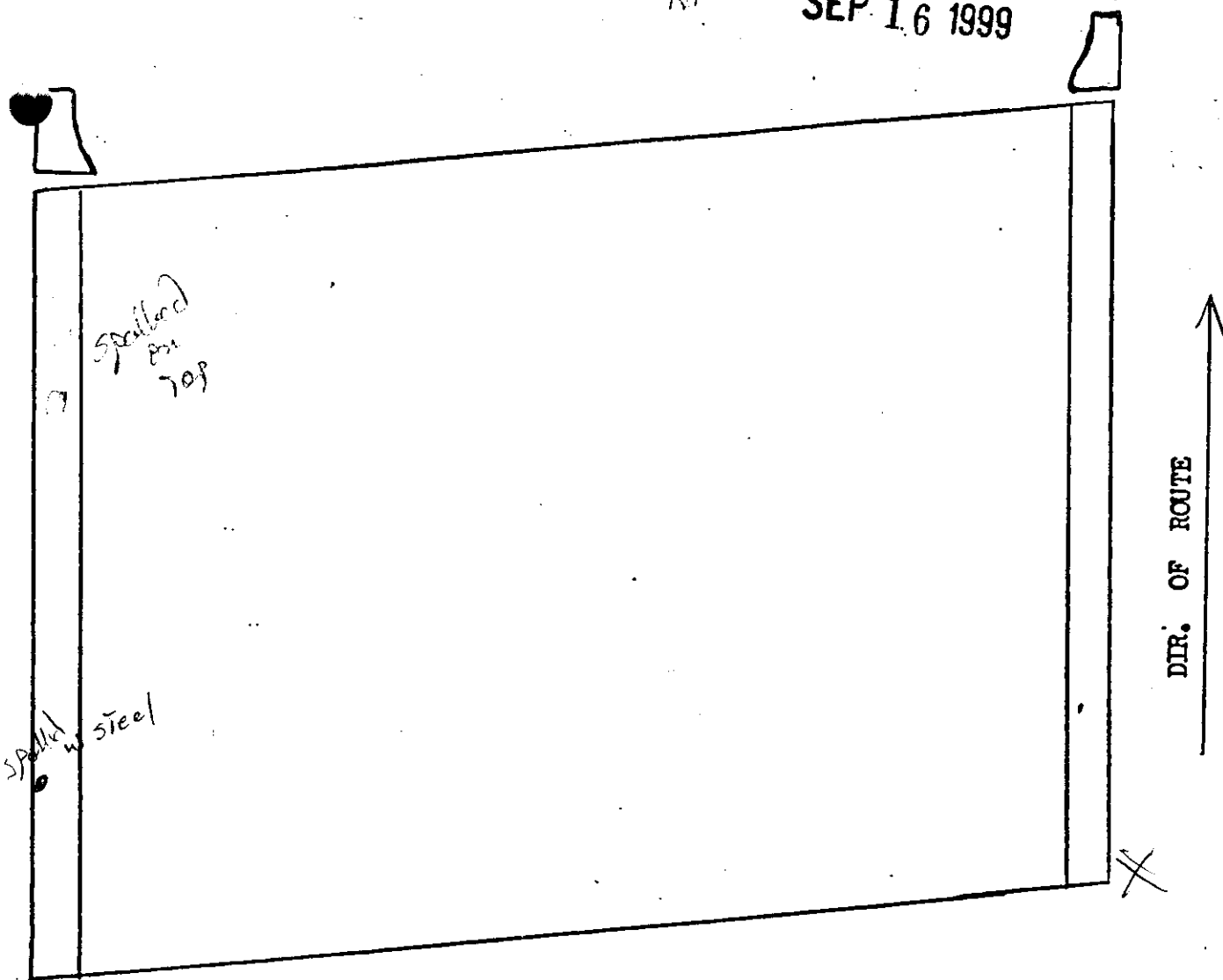
O

P

Q

— DIR. ROUTE —→

BRIDGE NO. 29 I-40 5.89 SK. 60' RT. SPAN NO. 9-A
 RT SEP 16 1999



DECK	G (F) P C	Normal Traffic Abut.
RAPET	G (F) P C	Spalled w/ steel (small) + Spalled top
AINS	G (F) P C	
INT	G F P C	

RT

SEP 16 1999

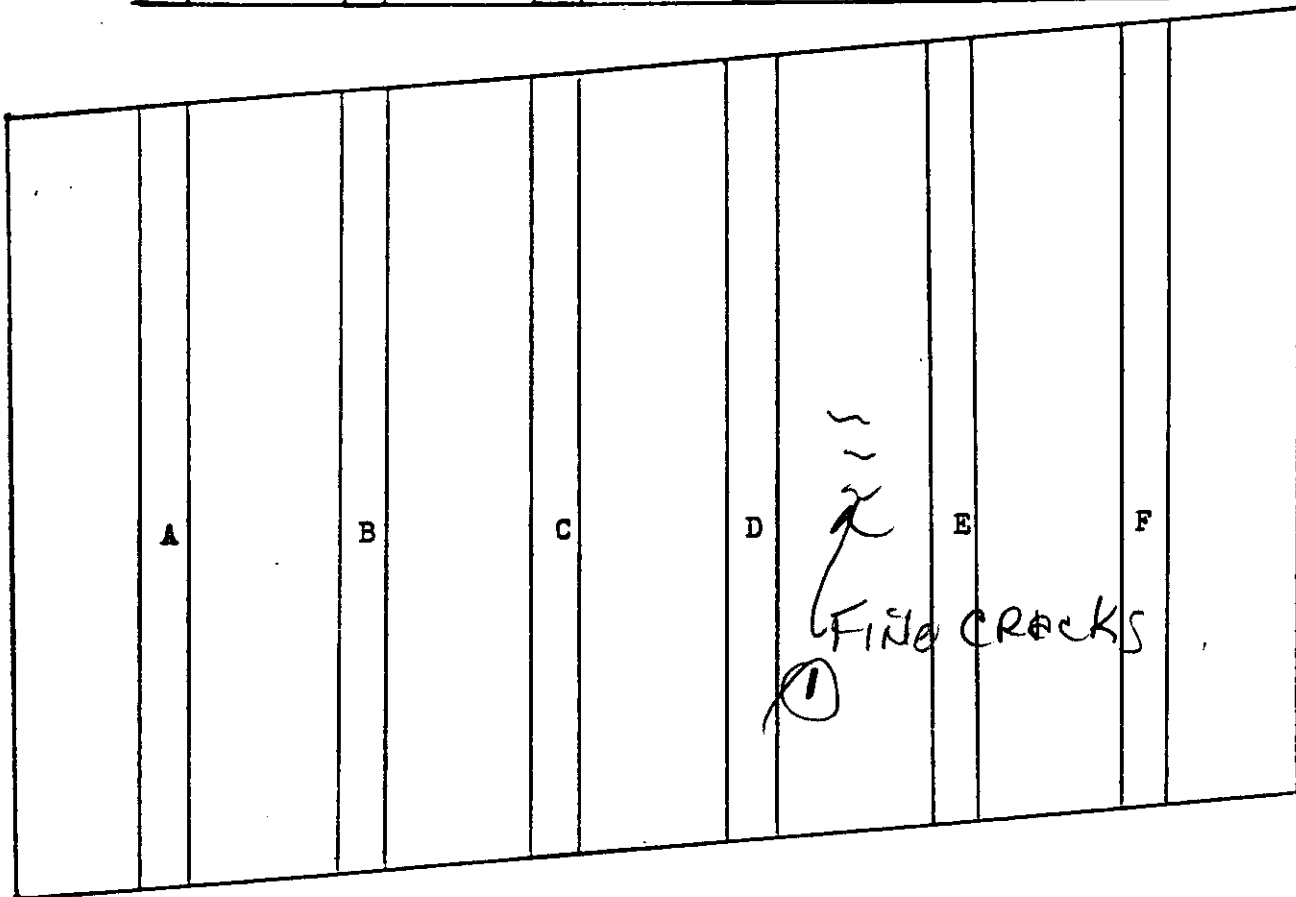
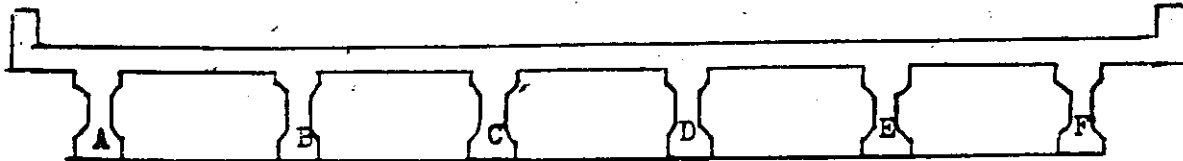
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Two 5.09^{ft}

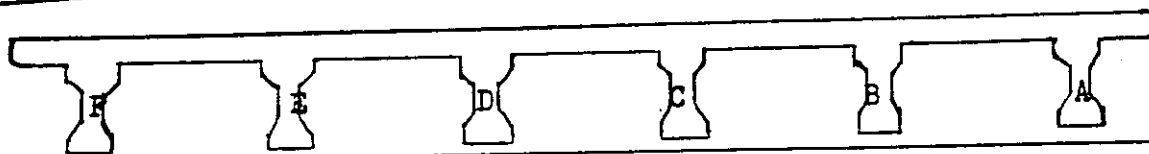
SK. 60 RT.

SPAN NO.

9-A



↑
DIR. OF ROUTE



ELEMENT

RATING

COMMENT

BOTTOM DECK

G F P C

CONC. PANELS SEE #1

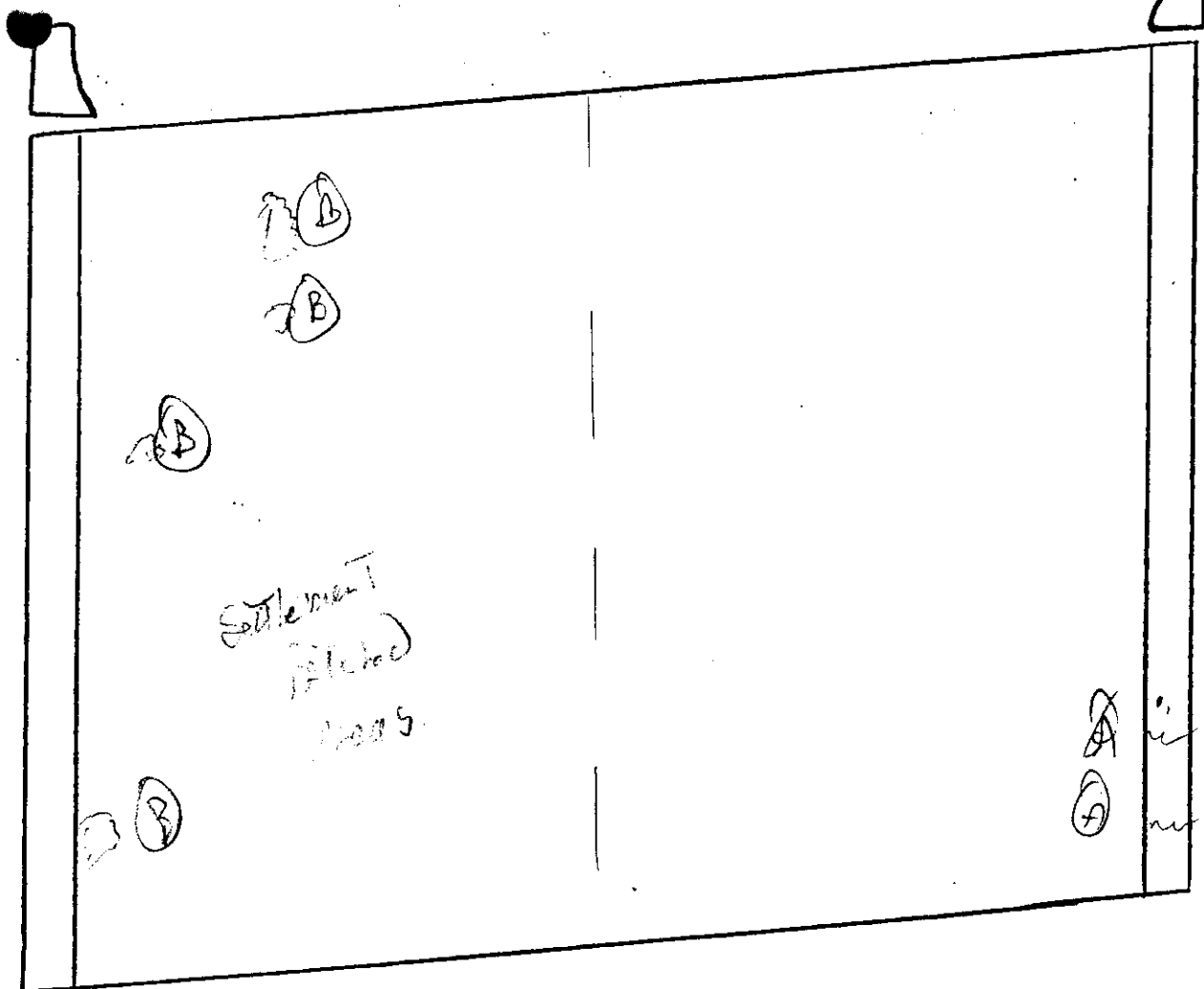
CONC. I. BEAMS

A G F P C
B G F P C
C G F P C
D G F P C
E G F P C
F G F P C

DIA.

G F P C

BRIDGE NO. 29 I-40 5.09 SK. 60° RT. SPAN NO. 9-B
 RT
 SEP 16 1999



DECK	G (F) P C	Settlement Patches Areas (B)
APET	G (F) P C	Five Cracks (A)
INS	G (F) P C	
NT	G F P C	

$$R_1$$
BRIDGE NO. 79 I-40 5.09

BENT NO. _____ SPAN NO. 9-B ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	Conc. Panels
CONC. I . BEAM A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
DIA.	G F P C	
BACKWALLS .	G F P C	

Rt. Lamp

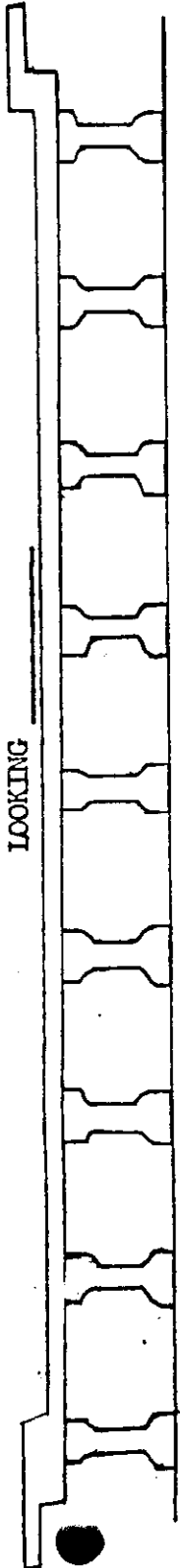
BR. NO. 79 Two 5.09

SK. 60° Lt SPAN NO. 9-B

RT

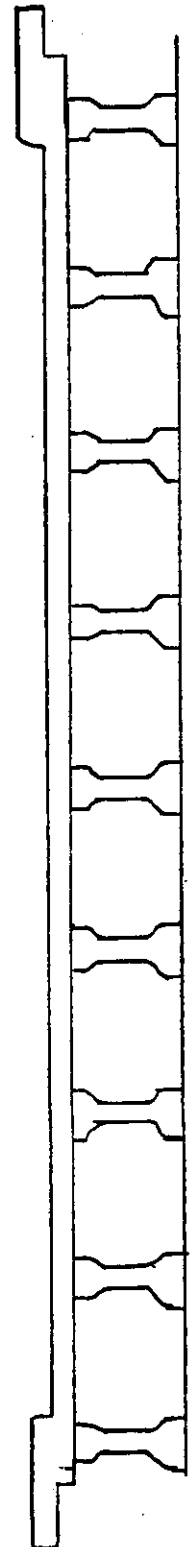
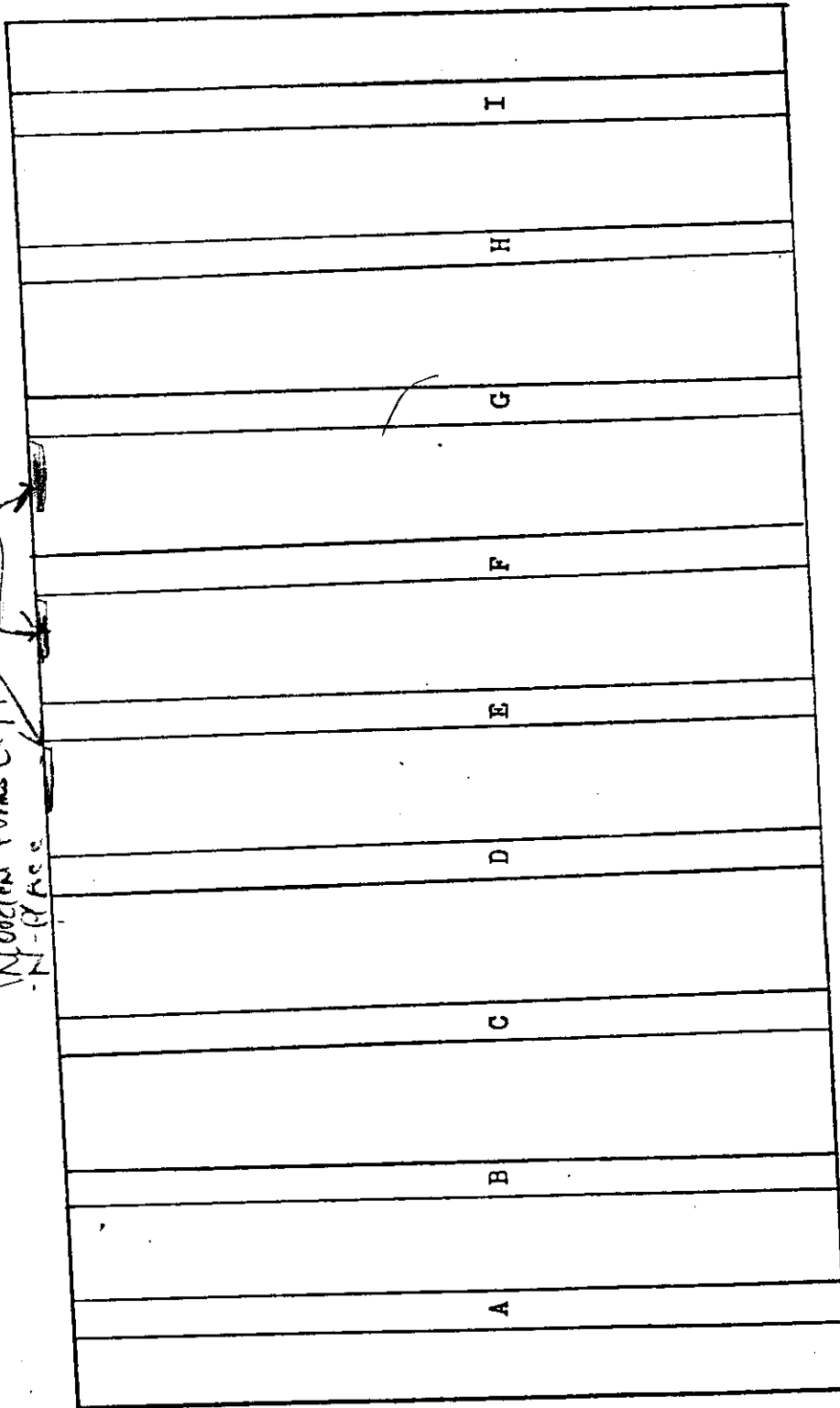
SEP 16 1999

← DIR. OF ROUTE



LOOKING

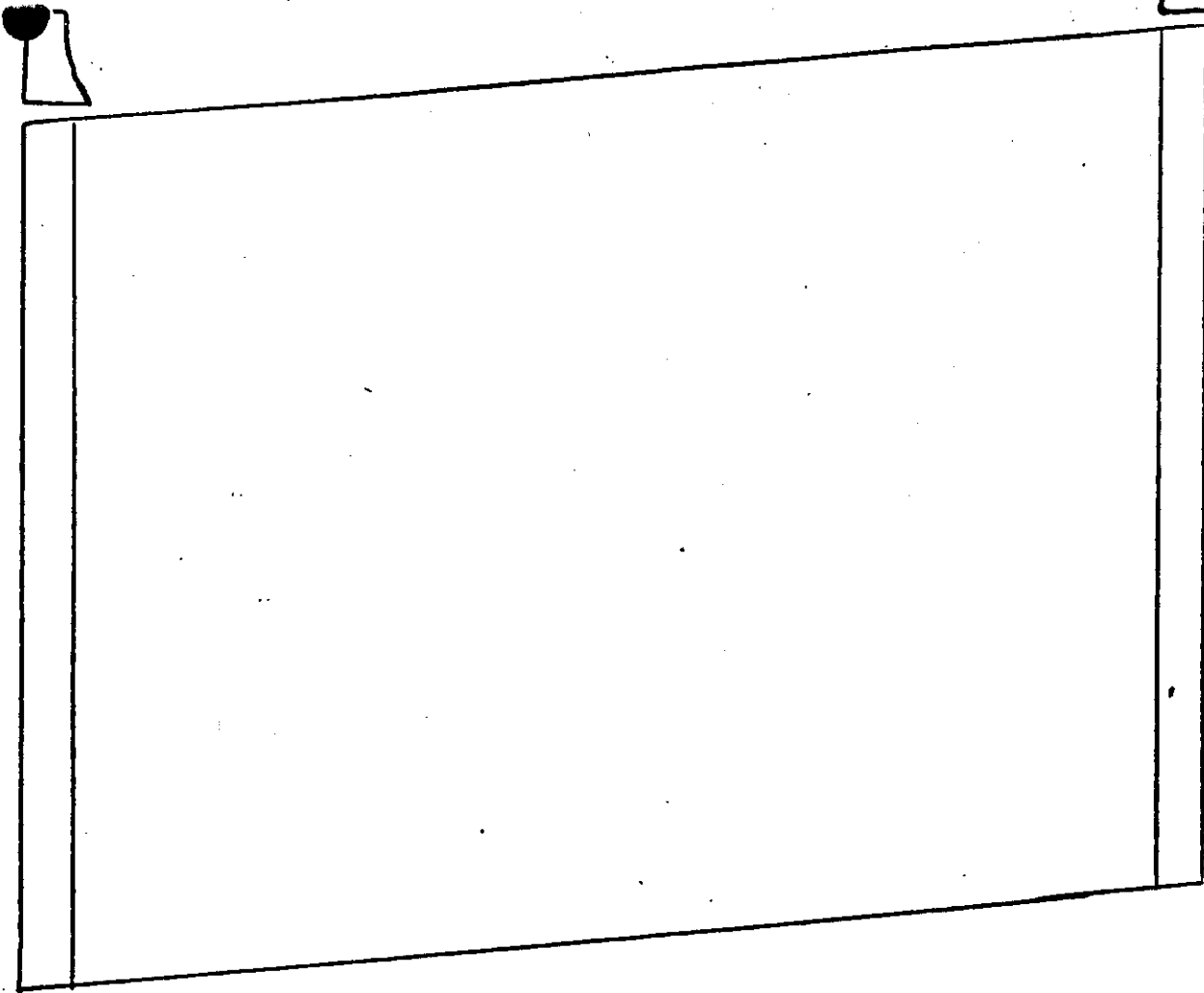
Wooden Forms Left
- 2-4' Acc



LOOKING

BRIDGE NO. 79 I-410 5.09 SK. 60° RT. SPAN NO. 10-A
RT

SEP 16 1999



DIR. OF ROUTE

DECK	G	(F)	P	C	Normal Traffic Abra.
APET	G	(F)	P	C	
INS	G	F	(P)	C	Drain Stopped up
NT	G	F	P	C	

BR. NO.

79I-405.09⁴SK. 60

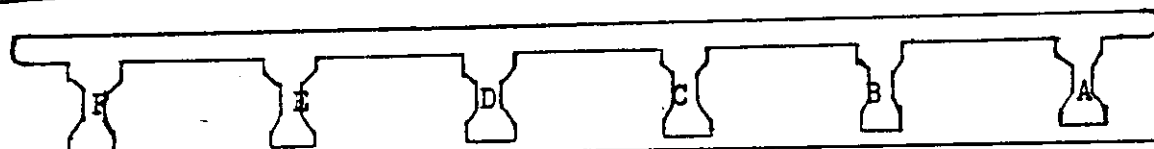
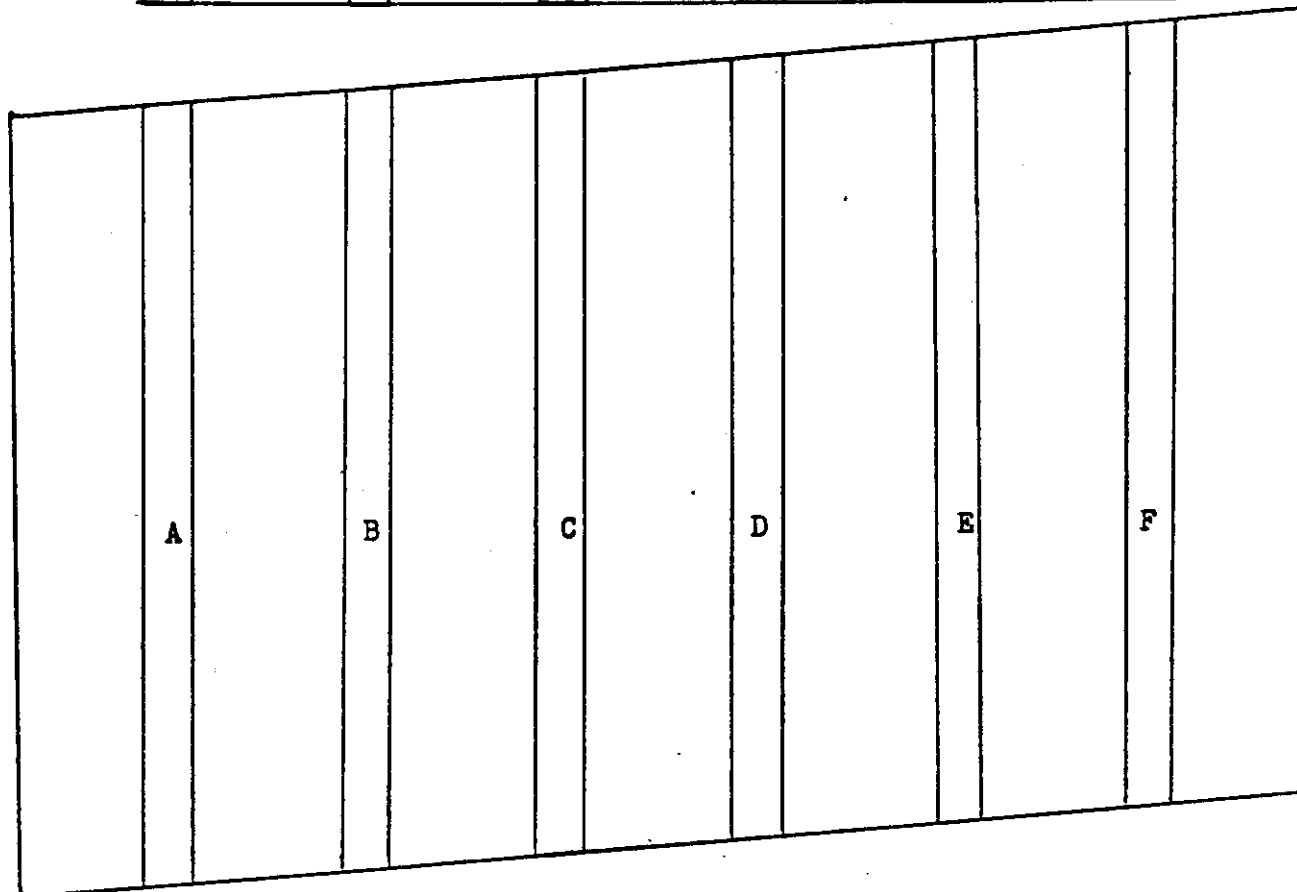
RT.

SPAN NO.

10-A

SEP 16 1999

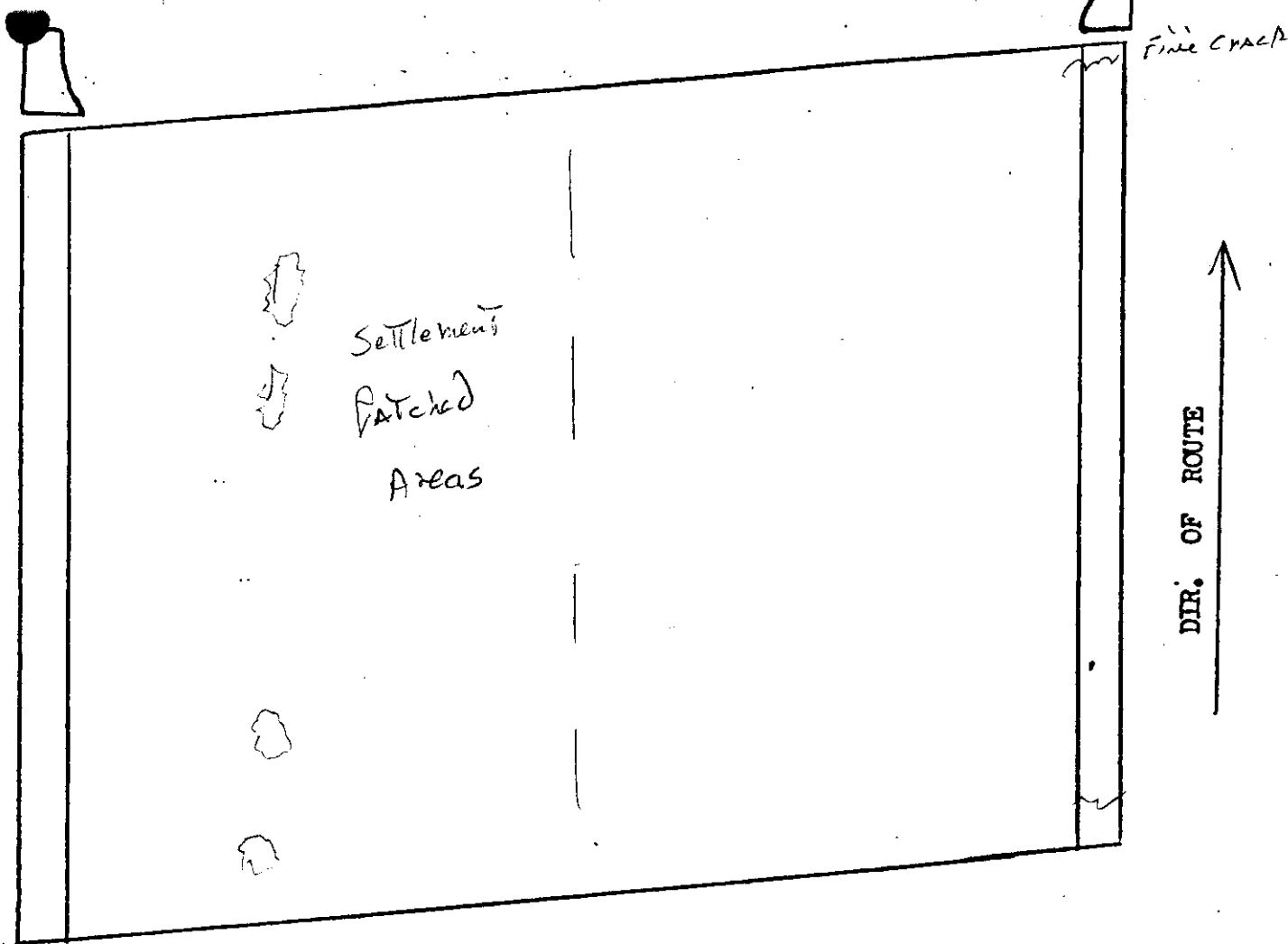
RT



ELEMENT	RATING	COMMENT
BOTTOM DECK	G F P C	
CONC. I. BEAMS	A G F P C	
	B G F P C	
	C G F P C	
	D G F P C	
	E G F P C	
	F G F P C	
DIA.	G F P C	

BRIDGE NO. 79 I-40 5.09^{RT} SK. 60' RT. SPAN NO. 10-B

SEP 16 1999



ECK	G (F) P C	Normal Abra - Settlement Patched Area
APET	G (F) P C	Fine Cracked
INS	G (F) P C	
NT	G F P C	

SEP 16 1999

BRIDGE NO. 79 I-40 5.09

BENT NO. _____ SPAN NO. 10-B ABT. NO. _____ PIER NO. _____

ELEMENT	RATING	COMMENTS
BOTTOM DECK	G F P C	
CONC. I . BEAM A	G F P C	
B	G F P C	SEE #11
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	
H	G F P C	
I	G F P C	
DIA.	G F P C	
BACKWALLS .	G F P C	

Rt. Kamp

BR. NO. 79 T210 5.09

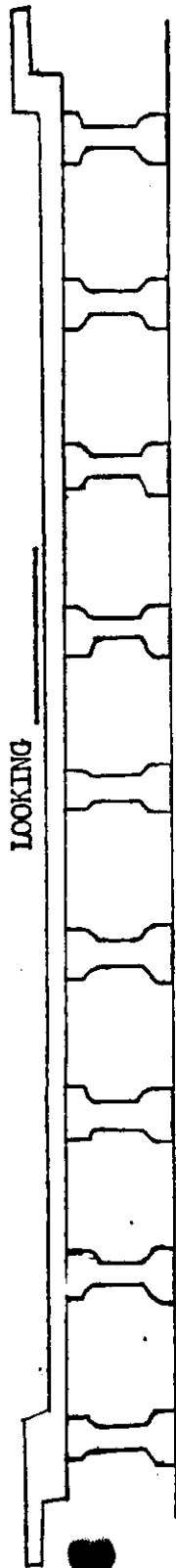
SK.

SPAN NO. 10 B

RT

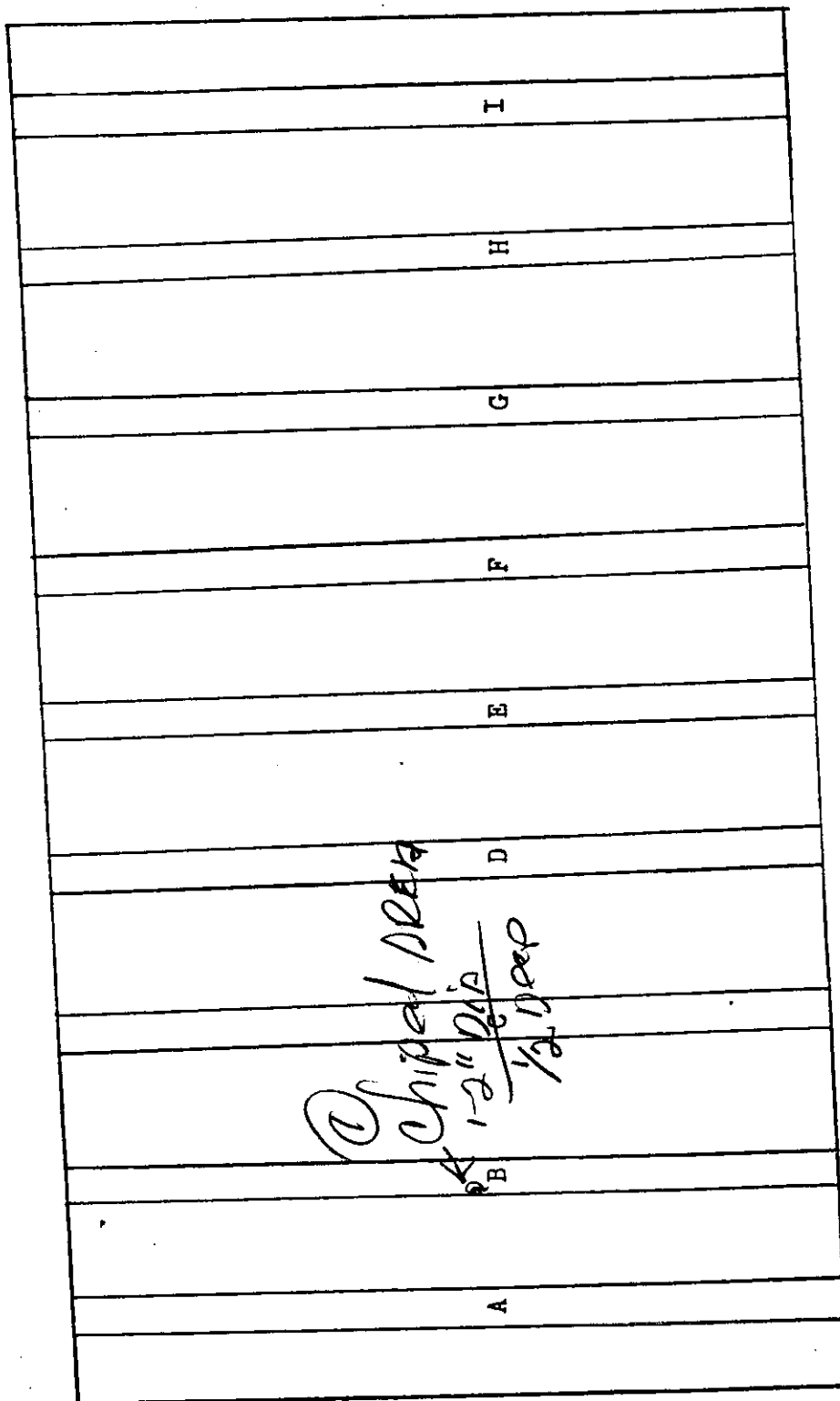
SEP 16 1999

← DIR. OF ROUTE



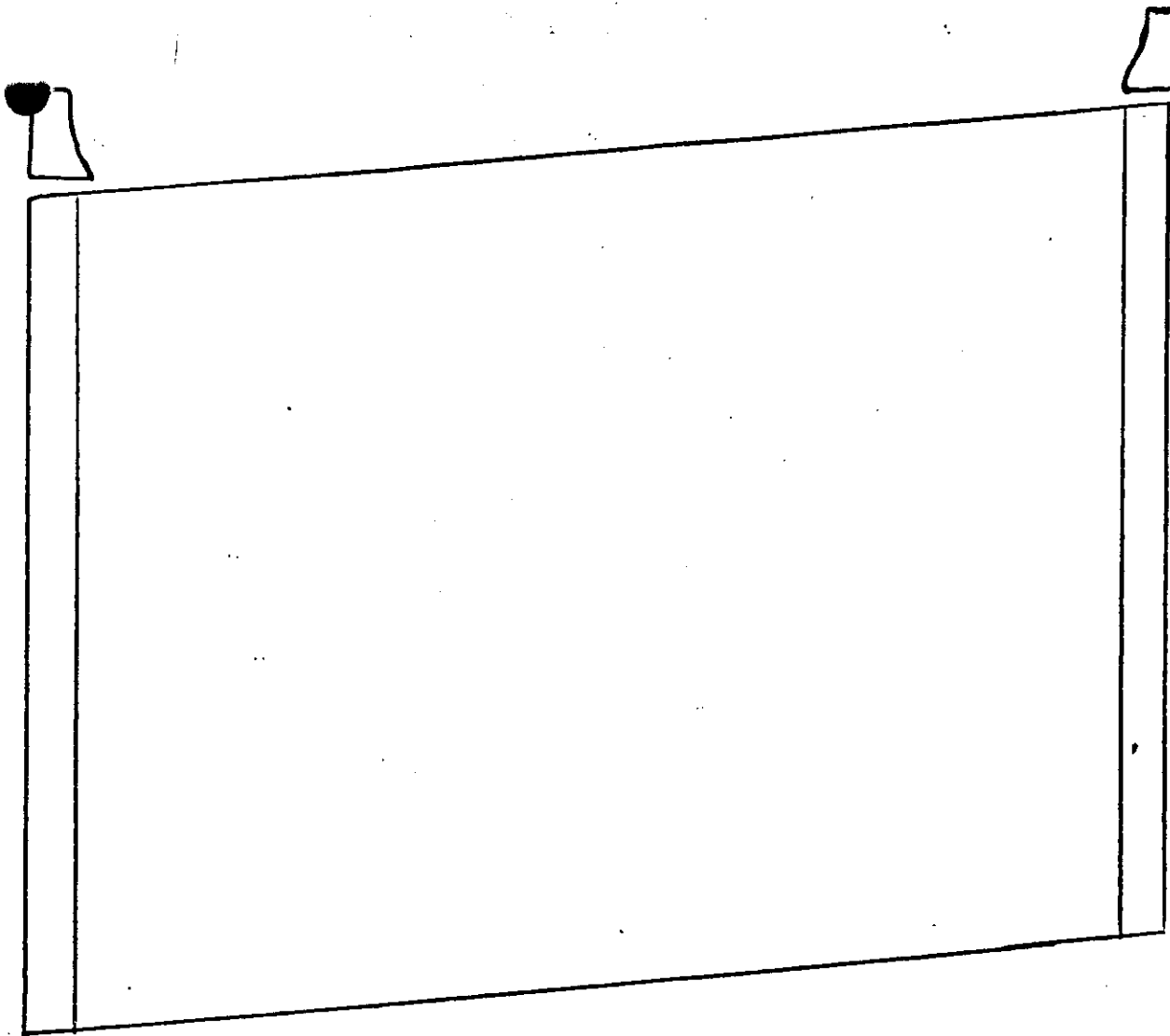
LOOKING

LOOKING



LOOKING

BRIDGE NO. 79 I-40 5.09 ^{RT} SK. 60' RT. SPAN SEP 16 1999 NO. 11-A



DIR. OF ROUTE

DECK	G	(F)	P	C	Normal Abra.
PAVET	G	(F)	P	C	
WINS	G	(F)	P	C	
NT	G	F	P	C	

BR. NO.

79

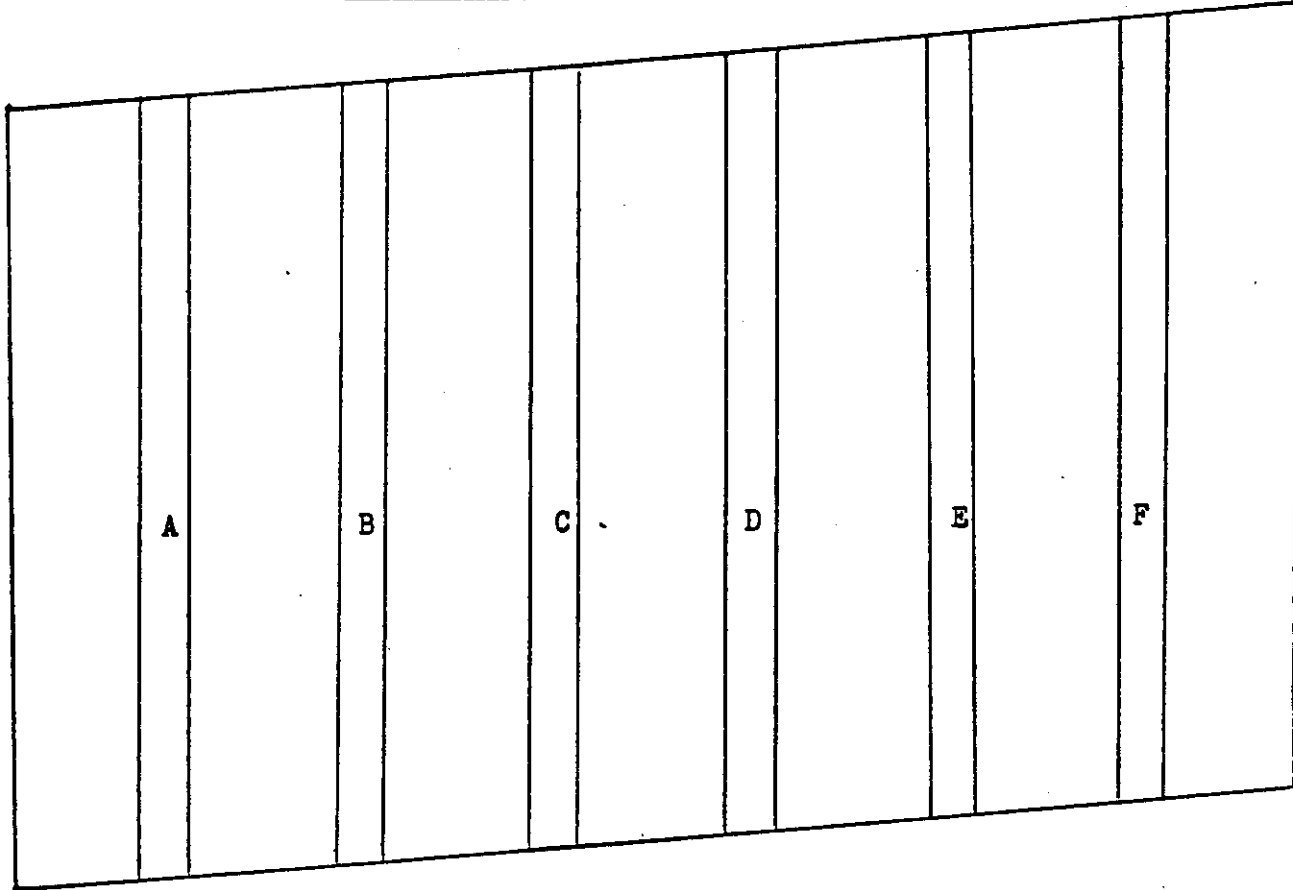
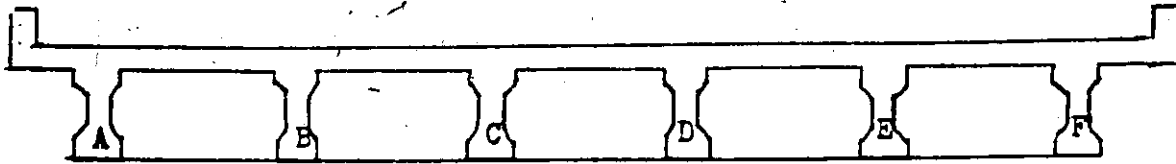
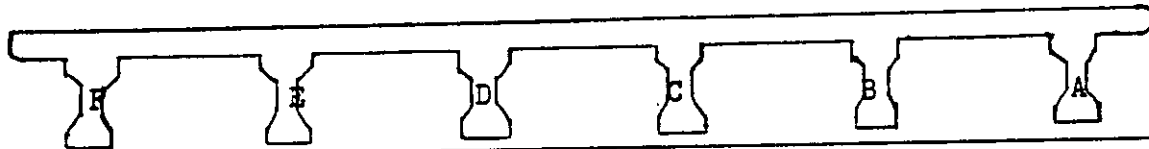
RT

SEP 16 1999

I-40 509SK. 60

RT.

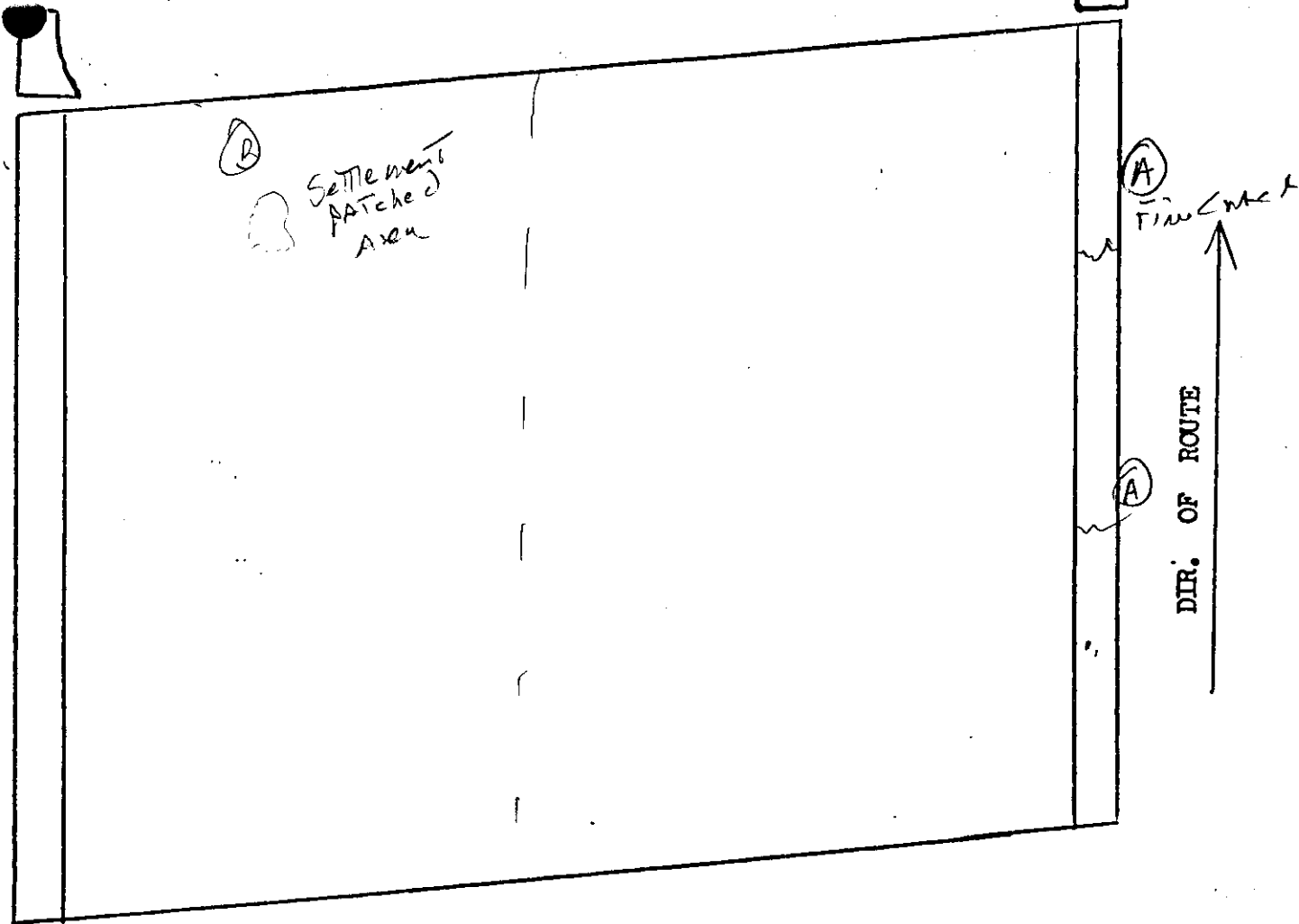
SPAN NO.

11-A
 ↑
 DIR. OF ROUTE


ELEMENT	RATING	COMMENT
BOTTOM DECK	G F P C	CONC. PILES
CONC. I. BEAMS	A G F P C	
	B G F P C	
	C G F P C	
	D G F P C	
	E G F P C	
	F G F P C	
DIA.	G F P C	

BRIDGE NO. 29 I-40 5.09 SK. 60 RT. SPAN NO. 11-B
RT

SEP 16 1999



ECK	G (F) P C	Settlement Patched Area (B) Traffic Abn. Normal
APET	G (F) P C	Fine Crack (A)
INS	G (F) P C	
NT	G F P C	

SEP 16 1999

BRIDGE NO. 79 T-40 5.09 RT

BENT NO. _____ SPAN NO. 11-6 ABT. NO. _____ PIER NO. _____

ELEMENT	PAVING	COMMENTS
BOTTOM DECK	G F P C	
CONC. I. BEAM A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
F	G F P C	
G	G F P C	SEE #1
H	G F P C	SEE #1
I	G F P C	
DIA.	G F P C	
BACKWALLS.	G F P C	
Earthquake Devices	G F P C	

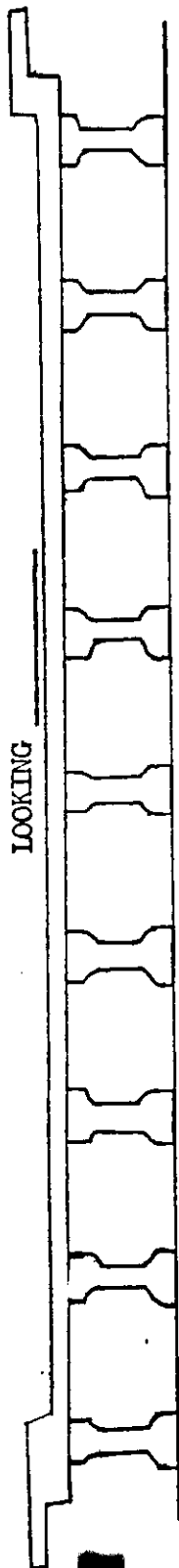
BR. NO. 79 I-40 5.09
RT

SK. 60°RT SPAN NO. 11-B

SEP 16 1999

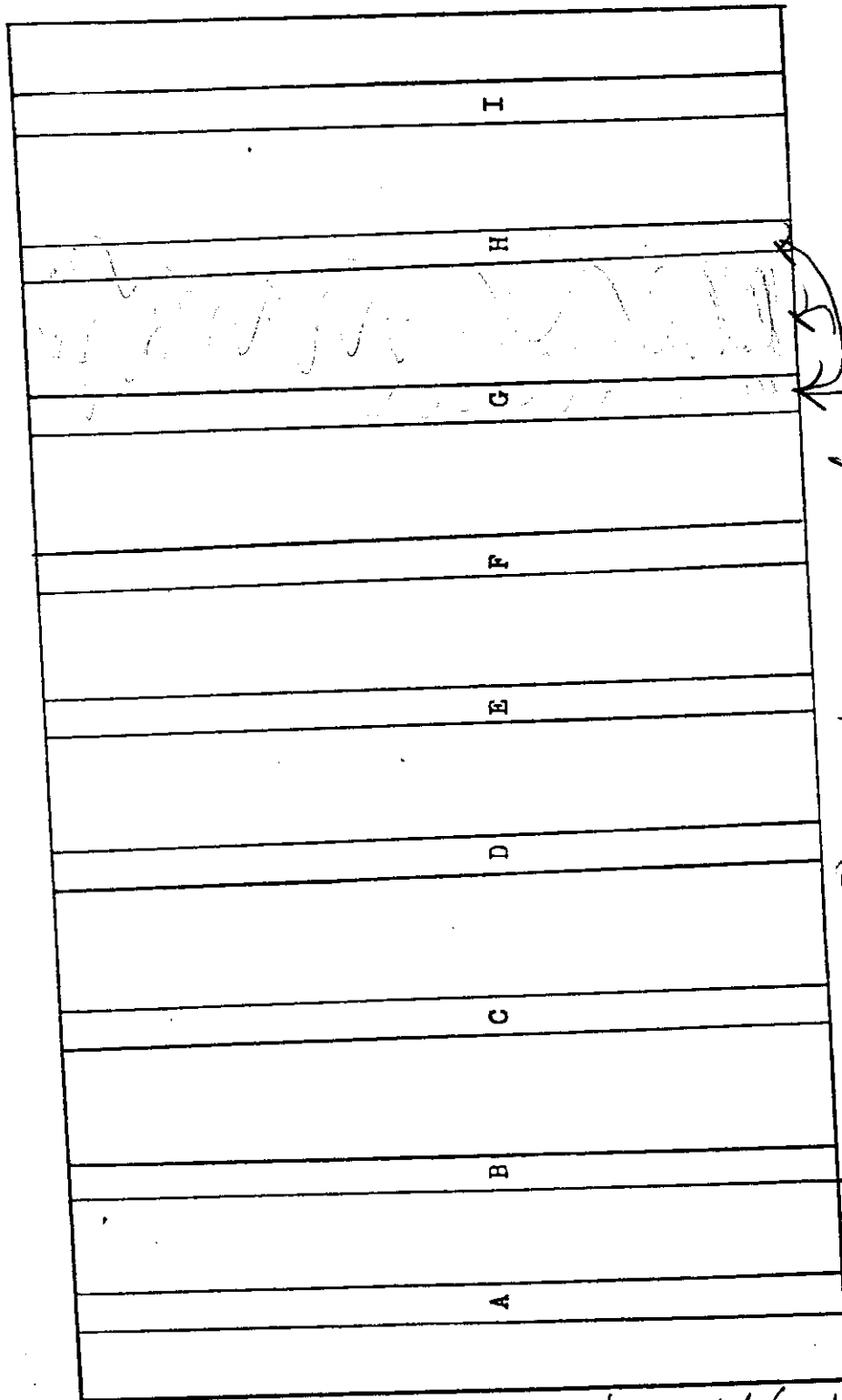
[Signature]

← DIR. OF ROUTE



LOOKING

LOOKING



① Smoke Stained AREA

LOOKING

EARTH Quake Devices Attached
to CTR's & Abut CAP

BRIDGE NO 79 T-410 5.09ABUT. NO. 1 58

SEP 16 1999

RT

LOOKING Back

211

VIEW

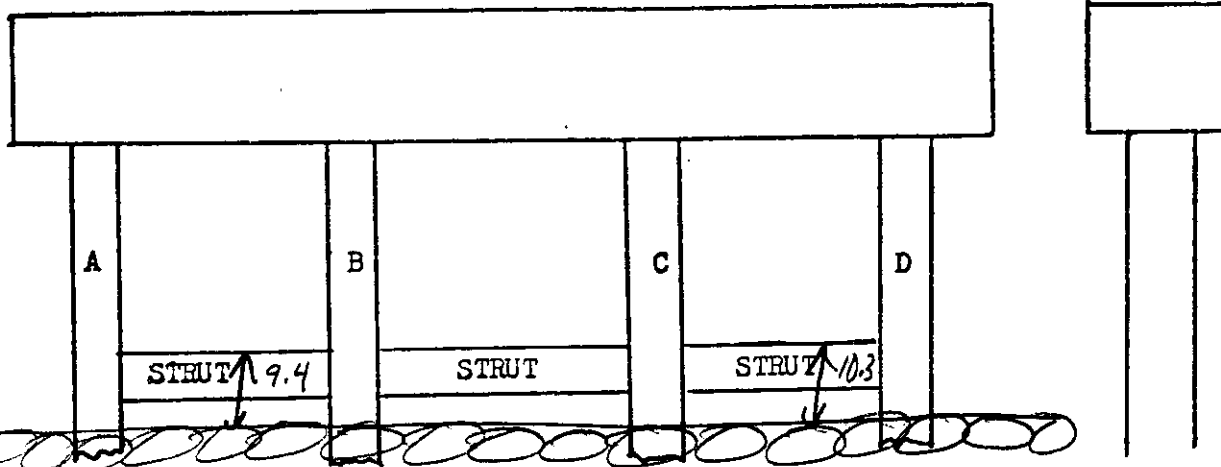
ELEMENT	RATING	COMMENT
BEARING	(G) F P C	Peds
CAP	G (F) P C	Popouts
WINGS	G (F) P C	Popouts
EMBANKMENT	(G) F P C	
PAVEMENT	G F P C	none
VEG.	G (F) P C	Summer Growth

BR. NO. 79 I-40 5.09SK. 60° RtBENT NO. 135

SEP 16 1999

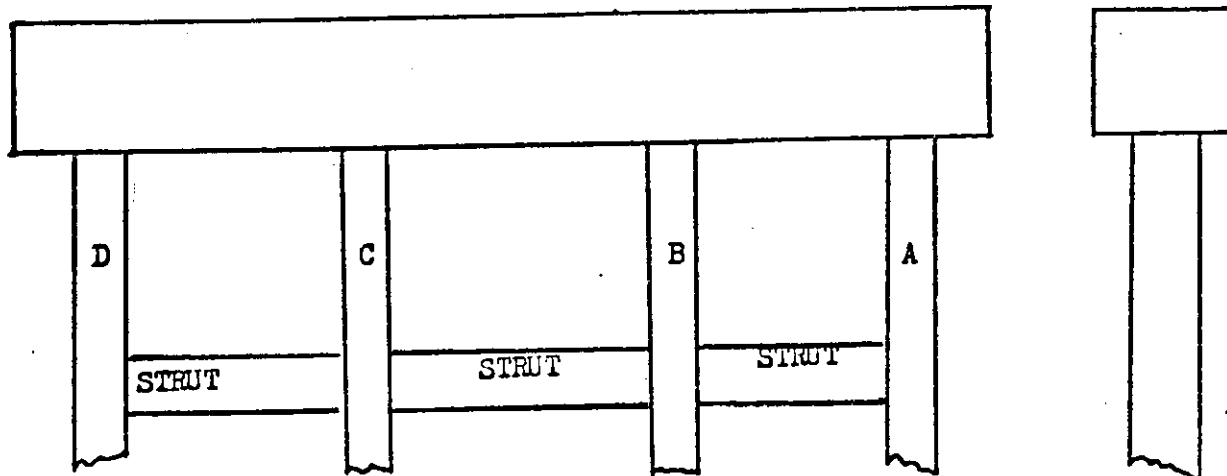
FRONT VIEW

END VIEW



REAR VIEW

END VIEW



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

Ads

CAP

G F P C

Popouts

PILING

A

G F P C

small Popouts

B

G F P C

C

G F P C

D

G F P C

STRUT

G

F

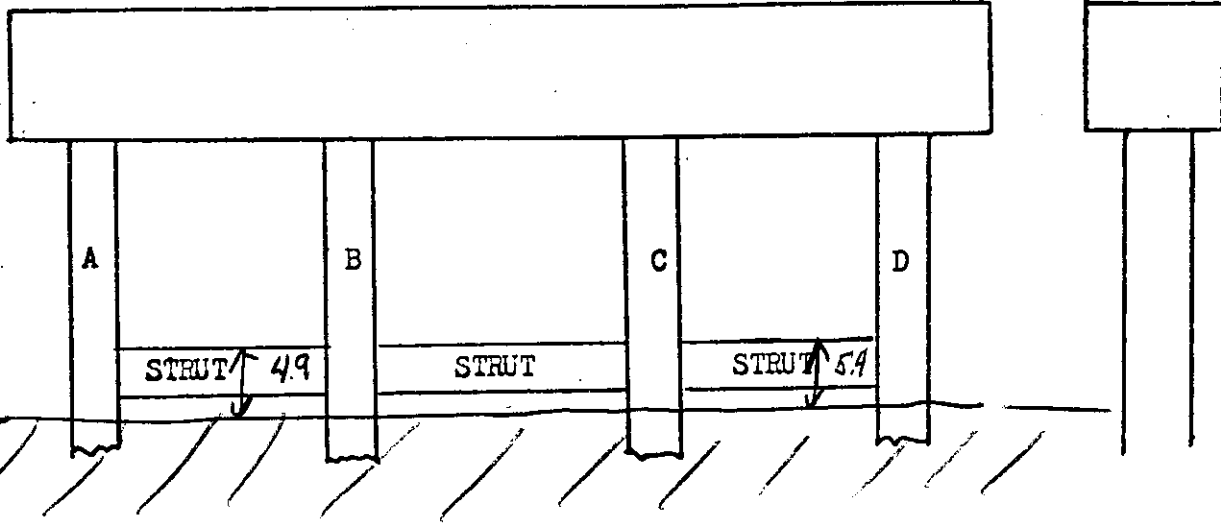
P

C

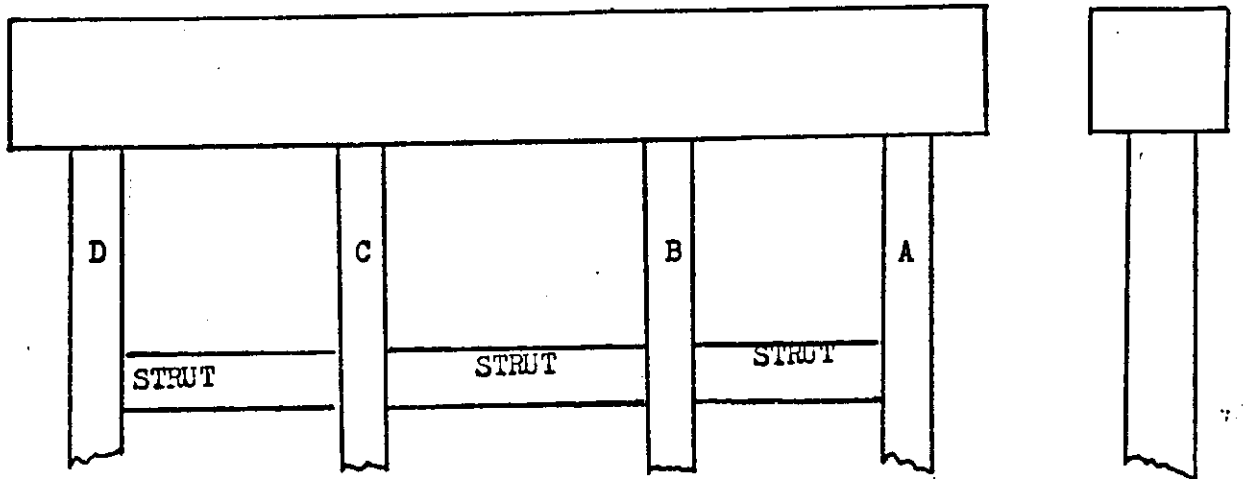
Popouts

BR. NO. 79 I-40 5.09SK. 60° Rt BENT NO. 2SEP 16 1999 30 END VIEW

FRONT VIEW



REAR VIEW



END VIEW

ELEMENT	RATING	COMMENT
BEARINGS	(G) F P C	Pads
CAP	G (F) P C	Repairs
PILING	A G (F) P C	Repairs
	B G F P C	
	C G F P C	
	D G (F) P C	
STRUT	G (F) P C	Repairs

BR. NO. 79 I-40 S.09SK. 60° LtBENT NO. 3

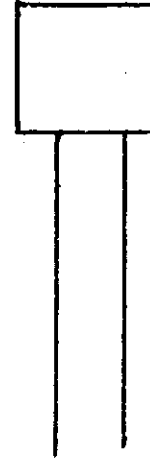
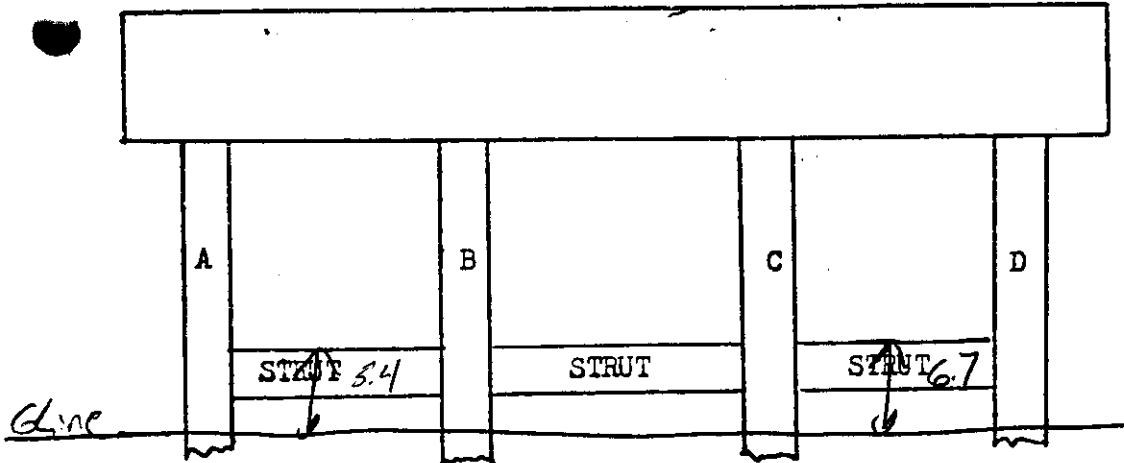
58

RT

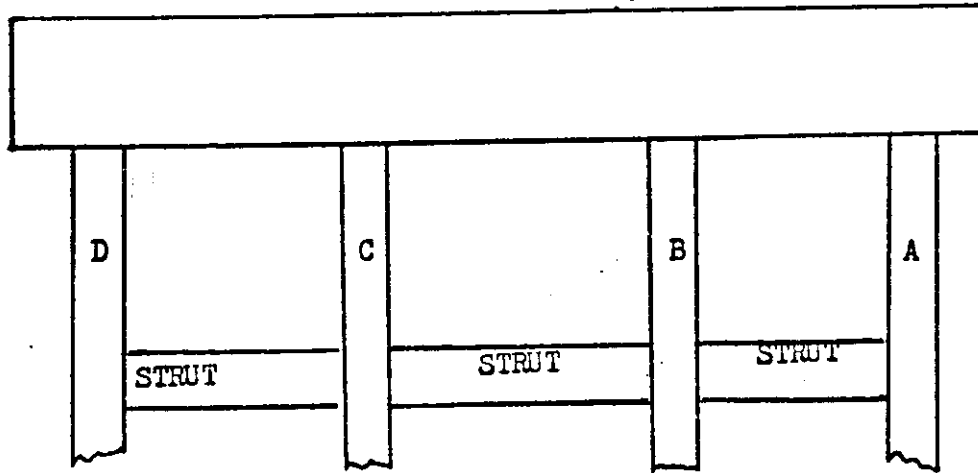
SEP 16 1999

END VIEW

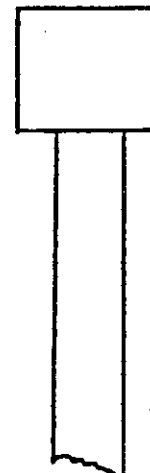
FRONT VIEW



REAR VIEW



END VIEW



ELEMENT RATING COMMENT

BEARINGS

(G) F P C Repairs

CAP

G (F) P C Repairs

PILING

A G (F) P C Repairs

B G (F) P C

C G (F) P C

D G (F) P C

STRUT

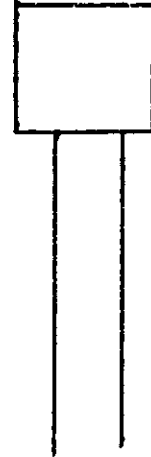
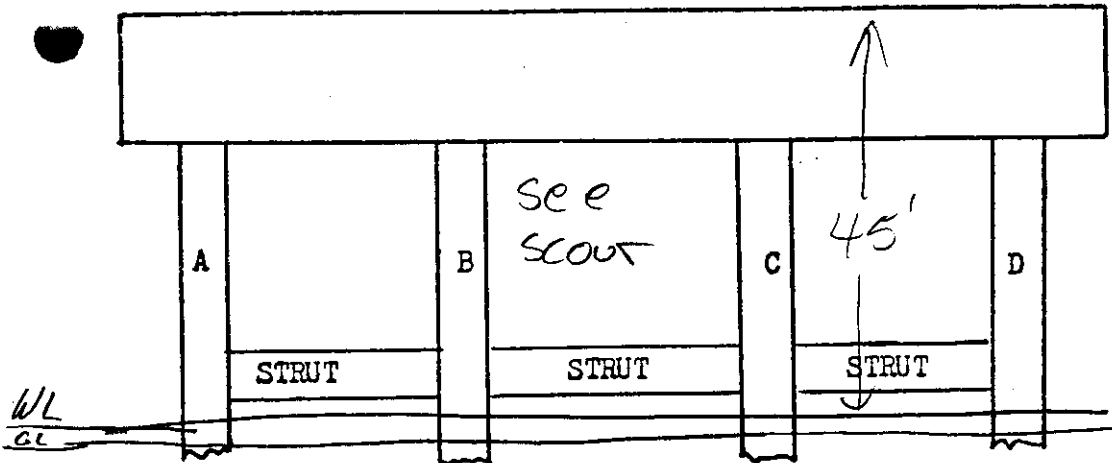
G (F) P C Repairs

BR. NO. 79 F-40 S.09SK. BENT NO. 30

RT

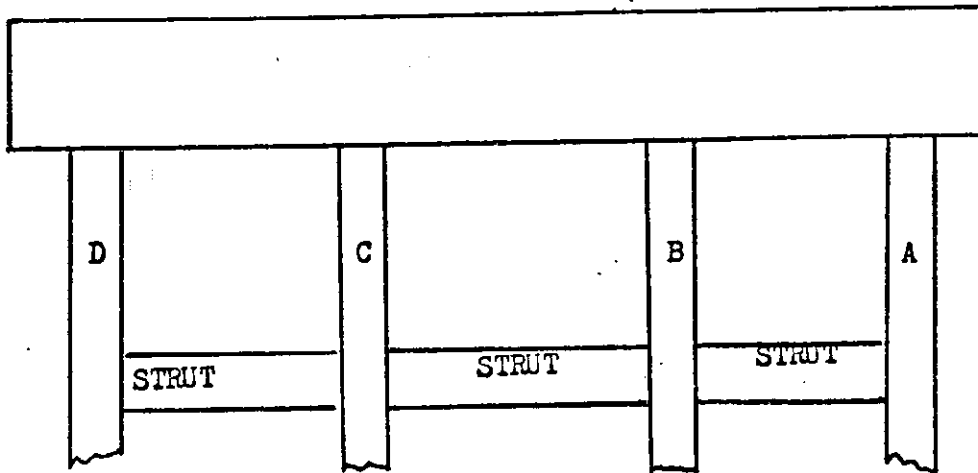
FRONT VIEW

SEP 16 1999 END VIEW



REAR VIEW

END VIEW



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

Reels

CAP

G F P C

Reels

PILING

A

G F P C

Reels + water Abr.

B

G F P C

C

G F P C

D

G F P C

STRUT

G F P C

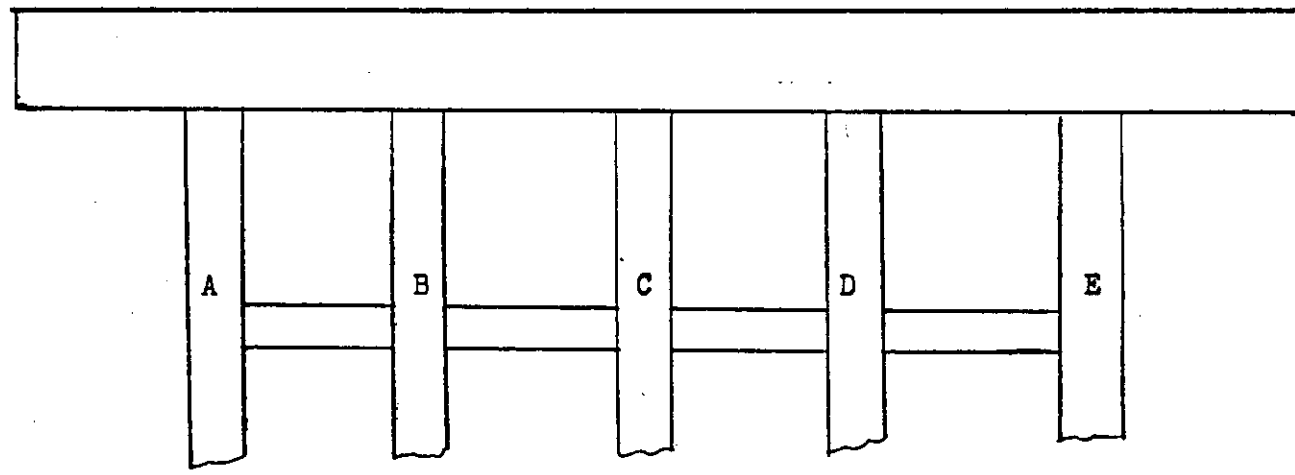
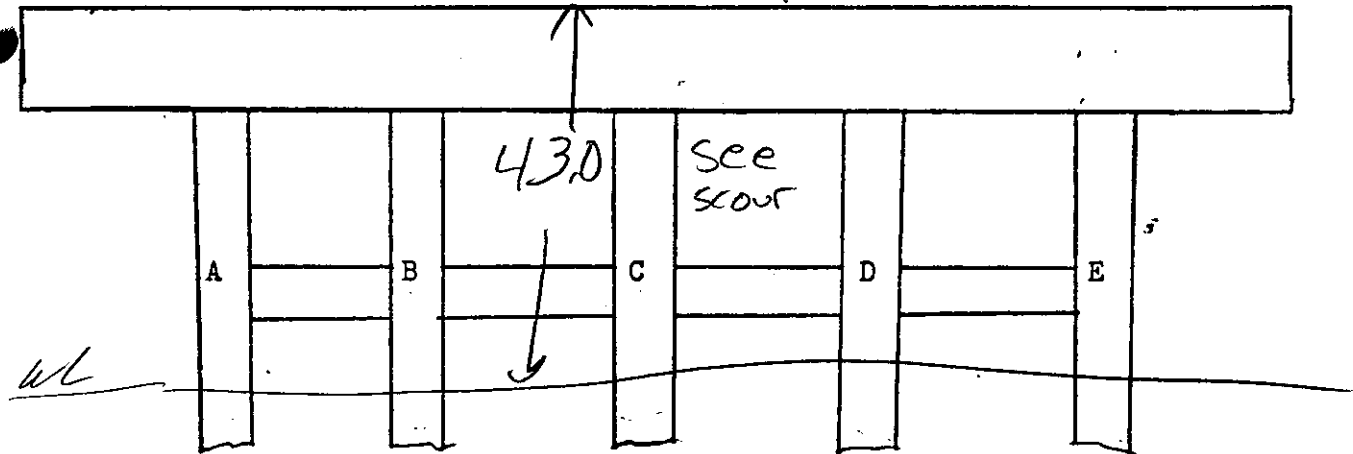
Reels

SEP 16 1999

38

BRIDGE NO. 79 I-40 5.09^{RT}

BENT NO. 5



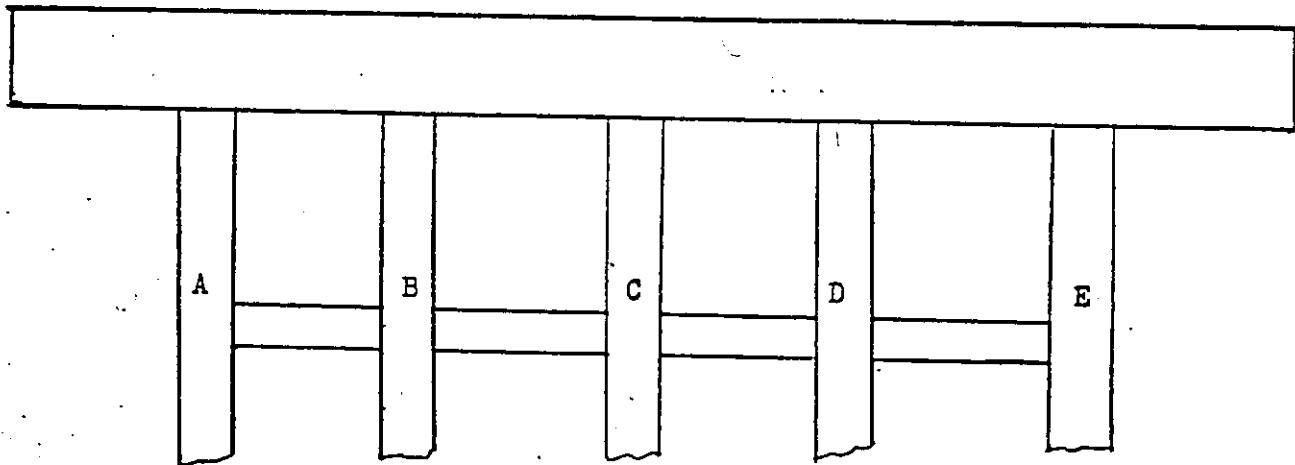
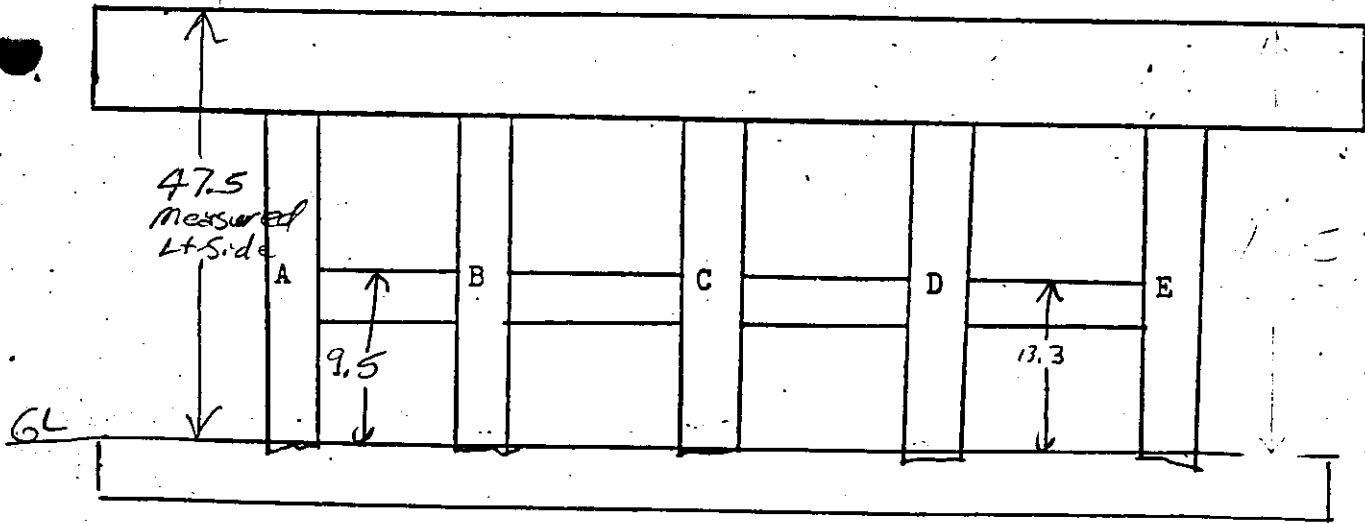
ELEMENT	RATINGS	COMMENT
CAP	G (F) P C	Repairs
COLUMN A-E	G (F) P C	Repairs + water Abr.
	G F P C	
	G F P C	
	G F P C	
BEARINGS	G (F) P C	Pads
PIERS	G (F) P C	Repairs

SEP 16 1999

58

BRIDGE NO. 79 I-40 5.09 RT

BENT NO. 6



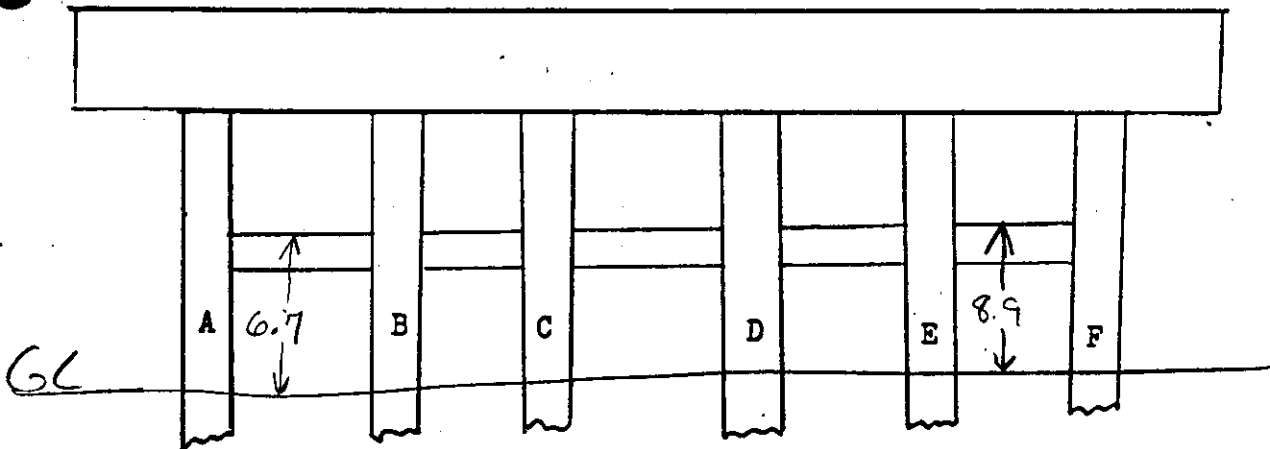
ELEMENT	RATINGS	COMMENT
CAP	G (F) P C	Repairs
COLUMN A-E	G (F) P C	Repairs
	G F P C	
	G F P C	
	G F P C	
BEARINGS	G (F) P C	Reas
PIERS	G (F) P C	Repairs

SEP 16 1999 30

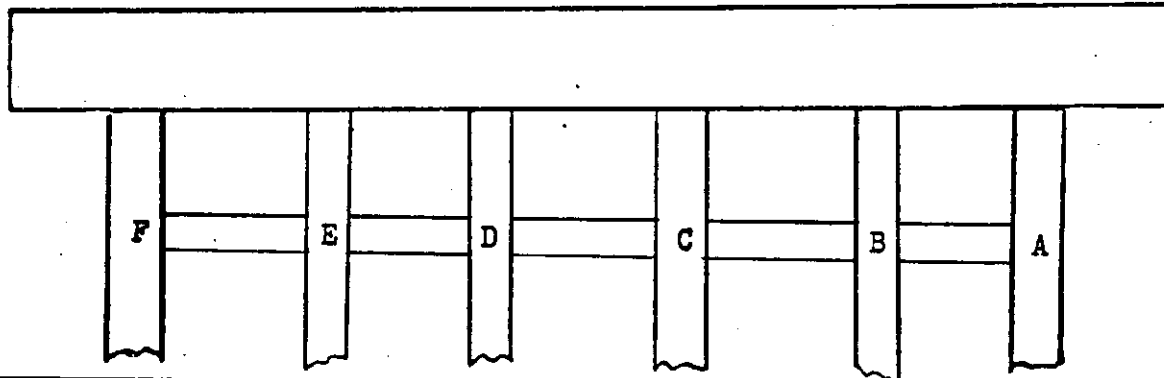
BR. NO. 79 I-40 5.09^{RT} SK. _____

BENT NO. 7

FRONT



REAR



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

Pods

CAP

G F P C

Popouts

COLUMNS A

G F P C

Popouts

B

G F P C

C

G F P C

D

G F P C

E

G F P C

F

G F P C

STRUTS

G F P C

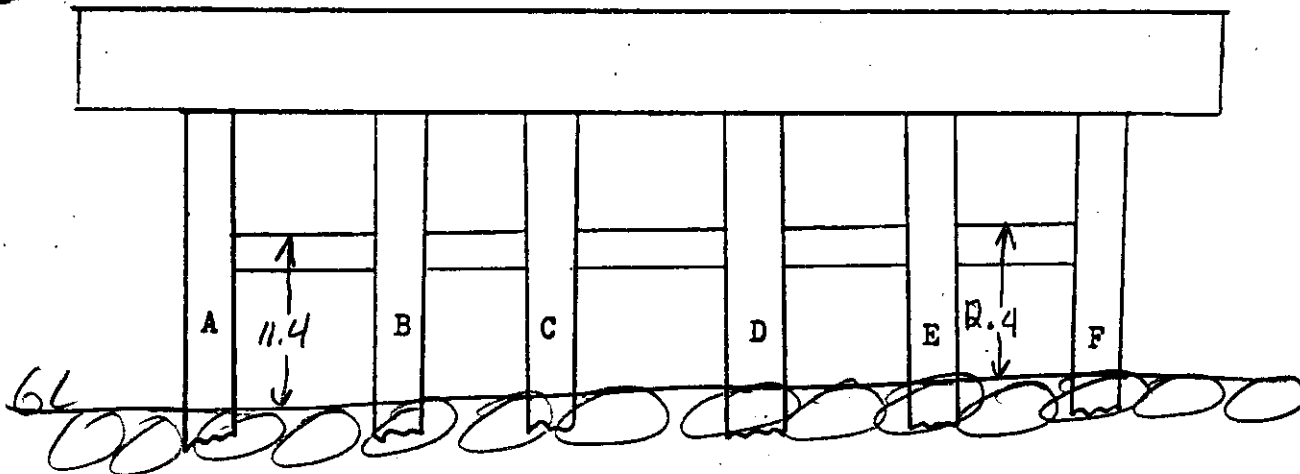
Popouts

BR. NO. 79-I-40 ^{RT}5.09 SK.

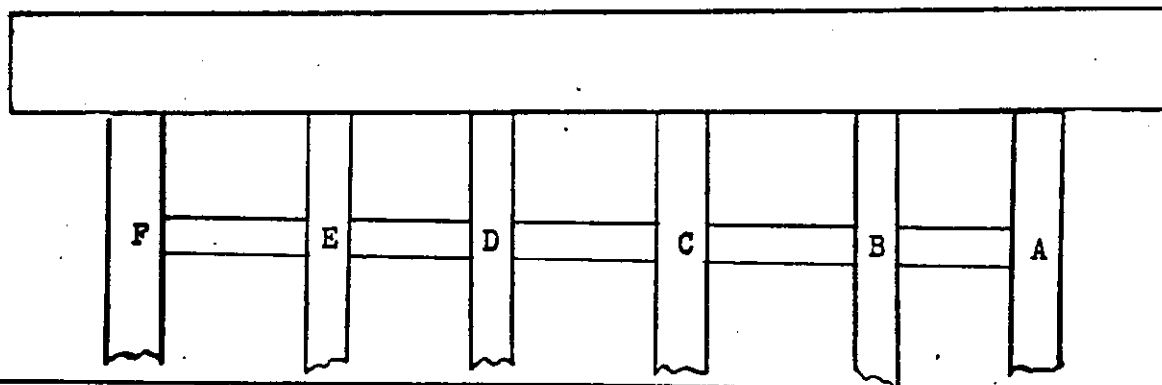
SEP 16 1999 30

BENT NO. 8

FRONT



REAR



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

Pads

CAP

G F P C

Pavement

COLUMNS A

G F P C

Pavement

B

G F P C

C

G F P C

D

G F P C

E

G F P C

F

G F P C

STRUTS

G F P C

Pavement

BR. NO. 79 I-40 5.09 RT

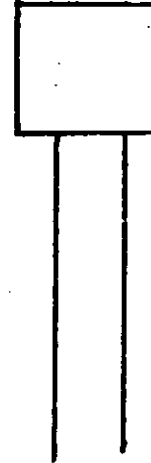
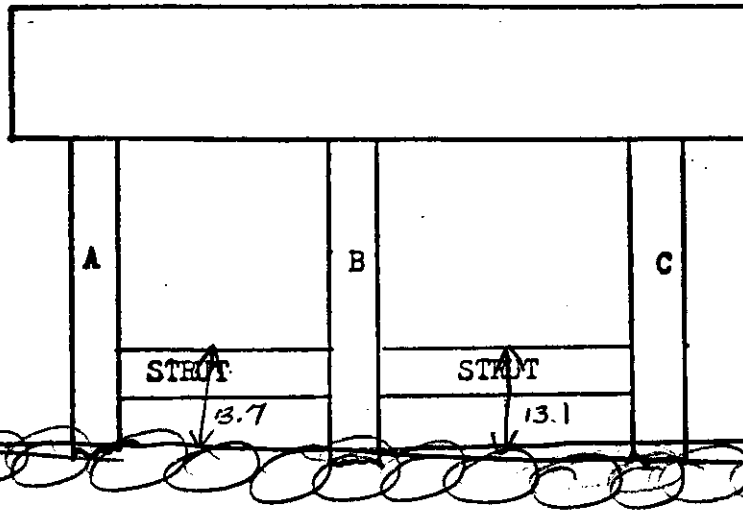
SK. _____

BENT NO. 9-A

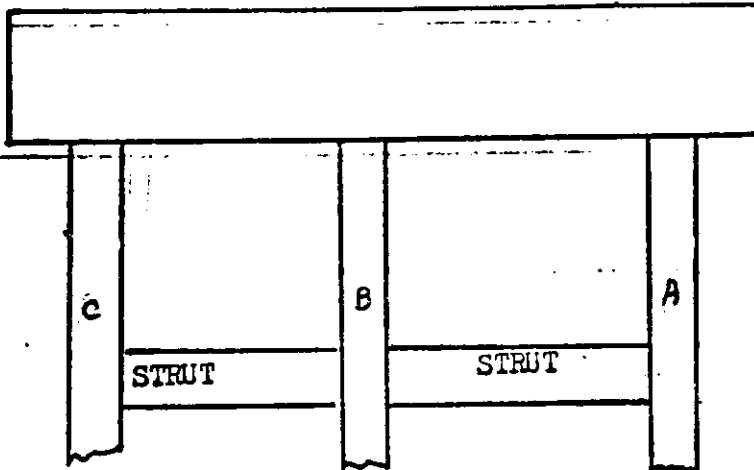
SEP 16 1999

END VIEW

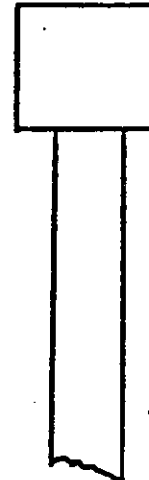
FRONT VIEW



REAR VIEW



END VIEW



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

Roads

CAP

G F P C

Roads

PILING

A

G F P C

Roads

B

G F P C

C

G F P C

STRUT

G F P C

Roads

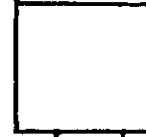
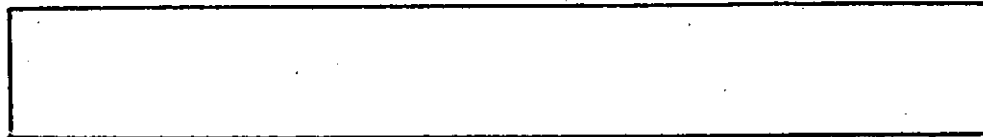
BR. NO. 79 I-40 5.09RT. Kamp
SK. _____BENT NO. 9-B

RT

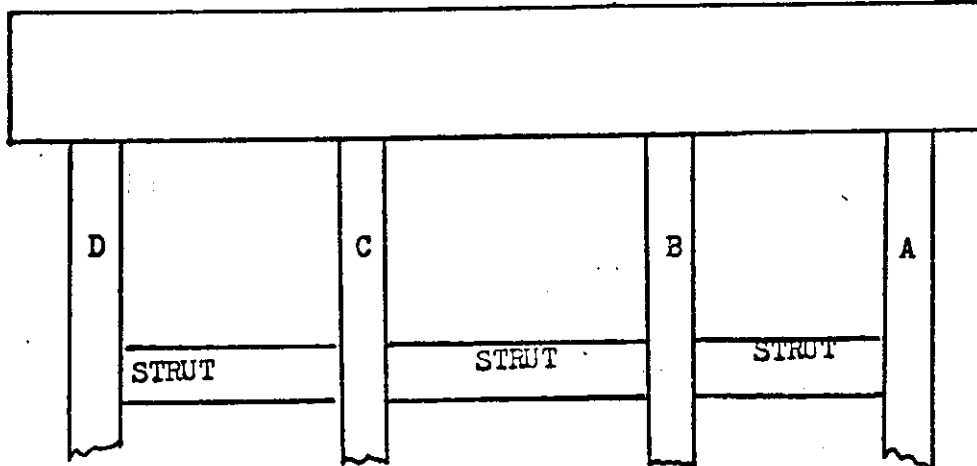
SEP 16 1999

END VIEW

FRONT VIEW



REAR VIEW



END VIEW



ELEMENT

RATING

COMMENT

BEARINGS

G F P C Rds

CAP

G F P C Rds

PILING

A G F P C Rds

B G F P C

C G F P C

D G F P C

STRUT

G F P C Rds

BR. NO. 79 T-40 5.09-RT

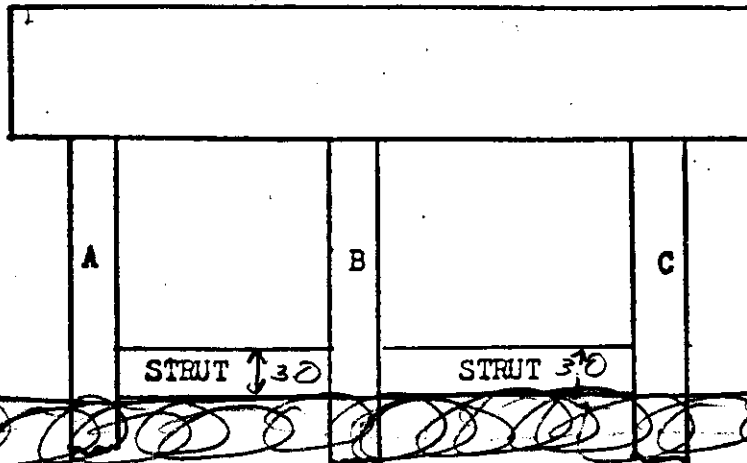
SK. _____

BENT NO. 10-A

SEP 16 1999

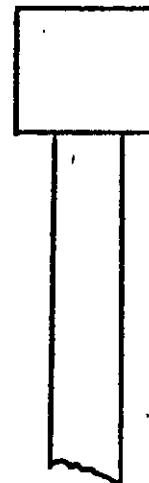
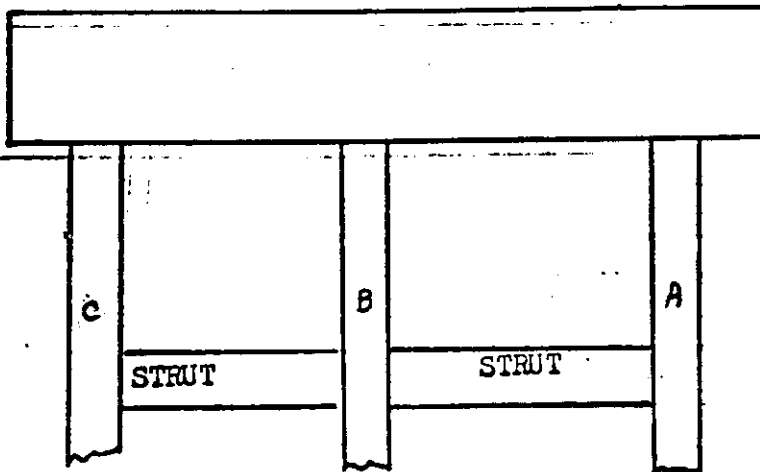
FRONT VIEW

END VIEW



REAR VIEW

END VIEW



ELEMENT

RATING

COMMENT

BEARINGS

G F P C

Pads

CAP

G F P C

Pads

PILING

A

G F P C

Pads

B

G F P C

C

G F P C

STRUT

G F P C

Pads

BR. NO. 79 I-40 5.09
RT

Rt. Lamp

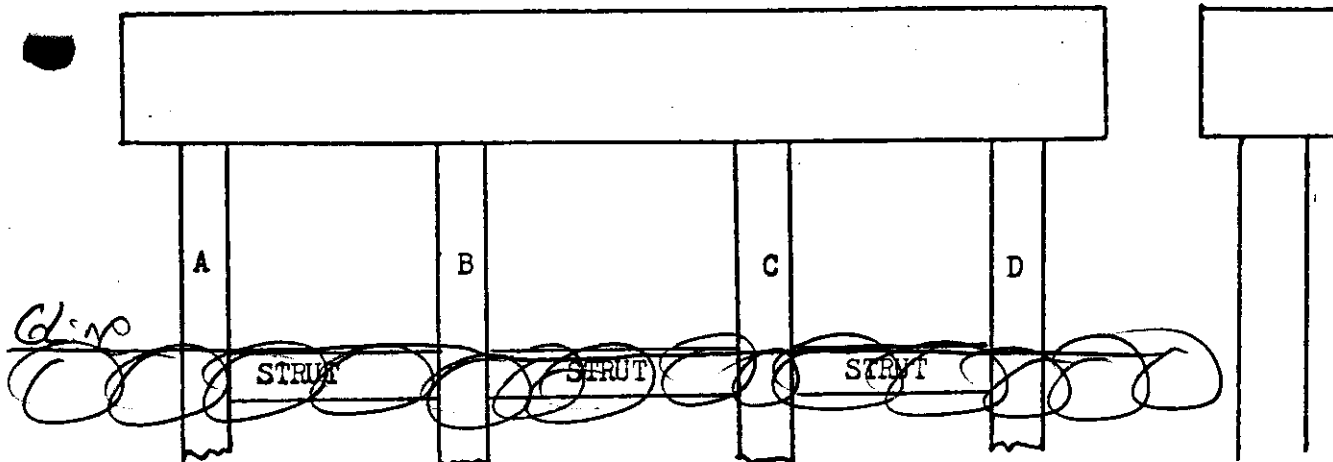
SK. _____

BENT NO. 10-B

SEP 16 1999

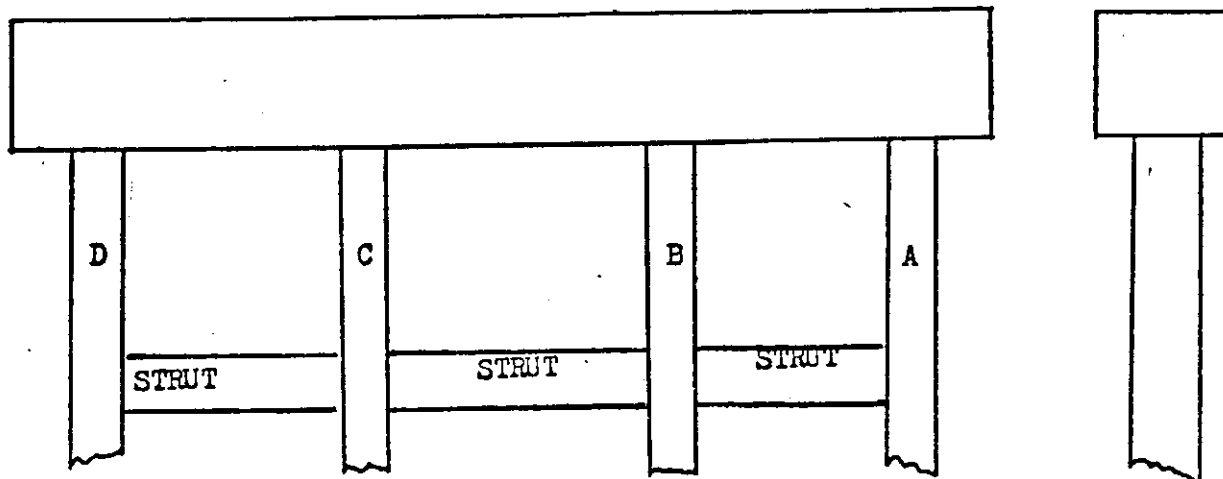
END VIEW

FRONT VIEW



REAR VIEW

END VIEW



ELEMENT

RATING

CONTENT

BEARINGS

G F P C

Pads

CAP

G F P C

Popouts

PILING

A

G F P C

Popouts

B

G F P C

C

G F P C

D

G F P C

STRUT

G F P C

NV

BRIDGE NO 79 T-40 509

SEP 16 1999

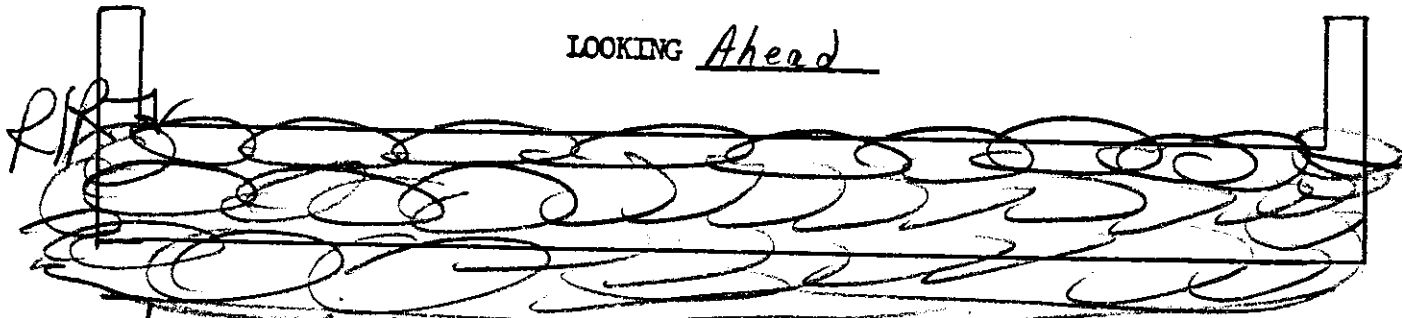
ABUT. NO. 2-A
RTLOOKING Ahead

VIEW

ELEMENT	RATING	COMMENT
BEARING	(G) F P C	Pads
CAP	G (F) P C	Repouts
WINGS	G (F) P C	Repouts
EMBANKMENT	(G) F P C	
PAVEMENT	G F P C	none
VEG.	G (F) P C	summer Growth

BRIDGE NO 79 2-40 5.09

SEP 16 1999

ABUT. NO. 2-BRT 1/4LOOKING Ahead

Most of
Abut Cap covered w/ (Rip Rap)

VIEW

ELEMENT	RATING	COMMENT
BEARING	(G) F P C	
CAP	(G) F P C	
WINGS	(G) F P C	
EMBANKMENT	(G) F P C	
PAVEMENT <u>Rip Rap</u>	(G) F P C	
VEG.	(G) F P C	

PROJECT # 7900
REV 5-14-91:
ABUT. DET.

04

1995

BRIDGE SCOUR REPAIR DETAIL

I-40/240 OVER WOLF RIVER

BRIDGE NO. 79-140-5.09

SHELBY COUNTY

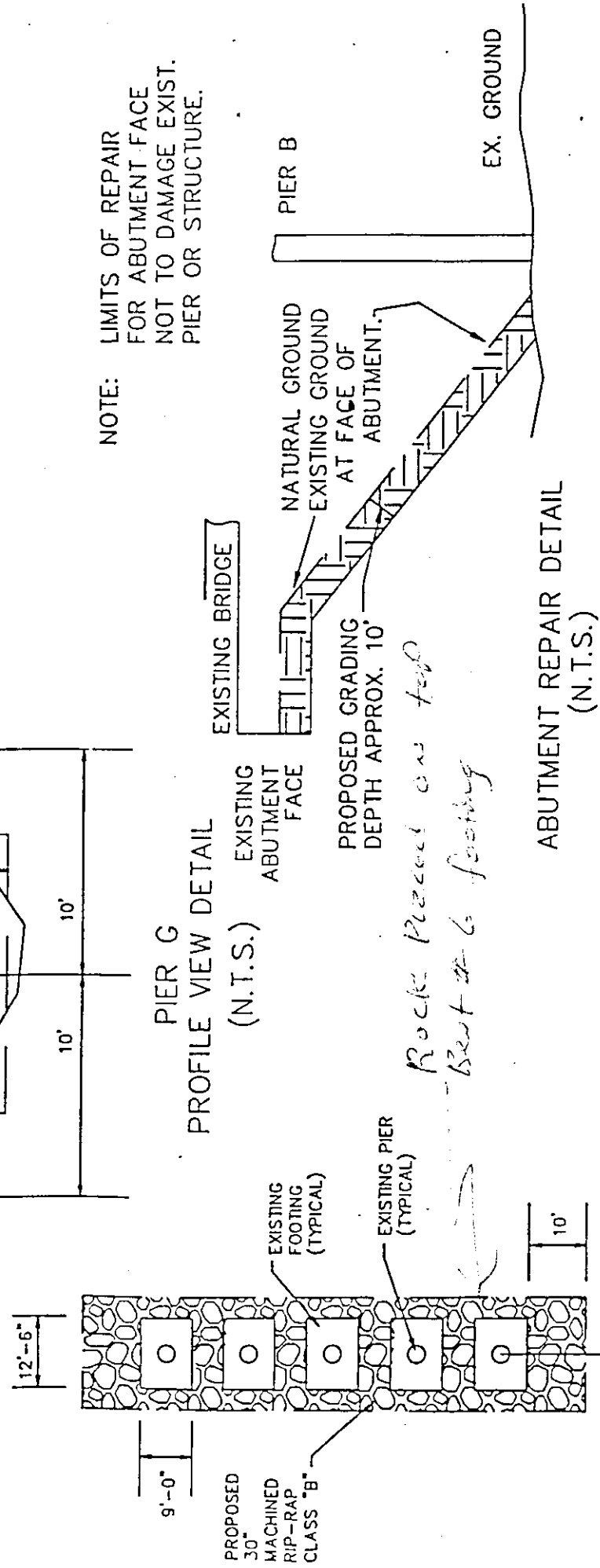
1991

DETAILS FOR
BOTH STRUCTURES

PLAN VIEW DETAIL
(N.T.S.)

Best # 6 only

PIER G



PIER G
PROFILE VIEW DETAIL
(N.T.S.)

ABUTMENT REPAIR DETAIL
(N.T.S.)

NOTE: LIMITS OF REPAIR
FOR ABUTMENT FACE
NOT TO DAMAGE EXIST.
PIER OR STRUCTURE.

*Rock placed on top
Best # 6 footing*

PROPOSED
30"
MACHINED
RIP-RAP
CLASS "B"

EXISTING
FOOTING
(TYPICAL)

EXISTING PIER
(TYPICAL)

EXISTING BRIDGE

EXISTING
ABUTMENT
FACE

PROPOSED GRADING
DEPTH APPROX. 10'

NATURAL GROUND
EXISTING GROUND
AT FACE OF
ABUTMENT.

PIER B

EX. GROUND

EXISTING PIER

TOP OF FOOTING
ELEV. 195.00

PROPOSED
30"
MACHINED
RIP-RAP
CLASS "B"

EXISTING
GROUND

LINE

30"

4'-6"

10'

10'

9'-0"

12'-6"

10'

10'

10'

E.B.L.

File H = Bent 7 72 piles driven Size 1 = 14' x 14'
Original pile length 1-36 = 45' Test pile length 55'
 37-72 = 25' Test piles were 29 & 65
Min. req. bearing ton = Not Listed Min. req. bearing ton for test pile = 85

File I = Bent 8 72 piles driven Size 1 = 14' x 14'
Original pile length 45' & 50' Test pile length 55'
Min. req. bearing ton = 58 Test piles were 29 & 68
Min. req. bearing ton for test pile = 85

File JA = Bent 9 60 piles driven Size 1 = 14' x 14'
Original pile length 20' Test pile length 30'
Min. req. bearing ton = 30 Test piles were 5 & 23
Min. req. bearing ton for test pile = Not listed

File KA = Bent 10 36 piles driven Size 1 = 14' x 14'
Original pile length 30' Test pile length 55'
Min. req. bearing ton = 65 Test piles were 5 & 23
Min. req. bearing ton for test pile = 95

File JB & Ramp L = Bent 11 24 piles driven Size 1 = 14' x 14'
Original pile length 25' Test pile length 25, 30, & 15
Min. req. bearing ton = Not listed Test piles were 5, 12, & 13
Min. req. bearing ton for test pile = 75

File KB & Ramp L = Bent 12 24 piles driven Size 1 = 14' x 14'
Original pile length 25' Test pile length 55
Min. req. bearing ton = 51 Test pile was 13
Min. req. bearing ton for test pile = 75

File LA = Abut. 2 15 piles driven Size 1 = 14' x 14'
Original pile length 20' Test pile length 55
Min. req. bearing ton = 44 Test piles were 3 & 13
Min. req. bearing ton for test pile = 65

File LB = Abut. 3 9 piles driven Size 1 = 14' x 14'
Original pile length 20' Test pile length 55
Min. req. bearing ton = 44 Test pile was 5
Min. req. bearing ton for test pile = 65

BRIDGE PILE SUMMARY REPORT

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Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
ABUT	1	1	14" Conc.	25.	44.	06/23/76
ABUT	1	2	14" Conc.	25.	51.	06/23/76
ABUT	1	3	14" Conc.	49.	90.	06/15/76
ABUT	1	4	14" Conc.	25.	73.	06/23/76
ABUT	1	5	14" Conc.	25.	42.	06/23/76
ABUT	1	6	14" Conc.	25.	39.	06/23/76
ABUT	1	7	14" Conc.	25.	44.	06/23/76
ABUT	1	8	14" Conc.	25.	43.	06/24/76
ABUT	1	9	14" Conc.	25.	40.	06/24/76
ABUT	1	10	14" Conc.	25.	42.	06/24/76
ABUT	1	11	14" Conc.	49.	90.	06/15/76
ABUT	1	12	14" Conc.	25.	0.	06/24/76
ABUT	1	13	14" Conc.	25.	34.	06/24/76

Average Pile Length (ft) = 28.6923

ABUT	2	1	14" Conc.	19.41	156.	06/22/77
ABUT	2	2	14" Conc.	18.25	125.	06/22/77
ABUT	2	3	14" Conc.	35.	50.	06/21/77
ABUT	2	4	14" Conc.	18.	130.	06/22/77
ABUT	2	5	14" Conc.	18.16	177.	06/22/77
ABUT	2	6	14" Conc.	16.58	75.	06/22/77
ABUT	2	7	14" Conc.	19.16	132.	06/22/77
ABUT	2	8	14" Conc.	17.58	145.	06/27/77
ABUT	2	9	14" Conc.	17.16	135.	06/27/77
ABUT	2	10	14" Conc.	18.66	123.	06/27/77
ABUT	2	11	14" Conc.	18.66	123.	06/27/77
ABUT	2	12	14" Conc.	18.	0.	06/27/77
ABUT	2	13	14" Conc.	45.	62.	06/21/77
ABUT	2	14	14" Conc.	17.33	120.	06/22/77
ABUT	2	15	14" Conc.	17.16	127.	06/25/77

Average Pile Length (ft) = 20.9407

ABUT	3	1	14" Conc.	20.	80.	06/22/77
ABUT	3	2	14" Conc.	20.	88.	06/22/77
ABUT	3	3	14" Conc.	20.	91.	06/22/77
ABUT	3	4	14" Conc.	20.	84.	06/22/77
ABUT	3	5	14" Conc.	55.	82.	06/21/77

* 0. = No Data Available

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Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	File Number	File Number	File Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
ABUT	3	6	14" Conc.	20.	83.	06/22/77
ABUT	3	7	14" Conc.	20.	92.	06/22/77
ABUT	3	8	14" Conc.	20.	85.	06/22/77
ABUT	3	9	14" Conc.	20.	70.	06/22/77

Average Pile Length (ft) = 23.8889

BENT	1	1	14" Conc.	19.	115.	09/14/76
BENT	1	2	14" Conc.	20.	96.	09/14/76
BENT	1	3	14" Conc.	20.	61.	09/14/76
BENT	1	4	14" Conc.	17.	98.	09/14/76
BENT	1	5	14" Conc.	15.	49.	08/31/76
BENT	1	6	14" Conc.	20.	88.	09/14/76
BENT	1	7	14" Conc.	18.	124.	09/14/76
BENT	1	8	14" Conc.	20.	144.	09/14/76
BENT	1	9	14" Conc.	18.16	0.	09/03/76
BENT	1	10	14" Conc.	19.41	0.	09/03/76
BENT	1	11	14" Conc.	20.	0.	09/03/76
BENT	1	12	14" Conc.	20.	0.	09/03/76
BENT	1	13	14" Conc.	20.	43.	09/03/76
BENT	1	14	14" Conc.	20.	89.	09/03/76
BENT	1	15	14" Conc.	20.	66.	09/03/76
BENT	1	16	14" Conc.	20.	73.	09/03/76
BENT	1	17	14" Conc.	20.	92.	09/03/76
BENT	1	18	14" Conc.	20.	54.	09/03/76
BENT	1	19	14" Conc.	20.	64.	09/14/76
BENT	1	20	14" Conc.	17.	98.	09/14/76
BENT	1	21	14" Conc.	17.25	102.	09/14/76
BENT	1	22	14" Conc.	20.	70.	09/14/76
BENT	1	23	14" Conc.	20.	72.	09/14/76
BENT	1	24	14" Conc.	20.	56.	09/14/76
BENT	1	25	14" Conc.	20.	69.	09/03/76
BENT	1	26	14" Conc.	20.	74.	09/03/76
BENT	1	27	14" Conc.	20.	65.	09/03/76
BENT	1	28	14" Conc.	20.	58.	09/03/76
BENT	1	29	14" Conc.	15.	47.	08/31/76
BENT	1	30	14" Conc.	20.	44.	09/02/76
BENT	1	31	14" Conc.	20.	110.	09/15/76
BENT	1	32	14" Conc.	13.33	80.	09/03/76
BENT	1	33	14" Conc.	18.16	0.	09/03/76

* 0. = No Data Available

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Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	1	34	14" Conc.	20.	94.	09/15/76
BENT	1	35	14" Conc.	20.	62.	09/02/76
BENT	1	36	14" Conc.	20.	56.	09/02/76
BENT	1	37	14" Conc.	20.	49.	09/16/76
BENT	1	38	14" Conc.	20.	47.	09/16/76
BENT	1	39	14" Conc.	20.	54.	09/16/76
BENT	1	40	14" Conc.	20.	44.	09/16/76
BENT	1	41	14" Conc.	20.	40.	09/16/76
BENT	1	42	14" Conc.	20.	0.	09/16/76
BENT	1	43	14" Conc.	12.16	41.	09/16/76
BENT	1	44	14" Conc.	13.33	130.	09/16/76
BENT	1	45	14" Conc.	14.5	61.	09/16/76
BENT	1	46	14" Conc.	20.	57.	09/16/76
BENT	1	47	14" Conc.	20.	57.	09/16/76
BENT	1	48	14" Conc.	20.	91.	09/16/76

Average Pile Length (ft) = 18.9021

BENT	2	1	14" Conc.	17.	74.	08/12/76
BENT	2	2	14" Conc.	20.	70.	08/12/76
BENT	2	3	14" Conc.	20.	65.	08/12/76
BENT	2	4	14" Conc.	15.	50.	08/13/76
BENT	2	5	14" Conc.	20.	0.	08/11/76
BENT	2	6	14" Conc.	18.	56.	08/11/76
BENT	2	7	14" Conc.	20.	87.7	08/07/76
BENT	2	8	14" Conc.	18.	64.	08/11/76
BENT	2	9	14" Conc.	17.	61.	08/13/76
BENT	2	10	14" Conc.	18.66	0.	08/11/76
BENT	2	11	14" Conc.	20.	0.	08/11/76
BENT	2	12	14" Conc.	20.	0.	08/13/76
BENT	2	13	14" Conc.	15.	0.	08/13/76
BENT	2	14	14" Conc.	15.	0.	08/12/76
BENT	2	15	14" Conc.	18.	53.	08/12/76
BENT	2	16	14" Conc.	18.	60.	08/12/76
BENT	2	17	14" Conc.	20.	91.	08/13/76
BENT	2	18	14" Conc.	20.	58.	08/13/76
BENT	2	19	14" Conc.	20.	55.	08/13/76
BENT	2	20	14" Conc.	18.	70.	08/14/76
BENT	2	21	14" Conc.	20.	62.	08/14/76
BENT	2	22	14" Conc.	20.	66.	08/14/76

* 0. = No Data Available

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Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	2	23	14" Conc.	20.	50.	08/14/76
BENT	2	24	14" Conc.	18.	65.	08/14/76
BENT	2	25	14" Conc.	20.	59.	08/12/76
BENT	2	26	14" Conc.	18.	76.	08/12/76
BENT	2	27	14" Conc.	19.	53.	08/12/76
BENT	2	28	14" Conc.	20.	56.	08/12/76
BENT	2	29	14" Conc.	20.	56.	08/12/76
BENT	2	30	14" Conc.	20.	56.	08/12/76
BENT	2	31	14" Conc.	15.	61.	08/12/76
BENT	2	32	14" Conc.	20.	82.	08/13/76
BENT	2	33	14" Conc.	20.	0.	08/16/76
BENT	2	34	14" Conc.	15.08	52.	08/16/76
BENT	2	35	14" Conc.	20.	72.	08/16/76
BENT	2	36	14" Conc.	20.	64.	08/16/76
BENT	2	37	14" Conc.	20.	69.	08/14/76
BENT	2	38	14" Conc.	20.	73.	08/14/76
BENT	2	39	14" Conc.	20.	73.2	08/07/76
BENT	2	40	14" Conc.	20.	0.	08/14/76
BENT	2	41	14" Conc.	20.	0.	08/14/76
BENT	2	42	14" Conc.	20.	0.	08/14/76
BENT	2	43	14" Conc.	15.41	0.	08/16/76
BENT	2	44	14" Conc.	20.	115.	08/16/76
BENT	2	45	14" Conc.	20.	89.	08/16/76
BENT	2	46	14" Conc.	19.25	92.	08/17/76
BENT	2	47	14" Conc.	18.	93.	08/17/76
BENT	2	48	14" Conc.	20.	65.	08/16/76
BENT	2	49	14" Conc.	20.	80.	08/17/76
BENT	2	50	14" Conc.	20.	90.	08/17/76
BENT	2	51	14" Conc.	20.	60.	08/17/76
BENT	2	52	14" Conc.	19.41	34.	08/17/76
BENT	2	53	14" Conc.	20.	51.	08/16/76
BENT	2	54	14" Conc.	20.	66.	08/16/76
BENT	2	55	14" Conc.	20.	93.	08/16/76
BENT	2	56	14" Conc.	20.	90.	08/16/76
BENT	2	57	14" Conc.	20.	118.	08/17/76
BENT	2	58	14" Conc.	20.	105.	08/17/76
BENT	2	59	14" Conc.	20.	114.	08/16/76
BENT	2	60	14" Conc.	20.	100.	08/16/76
BENT	2	61	14" Conc.	20.	93.	08/17/76
BENT	2	62	14" Conc.	20.	119.	08/17/76

* 0. = No Data Available

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Project #...: 79007-3127-44

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Substructure Name	Pile Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	2	63	14" Conc.	20.	114.	08/17/76
BENT	2	64	14" Conc.	20.	104.	08/17/76

Average Pile Length (ft) = 19.1377

BENT	3	1	14" Conc.	19.	91.	06/06/77
BENT	3	2	14" Conc.	31.	30.	06/04/77
BENT	3	3	14" Conc.	45.5	0.	06/04/77
BENT	3	4	14" Conc.	28.	40.	06/06/77
BENT	3	5	14" Conc.	55.	34.	06/02/77
BENT	3	6	14" Conc.	55.	31.	06/02/77
BENT	3	7	14" Conc.	55.	55.	06/01/77
BENT	3	8	14" Conc.	55.	32.	06/02/77
BENT	3	9	14" Conc.	42.	32.	06/04/77
BENT	3	10	14" Conc.	53.	0.	06/04/77
BENT	3	11	14" Conc.	55.	0.	06/06/77
BENT	3	12	14" Conc.	55.	0.	06/06/77
BENT	3	13	14" Conc.	15.	0.	06/06/77
BENT	3	14	14" Conc.	15.58	108.	06/04/77
BENT	3	15	14" Conc.	26.66	31.	06/04/77
BENT	3	16	14" Conc.	18.	87.	06/06/77
BENT	3	17	14" Conc.	37.	30.	06/06/77
BENT	3	18	14" Conc.	51.	37.	06/03/77
BENT	3	19	14" Conc.	50.	33.	06/03/77
BENT	3	20	14" Conc.	46.	44.	06/03/77
BENT	3	21	14" Conc.	50.	30.	06/03/77
BENT	3	22	14" Conc.	50.	31.	06/02/77
BENT	3	23	14" Conc.	55.	32.	06/02/77
BENT	3	24	14" Conc.	55.	28.	06/02/77
BENT	3	25	14" Conc.	50.	41.	06/03/77
BENT	3	26	14" Conc.	50.	32.	06/03/77
BENT	3	27	14" Conc.	50.	31.	06/03/77
BENT	3	28	14" Conc.	55.	30.	06/03/77
BENT	3	29	14" Conc.	30.	40.	06/06/77
BENT	3	30	14" Conc.	46.	31.	06/03/77
BENT	3	31	14" Conc.	46.	23.	06/03/77
BENT	3	32	14" Conc.	27.	36.	06/06/77
BENT	3	33	14" Conc.	45.	26.	05/13/77
BENT	3	34	14" Conc.	45.	25.	05/13/77
BENT	3	35	14" Conc.	45.	31.	05/13/77

* 0. = No Data Available

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Project #...: 79007-3127-44

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Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	3	36	14" Conc.	45.	33.	05/13/77
BENT	3	37	14" Conc.	45.	28.	05/13/77
BENT	3	38	14" Conc.	45.	0.	05/13/77
BENT	3	39	14" Conc.	55.	42.	03/03/77
BENT	3	40	14" Conc.	45.	0.	05/13/77
BENT	3	41	14" Conc.	45.	0.	05/13/77
BENT	3	42	14" Conc.	45.	25.	05/13/77
BENT	3	43	14" Conc.	45.	31.	05/13/77
BENT	3	44	14" Conc.	45.	27.	05/13/77
BENT	3	45	14" Conc.	45.	26.	05/13/77
BENT	3	46	14" Conc.	45.	26.	05/13/77
BENT	3	47	14" Conc.	45.	29.	05/13/77
BENT	3	48	14" Conc.	45.	35.	05/13/77
BENT	3	49	14" Conc.	45.	35.	05/16/77
BENT	3	50	14" Conc.	45.	31.	05/16/77
BENT	3	51	14" Conc.	45.	29.	05/16/77
BENT	3	52	14" Conc.	45.	28.	05/16/77
BENT	3	53	14" Conc.	45.	36.	05/14/77
BENT	3	54	14" Conc.	45.	30.	05/13/77
BENT	3	55	14" Conc.	45.	25.	05/13/77
BENT	3	56	14" Conc.	45.	31.	05/14/77
BENT	3	57	14" Conc.	45.	41.	05/14/77
BENT	3	58	14" Conc.	45.	25.	05/14/77
BENT	3	59	14" Conc.	44.41	30.	05/14/77
BENT	3	60	14" Conc.	45.	28.	05/14/77
BENT	3	61	14" Conc.	45.	31.	05/14/77
BENT	3	62	14" Conc.	45.	24.	05/14/77
BENT	3	63	14" Conc.	45.	28.	05/14/77
BENT	3	64	14" Conc.	45.	26.	05/14/77

Average Pile Length (ft) = 44.0805

BENT	4	1	14" Conc.	50.	24.	07/07/77
BENT	4	2	14" Conc.	50.	23.	07/07/77
BENT	4	3	14" Conc.	50.	29.	07/05/77
BENT	4	4	14" Conc.	50.	29.	07/07/77
BENT	4	5	14" Conc.	55.	36.	07/05/77
BENT	4	6	14" Conc.	46.5	52.	07/06/77
BENT	4	7	14" Conc.	55.	50.	06/30/77
BENT	4	8	14" Conc.	55.	34.	07/01/77

* 0. = No Data Available

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Substructure Name	File Number	File Number	File Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	4	9	14" Conc.	55.	31.	07/05/77
BENT	4	10	14" Conc.	43.41	0.	07/01/77
BENT	4	11	14" Conc.	50.	0.	07/01/77
BENT	4	12	14" Conc.	55.	0.	07/01/77
BENT	4	13	14" Conc.	50.	24.	07/07/77
BENT	4	14	14" Conc.	50.	50.	07/07/77
BENT	4	15	14" Conc.	50.	37.	07/07/77
BENT	4	16	14" Conc.	50.	25.	07/07/77
BENT	4	17	14" Conc.	50.	20.	07/06/77
BENT	4	18	14" Conc.	50.	31.	07/06/77
BENT	4	19	14" Conc.	50.	38.	07/06/77
BENT	4	20	14" Conc.	50.	24.	07/06/77
BENT	4	21	14" Conc.	55.	29.	07/06/77
BENT	4	22	14" Conc.	55.	30.	07/05/77
BENT	4	23	14" Conc.	55.	29.	07/05/77
BENT	4	24	14" Conc.	55.	27.	07/05/77
BENT	4	25	14" Conc.	50.	31.	07/06/77
BENT	4	26	14" Conc.	55.	39.	07/05/77
BENT	4	27	14" Conc.	55.	25.	07/05/77
BENT	4	28	14" Conc.	55.	34.	07/06/77
BENT	4	29	14" Conc.	50.	30.	07/07/77
BENT	4	30	14" Conc.	50.	33.	07/07/77
BENT	4	31	14" Conc.	50.	37.	07/07/77
BENT	4	32	14" Conc.	50.	29.	07/07/77
BENT	4	33	14" Conc.	55.	24.	06/24/77
BENT	4	34	14" Conc.	55.	0.	07/07/77
BENT	4	34	14" Conc.	55.	25.	06/24/77
BENT	4	35	14" Conc.	55.	25.	06/24/77
BENT	4	36	14" Conc.	55.	21.	06/24/77
BENT	4	37	14" Conc.	50.	32.	06/20/77
BENT	4	38	14" Conc.	45.	32.	06/20/77
BENT	4	39	14" Conc.	55.	53.	06/12/77
BENT	4	40	14" Conc.	55.	0.	06/20/77
BENT	4	41	14" Conc.	55.	0.	06/20/77
BENT	4	42	14" Conc.	55.	0.	06/20/77
BENT	4	43	14" Conc.	48.	51.	06/20/77
BENT	4	44	14" Conc.	47.	24.	06/20/77
BENT	4	45	14" Conc.	55.	24.	06/24/77
BENT	4	46	14" Conc.	55.	26.	06/24/77
BENT	4	47	14" Conc.	55.	30.	06/25/77

* 0. = No Data Available

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Project #...: 79007-3127-44

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Substructure Name	Pile Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	4	48	14" Conc.	55.	24.	06/25/77
BENT	4	49	14" Conc.	55.	30.	06/24/77
BENT	4	50	14" Conc.	55.	30.	06/24/77
BENT	4	51	14" Conc.	55.	24.	06/25/77
BENT	4	52	14" Conc.	51.33	101.	06/25/77
BENT	4	53	14" Conc.	55.	28.	06/20/77
BENT	4	54	14" Conc.	55.	29.	06/20/77
BENT	4	55	14" Conc.	49.	28.	06/20/77
BENT	4	56	14" Conc.	31.	30.	06/24/77
BENT	4	57	14" Conc.	55.	29.	06/23/77
BENT	4	58	14" Conc.	55.	24.	06/23/77
BENT	4	59	14" Conc.	37.	29.	06/24/77
BENT	4	60	14" Conc.	50.	22.	06/25/77
BENT	4	61	14" Conc.	55.	28.	06/24/77
BENT	4	62	14" Conc.	55.	26.	06/24/77
BENT	4	63	14" Conc.	55.	22.	06/25/77
BENT	4	64	14" Conc.	55.	23.	06/25/77

Average Pile Length (ft) = 51.9729

BENT	5	1	14" Conc.	25.	25.	09/23/77
BENT	5	2	14" Conc.	30.	36.	09/23/77
BENT	5	3	14" Conc.	31.	41.	09/23/77
BENT	5	4	14" Conc.	32.	32.	09/23/77
BENT	5	5	14" Conc.	17.	100.	09/17/77
BENT	5	6	14" Conc.	29.	31.	09/16/77
BENT	5	7	14" Conc.	31.	32.	09/17/77
BENT	5	8	14" Conc.	32.	23.	09/16/77
BENT	5	9	14" Conc.	31.	52.	09/16/77
BENT	5	10	14" Conc.	30.	0.	09/16/77
BENT	5	11	14" Conc.	30.	30.	09/16/77
BENT	5	12	14" Conc.	32.	40.	09/16/77
BENT	5	13	14" Conc.	30.	40.	09/17/77
BENT	5	14	14" Conc.	30.	41.	09/17/77
BENT	5	15	14" Conc.	30.	35.	09/17/77
BENT	5	16	14" Conc.	29.	31.	09/24/77
BENT	5	17	14" Conc.	31.	33.	09/23/77
BENT	5	18	14" Conc.	31.	48.	09/23/77
BENT	5	19	14" Conc.	31.	0.	09/23/77
BENT	5	20	14" Conc.	31.	24.	09/23/77

* 0. = No Data Available

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Contract #: 6978

Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	5	21	14" Conc.	31.	38.	09/16/77
BENT	5	22	14" Conc.	31.	39.	09/16/77
BENT	5	23	14" Conc.	32.	42.	09/16/77
BENT	5	24	14" Conc.	31.	55.	09/16/77
BENT	5	25	14" Conc.	31.	45.	09/16/77
BENT	5	26	14" Conc.	31.	30.	09/16/77
BENT	5	27	14" Conc.	32.	52.	09/16/77
BENT	5	28	14" Conc.	31.	0.	09/16/77
BENT	5	29	14" Conc.	31.	37.	09/23/77
BENT	5	30	14" Conc.	31.	33.	09/23/77
BENT	5	31	14" Conc.	31.	31.	09/23/77
BENT	5	32	14" Conc.	31.	40.	09/23/77
BENT	5	33	14" Conc.	34.	36.	09/19/77
BENT	5	34	14" Conc.	34.	51.	09/19/77
BENT	5	35	14" Conc.	32.	45.	09/19/77
BENT	5	36	14" Conc.	33.	37.	09/22/77
BENT	5	37	14" Conc.	32.	56.	09/15/77
BENT	5	38	14" Conc.	35.	64.	09/15/77
BENT	5	39	14" Conc.	35.	46.	09/15/77
BENT	5	40	14" Conc.	55.	46.	09/12/77
BENT	5	41	14" Conc.	32.	0.	09/15/77
BENT	5	42	14" Conc.	31.	24.	09/15/77
BENT	5	43	14" Conc.	34.	59.	09/15/77
BENT	5	44	14" Conc.	32.	29.	09/19/77
BENT	5	45	14" Conc.	32.	52.	09/19/77
BENT	5	46	14" Conc.	32.	46.	09/19/77
BENT	5	47	14" Conc.	32.	46.	09/19/77
BENT	5	48	14" Conc.	32.	60.	09/22/77
BENT	5	49	14" Conc.	35.	27.	10/03/77
BENT	5	50	14" Conc.	34.	45.	10/03/77
BENT	5	51	14" Conc.	35.	51.	10/03/77
BENT	5	52	14" Conc.	35.	34.	10/03/77
BENT	5	53	14" Conc.	33.	49.	09/21/77
BENT	5	54	14" Conc.	33.	39.	09/21/77
BENT	5	55	14" Conc.	34.	33.	09/21/77
BENT	5	56	14" Conc.	33.	25.	09/21/77
BENT	5	57	14" Conc.	33.	51.	09/21/77
BENT	5	58	14" Conc.	33.	45.	09/21/77
BENT	5	59	14" Conc.	33.	35.	09/21/77
BENT	5	60	14" Conc.	34.	39.	09/21/77

* 0. = No Data Available

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Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	5	61	14" Conc.	34.	31.	10/03/77
BENT	5	62	14" Conc.	33.	49.	10/03/77
BENT	5	63	14" Conc.	34.	49.	10/03/77
BENT	5	64	14" Conc.	34.	47.	10/03/77
BENT	5	65	14" Conc.	35.	33.	09/22/77
BENT	5	66	14" Conc.	35.	30.	09/22/77
BENT	5	67	14" Conc.	35.	37.	09/22/77
BENT	5	68	14" Conc.	34.	29.	09/22/77
BENT	5	69	14" Conc.	35.	54.	09/22/77
BENT	5	70	14" Conc.	35.	0.	09/22/77
BENT	5	71	14" Conc.	33.	0.	09/26/77
BENT	5	72	14" Conc.	55.	42.	09/12/77
BENT	5	73	14" Conc.	36.	0.	09/19/77
BENT	5	74	14" Conc.	37.	47.	09/19/77
BENT	5	75	14" Conc.	37.	32.	09/19/77
BENT	5	76	14" Conc.	36.	44.	09/19/77
BENT	5	77	14" Conc.	34.	36.	09/21/77
BENT	5	78	14" Conc.	34.	32.	09/21/77
BENT	5	79	14" Conc.	34.	34.	09/21/77
BENT	5	80	14" Conc.	35.	33.	09/21/77

Average Pile Length (ft) = 32.925

BENT	6	1	14" Conc.	25.	63.	05/30/77
BENT	6	2	14" Conc.	25.	68.	05/31/77
BENT	6	3	14" Conc.	25.	66.	05/30/77
BENT	6	4	14" Conc.	25.	74.	05/27/77
BENT	6	5	14" Conc.	25.	51.	05/27/77
BENT	6	6	14" Conc.	25.	68.	05/27/77
BENT	6	7	14" Conc.	44.	33.	05/27/77
BENT	6	8	14" Conc.	48.	38.	05/27/77
BENT	6	9	14" Conc.	46.25	47.	05/27/77
BENT	6	10	14" Conc.	25.	0.	05/27/77
BENT	6	11	14" Conc.	25.	0.	05/27/77
BENT	6	12	14" Conc.	25.	73.	05/31/77
BENT	6	13	14" Conc.	22.	92.	05/31/77
BENT	6	14	14" Conc.	22.	73.	05/31/77
BENT	6	15	14" Conc.	25.	78.	05/30/77
BENT	6	16	14" Conc.	22.	127.	05/28/77
BENT	6	17	14" Conc.	24.25	102.	05/23/77

* 0. = No Data Available

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Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	6	18	14" Conc.	25.	114.	05/23/77
BENT	6	19	14" Conc.	25.	91.	05/27/77
BENT	6	20	14" Conc.	25.	94.	05/27/77
BENT	6	21	14" Conc.	47.66	39.	05/27/77
BENT	6	22	14" Conc.	25.	86.	05/30/77
BENT	6	23	14" Conc.	24.	0.	05/31/77
BENT	6	24	14" Conc.	25.	97.	05/30/77
BENT	6	25	14" Conc.	25.	0.	05/30/77
BENT	6	26	14" Conc.	24.	0.	05/30/77
BENT	6	27	14" Conc.	25.	100.	05/30/77
BENT	6	28	14" Conc.	25.	139.	05/27/77
BENT	6	29	14" Conc.	50.	114.	05/24/77
BENT	6	30	14" Conc.	50.	45.	05/26/77
BENT	6	31	14" Conc.	25.	0.	05/22/77
BENT	6	32	14" Conc.	49.	70.	05/27/77
BENT	6	33	14" Conc.	49.	64.	05/26/77
BENT	6	34	14" Conc.	25.	61.	05/26/77
BENT	6	35	14" Conc.	22.25	110.	05/31/77
BENT	6	36	14" Conc.	25.	63.	05/28/77
BENT	6	37	14" Conc.	24.41	65.	05/28/77
BENT	6	38	14" Conc.	25.	0.	05/28/77
BENT	6	39	14" Conc.	25.	0.	05/28/77
BENT	6	40	14" Conc.	45.	0.	05/28/77
BENT	6	41	14" Conc.	45.	0.	05/28/77
BENT	6	42	14" Conc.	45.	0.	05/28/77
BENT	6	43	14" Conc.	45.	0.	05/28/77
BENT	6	44	14" Conc.	46.41	0.	05/28/77
BENT	6	45	14" Conc.	46.	0.	05/26/77
BENT	6	46	14" Conc.	25.	62.	05/26/77
BENT	6	47	14" Conc.	25.	102.	05/26/77
BENT	6	48	14" Conc.	25.	72.	05/28/77
BENT	6	49	14" Conc.	25.	62.	05/28/77
BENT	6	50	14" Conc.	23.58	145.	05/07/77
BENT	6	51	14" Conc.	25.	88.	05/07/77
BENT	6	52	14" Conc.	25.	83.	05/07/77
BENT	6	53	14" Conc.	50.	105.	05/24/77
BENT	6	54	14" Conc.	25.	111.	05/28/77
BENT	6	55	14" Conc.	25.	55.	05/28/77
BENT	6	56	14" Conc.	49.25	68.	05/25/77
BENT	6	57	14" Conc.	50.	107.	05/25/77

* 0. = No Data Available

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Substructure Name	Pile Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	6	58	14" Conc.	25.	89.	05/30/77
BENT	6	59	14" Conc.	25.	79.	05/28/77
BENT	6	60	14" Conc.	25.	52.	05/30/77

Average Pile Length (ft) = 31.0677

BENT	7	1	14" Conc.	45.	31.	11/14/76
BENT	7	2	14" Conc.	45.	29.	11/10/76
BENT	7	3	14" Conc.	37.	0.	11/10/76
BENT	7	4	14" Conc.	25.	0.	11/05/76
BENT	7	5	14" Conc.	45.	37.	11/04/76
BENT	7	6	14" Conc.	45.	36.	11/04/76
BENT	7	7	14" Conc.	45.	32.	11/05/76
BENT	7	8	14" Conc.	45.	35.	11/04/76
BENT	7	9	14" Conc.	45.	37.	11/04/76
BENT	7	10	14" Conc.	45.	37.	11/04/76
BENT	7	11	14" Conc.	45.	32.	11/04/76
BENT	7	12	14" Conc.	45.	0.	11/04/76
BENT	7	13	14" Conc.	42.	29.	11/15/76
BENT	7	14	14" Conc.	45.	30.	11/15/76
BENT	7	15	14" Conc.	45.	39.	11/04/76
BENT	7	16	14" Conc.	45.	38.	11/04/76
BENT	7	17	14" Conc.	45.	42.	11/04/76
BENT	7	18	14" Conc.	45.	41.	11/04/76
BENT	7	19	14" Conc.	45.	40.	11/04/76
BENT	7	20	14" Conc.	45.	45.	11/04/76
BENT	7	21	14" Conc.	45.	44.	11/12/76
BENT	7	22	14" Conc.	25.	0.	11/12/76
BENT	7	23	14" Conc.	45.	29.	11/12/76
BENT	7	24	14" Conc.	45.	31.	11/12/76
BENT	7	25	14" Conc.	45.	34.	11/03/76
BENT	7	26	14" Conc.	45.	49.	11/03/76
BENT	7	27	14" Conc.	45.	42.	11/05/76
BENT	7	28	14" Conc.	45.	57.	11/05/76
BENT	7	29	14" Conc.	55.	75.	10/29/76
BENT	7	30	14" Conc.	45.	45.	11/05/76
BENT	7	31	14" Conc.	45.	29.	11/05/76
BENT	7	32	14" Conc.	45.	42.	11/03/76
BENT	7	33	14" Conc.	45.	65.	11/05/76
BENT	7	34	14" Conc.	45.	44.	11/05/76

* 0. = No Data Available

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Project #...: 79007-3127-44

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Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	7	35	14" Conc.	45.	38.	11/05/76
BENT	7	36	14" Conc.	35.	0.	11/08/76
BENT	7	37	14" Conc.	25.	46.	11/01/76
BENT	7	38	14" Conc.	25.	60.	11/11/76
BENT	7	39	14" Conc.	25.	0.	11/11/76
BENT	7	40	14" Conc.	25.	0.	11/06/76
BENT	7	41	14" Conc.	25.	0.	11/06/76
BENT	7	42	14" Conc.	25.	0.	11/06/76
BENT	7	43	14" Conc.	25.	136.	11/08/76
BENT	7	44	14" Conc.	25.	112.	11/08/76
BENT	7	45	14" Conc.	25.	0.	11/08/76
BENT	7	46	14" Conc.	25.	94.	11/12/76
BENT	7	47	14" Conc.	25.	61.	11/11/76
BENT	7	48	14" Conc.	25.	57.	11/11/76
BENT	7	49	14" Conc.	25.	60.	11/12/76
BENT	7	50	14" Conc.	25.	0.	11/09/76
BENT	7	51	14" Conc.	25.	0.	11/12/76
BENT	7	52	14" Conc.	25.	100.	11/12/76
BENT	7	53	14" Conc.	25.	100.	11/14/76
BENT	7	54	14" Conc.	25.	92.	11/14/76
BENT	7	55	14" Conc.	25.	84.	11/08/76
BENT	7	56	14" Conc.	25.	81.	11/08/76
BENT	7	57	14" Conc.	25.	96.	11/08/76
BENT	7	58	14" Conc.	25.	98.	11/12/76
BENT	7	59	14" Conc.	25.	71.	11/12/76
BENT	7	60	14" Conc.	25.	74.	11/12/76
BENT	7	61	14" Conc.	25.	64.	11/12/76
BENT	7	62	14" Conc.	25.	62.	11/12/76
BENT	7	63	14" Conc.	18.41	0.	11/09/76
BENT	7	64	14" Conc.	25.	0.	11/09/76
BENT	7	65	14" Conc.	55.	39.	11/03/76
BENT	7	66	14" Conc.	25.	0.	11/09/76
BENT	7	67	14" Conc.	25.	0.	11/09/76
BENT	7	68	14" Conc.	25.	0.	11/09/76
BENT	7	69	14" Conc.	25.	0.	11/09/76
BENT	7	70	14" Conc.	25.	0.	11/09/76
BENT	7	71	14" Conc.	25.	0.	11/09/76
BENT	7	72	14" Conc.	25.	0.	11/09/76

* 0. = No Data Available

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Substructure Name	File Number	File Number	File Size	In Place Length (ft)	*Bearing (Tons)	Drive Date

Average Pile Length (ft) = 34.6168						
BENT	8	1	14" Conc.	45.	27.	12/07/76
BENT	8	2	14" Conc.	45.	29.	12/07/76
BENT	8	3	14" Conc.	45.	26.	12/07/76
BENT	8	4	14" Conc.	45.	25.	12/06/76
BENT	8	5	14" Conc.	43.	25.	12/06/76
BENT	8	6	14" Conc.	45.	27.	12/03/76
BENT	8	7	14" Conc.	45.	26.	12/03/76
BENT	8	8	14" Conc.	45.	25.	12/03/76
BENT	8	9	14" Conc.	45.	26.	12/03/76
BENT	8	10	14" Conc.	45.	0.	12/07/76
BENT	8	11	14" Conc.	45.	0.	12/07/76
BENT	8	12	14" Conc.	45.	0.	12/07/76
BENT	8	13	14" Conc.	45.	24.	12/04/76
BENT	8	14	14" Conc.	45.	25.	12/06/76
BENT	8	15	14" Conc.	45.	28.	12/06/76
BENT	8	16	14" Conc.	45.	33.	12/06/76
BENT	8	17	14" Conc.	45.	31.	12/07/76
BENT	8	18	14" Conc.	45.	24.	12/02/76
BENT	8	19	14" Conc.	45.	30.	12/06/76
BENT	8	20	14" Conc.	40.	25.	12/03/76
BENT	8	21	14" Conc.	45.	27.	12/03/76
BENT	8	22	14" Conc.	45.	26.	12/03/76
BENT	8	23	14" Conc.	45.	38.	12/06/76
BENT	8	24	14" Conc.	45.	36.	12/06/76
BENT	8	25	14" Conc.	45.	30.	12/06/76
BENT	8	26	14" Conc.	45.	24.	12/03/76
BENT	8	27	14" Conc.	45.	28.	12/01/76
BENT	8	28	14" Conc.	45.	25.	12/02/76
BENT	8	29	14" Conc.	55.	37.	11/27/76
BENT	8	30	14" Conc.	45.	34.	12/02/76
BENT	8	31	14" Conc.	45.	28.	12/02/76
BENT	8	32	14" Conc.	45.	25.	12/02/76
BENT	8	33	14" Conc.	45.	26.	12/01/76
BENT	8	34	14" Conc.	45.	28.	12/03/76
BENT	8	35	14" Conc.	45.	23.	12/03/76
BENT	8	36	14" Conc.	45.	25.	12/03/76
BENT	8	37	14" Conc.	45.	28.	12/04/76

* 0. = No Data Available

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Project #...: 79007-3127-44

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Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	8	38	14" Conc.	45.	35.	12/05/76
BENT	8	39	14" Conc.	45.	26.	12/05/76
BENT	8	40	14" Conc.	45.	0.	12/05/76
BENT	8	41	14" Conc.	45.	0.	12/05/76
BENT	8	42	14" Conc.	45.	33.	12/05/76
BENT	8	43	14" Conc.	45.	27.	12/01/76
BENT	8	44	14" Conc.	45.	32.	12/01/76
BENT	8	45	14" Conc.	45.	30.	12/01/76
BENT	8	46	14" Conc.	45.	45.	12/03/76
BENT	8	47	14" Conc.	45.	40.	12/04/76
BENT	8	48	14" Conc.	45.	33.	12/03/76
BENT	8	49	14" Conc.	45.	32.	12/01/76
BENT	8	50	14" Conc.	45.	32.	12/01/76
BENT	8	51	14" Conc.	45.	28.	12/01/76
BENT	8	52	14" Conc.	45.	36.	11/30/76
BENT	8	53	14" Conc.	45.	38.	11/30/76
BENT	8	54	14" Conc.	45.	32.	11/30/76
BENT	8	55	14" Conc.	45.	33.	11/30/76
BENT	8	56	14" Conc.	45.	30.	11/30/76
BENT	8	57	14" Conc.	45.	40.	11/30/76
BENT	8	58	14" Conc.	45.	31.	12/01/76
BENT	8	59	14" Conc.	45.	31.	11/30/76
BENT	8	60	14" Conc.	45.	32.	12/01/76
BENT	8	61	14" Conc.	45.	30.	11/30/76
BENT	8	62	14" Conc.	45.	24.	11/27/76
BENT	8	63	14" Conc.	45.	31.	11/30/76
BENT	8	64	14" Conc.	45.	27.	11/25/76
BENT	8	65	14" Conc.	45.	35.	11/25/76
BENT	8	66	14" Conc.	45.	28.	11/25/76
BENT	8	67	14" Conc.	45.	29.	11/25/76
BENT	8	68	14" Conc.	55.	39.	11/22/76
BENT	8	69	14" Conc.	45.	24.	11/28/76
BENT	8	70	14" Conc.	45.	31.	11/28/76
BENT	8	71	14" Conc.	45.	41.	11/28/76
BENT	8	72	14" Conc.	45.	25.	11/28/76

Average Pile Length (ft) = 45.1806

BENT	9	1	14" Conc.	20.	49.	08/26/76
BENT	9	2	14" Conc.	20.	92.	08/26/76

* 0. = No Data Available

BRIDGE PILE SUMMARY REPORT

Federal ID#: 79I00400059

Page #: 16

SEP 16 1999

Location #.: 79-I0040-0509-RL

Print Date: 08/30/95

Crossing...: WOLF RIVER

*

Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	9	3	14" Conc.	20.	104.	08/26/76
BENT	9	4	14" Conc.	20.	93.	08/25/76
BENT	9	5	14" Conc.	30.	61.	08/19/76
BENT	9	6	14" Conc.	20.	92.	08/25/76
BENT	9	7	14" Conc.	20.	68.	08/25/76
BENT	9	8	14" Conc.	20.	75.	08/25/76
BENT	9	9	14" Conc.	20.	64.	08/25/76
BENT	9	10	14" Conc.	20.	49.	08/27/76
BENT	9	11	14" Conc.	20.	0.	08/27/76
BENT	9	12	14" Conc.	20.	0.	08/27/76
BENT	9	13	14" Conc.	20.	0.	08/27/76
BENT	9	14	14" Conc.	50.	0.	08/27/76
BENT	9	15	14" Conc.	20.	78.	08/27/76
BENT	9	16	14" Conc.	20.	49.	08/23/76
BENT	9	17	14" Conc.	20.	49.	08/23/76
BENT	9	18	14" Conc.	20.	85.	08/23/76
BENT	9	19	14" Conc.	20.	33.	08/27/76
BENT	9	20	14" Conc.	20.	68.	08/27/76
BENT	9	21	14" Conc.	17.41	52.	08/27/76
BENT	9	22	14" Conc.	20.	53.	08/27/76
BENT	9	23	14" Conc.	25.	26.	08/19/76
BENT	9	24	14" Conc.	20.	55.	08/27/76
BENT	9	25	14" Conc.	20.	68.	08/27/76
BENT	9	26	14" Conc.	20.	49.	08/27/76
BENT	9	27	14" Conc.	20.	53.	08/27/76
BENT	9	28	14" Conc.	20.	45.	08/27/76
BENT	9	29	14" Conc.	20.	58.	08/28/76
BENT	9	30	14" Conc.	20.	52.	08/28/76
BENT	9	31	14" Conc.	20.	47.	08/27/76
BENT	9	32	14" Conc.	20.	45.	08/28/76
BENT	9	33	14" Conc.	20.	57.	08/28/76
BENT	9	34	14" Conc.	20.	52.	08/27/76
BENT	9	35	14" Conc.	20.	45.	08/28/76
BENT	9	36	14" Conc.	20.	57.	08/28/76
BENT	9	37	14" Conc.	20.	58.	08/30/76
BENT	9	38	14" Conc.	20.	44.	08/30/76
BENT	9	39	14" Conc.	20.	53.	08/30/76
BENT	9	40	14" Conc.	20.	40.	08/30/76
BENT	9	41	14" Conc.	20.	52.	08/24/76
BENT	9	42	14" Conc.	20.	0.	08/28/76

* 0. = No Data Available

BRIDGE PILE SUMMARY REPORT

SEP 16 1999

Federal ID#: 79I00400059

Page #: 17

Location #.: 79-I0040-0509-RL

Print Date: 08/30/95

Crossing...: WOLF RIVER

*

Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	Pile Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	9	43	14" Conc.	20.	40.	08/28/76
BENT	9	44	14" Conc.	20.	44.	08/28/76
BENT	9	45	14" Conc.	20.	55.	08/28/76
BENT	9	46	14" Conc.	20.	64.	08/28/76
BENT	9	47	14" Conc.	20.	52.	08/28/76
BENT	9	48	14" Conc.	20.	44.	08/28/76
BENT	9	49	14" Conc.	20.	80.	08/30/76
BENT	9	50	14" Conc.	20.	67.	08/30/76
BENT	9	51	14" Conc.	20.	54.	08/30/76
BENT	9	52	14" Conc.	20.	83.	08/30/76
BENT	9	53	14" Conc.	20.	81.	08/30/76
BENT	9	54	14" Conc.	20.	74.	08/30/76
BENT	9	55	14" Conc.	20.	72.	08/30/76
BENT	9	56	14" Conc.	20.	53.	08/28/76
BENT	9	57	14" Conc.	20.	52.	08/28/76
BENT	9	58	14" Conc.	20.	71.	08/28/76
BENT	9	59	14" Conc.	20.	55.	08/28/76
BENT	9	60	14" Conc.	20.	64.	08/28/76

Average Pile Length (ft) = 20.7068

BENT	10	1	14" Conc.	30.	114.	08/15/77
BENT	10	2	14" Conc.	30.	100.	08/15/77
BENT	10	3	14" Conc.	30.	121.	08/15/77
BENT	10	4	14" Conc.	30.	126.	08/08/77
BENT	10	5	14" Conc.	55.	58.	08/09/77
BENT	10	6	14" Conc.	30.	100.	08/11/77
BENT	10	7	14" Conc.	30.	103.	08/11/77
BENT	10	8	14" Conc.	30.	100.	08/11/77
BENT	10	9	14" Conc.	30.	100.	08/11/77
BENT	10	10	14" Conc.	30.	0.	08/12/77
BENT	10	11	14" Conc.	30.	0.	08/12/77
BENT	10	12	14" Conc.	30.	0.	08/12/77
BENT	10	13	14" Conc.	30.	64.	08/12/77
BENT	10	14	14" Conc.	30.	100.	08/12/77
BENT	10	15	14" Conc.	30.	80.	08/12/77
BENT	10	16	14" Conc.	30.	100.	08/12/77
BENT	10	17	14" Conc.	30.	87.	08/12/77
BENT	10	18	14" Conc.	30.	98.	08/12/77
BENT	10	19	14" Conc.	30.	72.	08/12/77

* 0. = No Data Available

BRIDGE PILE SUMMARY REPORT

SEP 16 1999

Federal ID#: 79I00400059

Page #: 18

Location #: 79-I0040-0509-RL

Print Date: 08/30/95

Crossing...: WOLF RIVER

*

Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	10	20	14" Conc.	30.	84.	08/12/77
BENT	10	21	14" Conc.	30.	96.	08/12/77
BENT	10	22	14" Conc.	30.	97.	08/12/77
BENT	10	23	14" Conc.	55.	42.	08/09/77
BENT	10	24	14" Conc.	30.	101.	08/12/77
BENT	10	25	14" Conc.	30.	93.	08/15/77
BENT	10	26	14" Conc.	30.	115.	08/15/77
BENT	10	27	14" Conc.	30.	102.	08/15/77
BENT	10	28	14" Conc.	30.	101.	08/15/77
BENT	10	29	14" Conc.	30.	95.	08/15/77
BENT	10	30	14" Conc.	30.	94.	08/15/77
BENT	10	31	14" Conc.	30.	77.	08/15/77
BENT	10	32	14" Conc.	30.	73.	08/15/77
BENT	10	33	14" Conc.	30.	89.	08/15/77
BENT	10	34	14" Conc.	30.	100.	08/15/77
BENT	10	35	14" Conc.	30.	101.	08/15/77
BENT	10	36	14" Conc.	30.	104.	08/15/77

Average Pile Length (ft) = 31.3889

BENT	11	1	14" Conc.	16.	23.	08/25/76
BENT	11	2	14" Conc.	25.	59.	08/27/76
BENT	11	3	14" Conc.	21.	77.	08/19/76
BENT	11	4	14" Conc.	22.	120.	08/14/76
BENT	11	5	14" Conc.	20.	53.	08/09/76
BENT	11	6	14" Conc.	25.	100.	08/24/76
BENT	11	7	14" Conc.	19.	72.	08/24/76
BENT	11	8	14" Conc.	17.	72.	08/25/76
BENT	11	9	14" Conc.	25.	55.	08/20/76
BENT	11	10	14" Conc.	23.	0.	08/20/76
BENT	11	11	14" Conc.	25.	0.	08/20/76
BENT	11	12	14" Conc.	23.	53.	08/04/76
BENT	11	13	14" Conc.	15.	0.	08/04/76
BENT	11	14	14" Conc.	25.	54.	08/23/76
BENT	11	15	14" Conc.	25.	50.	08/23/76
BENT	11	16	14" Conc.	25.	57.	08/23/76
BENT	11	17	14" Conc.	25.	48.	08/20/76
BENT	11	18	14" Conc.	25.	55.	08/20/76
BENT	11	19	14" Conc.	25.	90.	08/25/76
BENT	11	20	14" Conc.	25.	52.	08/20/76

* 0. = No Data Available

BRIDGE PILE SUMMARY REPORT

SEP 16 1999

Federal ID#: 79I00400059

Page #: 19

Location #.: 79-I0040-0509-RL

Print Date: 08/30/95

Crossing...: WOLF RIVER

*

Project #...: 79007-3127-44

Contract #: 6978

Substructure Name	Pile Number	Pile Number	Pile Size	In Place Length (ft)	*Bearing (Tons)	Drive Date
BENT	11	21	14" Conc.	25.	50.	08/20/76
BENT	11	22	14" Conc.	25.	100.	08/25/76
BENT	11	23	14" Conc.	25.	93.	08/25/76
BENT	11	24	14" Conc.	25.	90.	08/25/76

Average Pile Length (ft) = 22.9583

BENT	12	1	14" Conc.	25.	113.	07/25/77
BENT	12	2	14" Conc.	25.	99.	07/25/77
BENT	12	3	14" Conc.	25.	101.	07/25/77
BENT	12	4	14" Conc.	25.	84.	07/25/77
BENT	12	5	14" Conc.	25.	81.	07/25/77
BENT	12	6	14" Conc.	25.	78.	07/25/77
BENT	12	7	14" Conc.	25.	78.	07/25/77
BENT	12	8	14" Conc.	25.	100.	07/25/77
BENT	12	9	14" Conc.	25.	88.	07/25/77
BENT	12	10	14" Conc.	25.	88.	07/25/77
BENT	12	11	14" Conc.	25.	100.	07/25/77
BENT	12	12	14" Conc.	25.	0.	07/25/77
BENT	12	13	14" Conc.	53.	42.	07/20/77
BENT	12	14	14" Conc.	25.	121.	07/26/77
BENT	12	15	14" Conc.	25.	111.	07/26/77
BENT	12	16	14" Conc.	25.	114.	07/26/77
BENT	12	17	14" Conc.	25.	114.	07/26/77
BENT	12	18	14" Conc.	25.	97.	07/26/77
BENT	12	19	14" Conc.	25.	100.	07/26/77
BENT	12	20	14" Conc.	25.	115.	07/26/77
BENT	12	21	14" Conc.	25.	82.	07/26/77
BENT	12	22	14" Conc.	25.	104.	07/26/77
BENT	12	23	14" Conc.	25.	110.	07/26/77
BENT	12	24	14" Conc.	25.	123.	07/26/77

Average Pile Length (ft) = 26.1667

* 0. = No Data Available

SCOUR REPAIR

SHELBY COUNTY

79-I40-5.09 / WOLF RIVER

PROJECT NO. 79002-4130-04

DONE BY REGION IV FORCES

WORK IN PROGRESS
5/19/98

FINAL COMPLETION PICTURES
IN REPAIR OFFICE DATED
10/2/98

PROJECT NUMBER REQUEST

(Include location map)

Date: 1-21-98

Requestor: MICHAEL ANDERSON
BRIDGE REPAIR

Phone No: 741-8398

Preliminary Engineering No.: 79002-4129-04

Construction No.: 79002-4130-04

County: SHELBY Bridge No.: 79-I40-5.09(L&R)

Crossing: WOLF RIVER

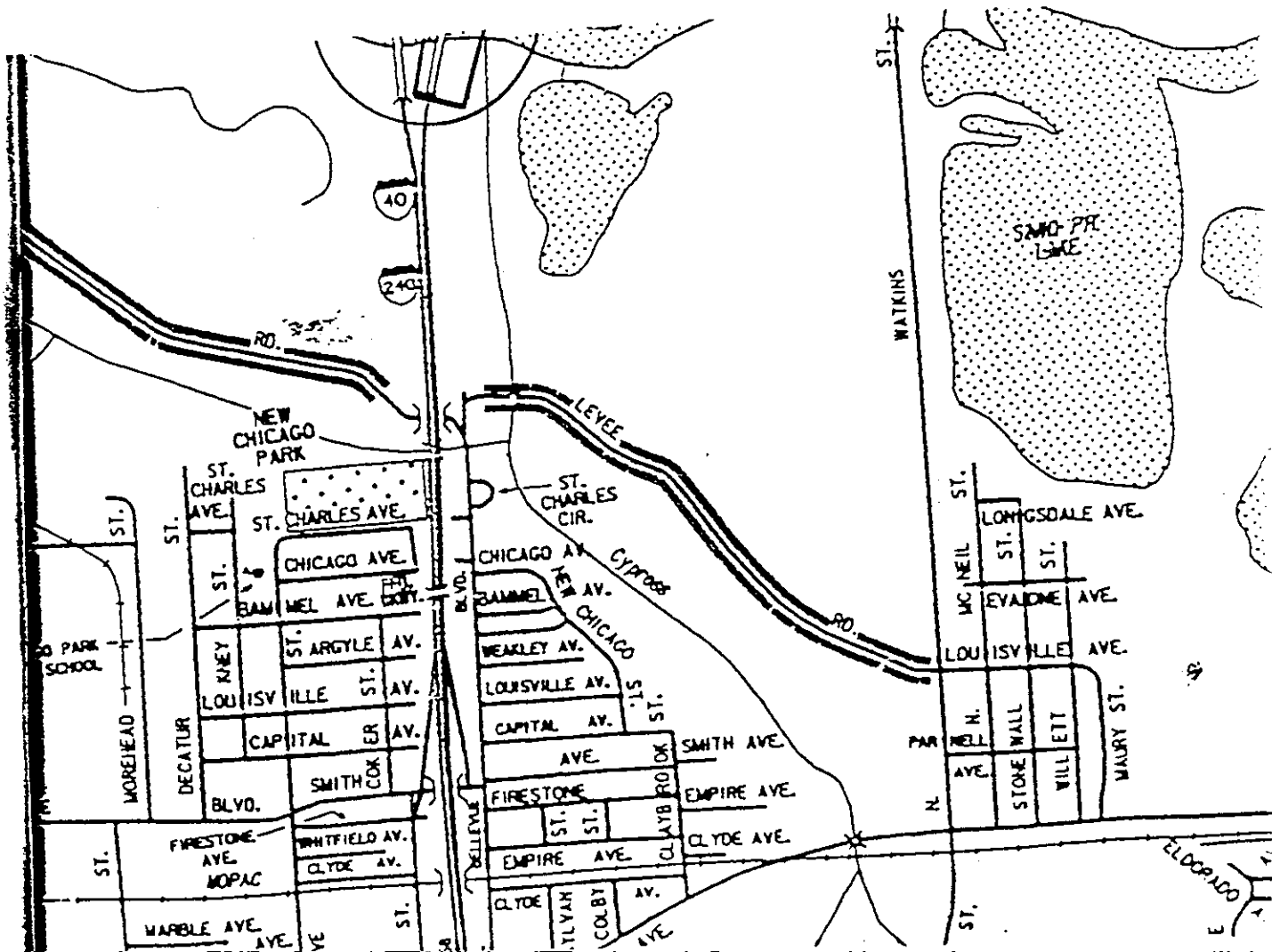
Type Work: INSTALLATION OF RIP-RAP AS BANK STABILIZATION
& REMOVAL OF STREAM OBSTRUCTION FROM CHANNEL

Est. Proj. Cost: \$ 262,500

Est. P.E. Cost: \$ 30,000.00

Proposed Letting: MAY '98

Consultant: GARVER+GARVER





STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE INSPECTION AND REPAIR OFFICE
NASHVILLE, TENNESSEE 37243-0338

Memorandum

DATE: September 17, 1998

TO: Mr. Bill Hazlerig, Region IV Bridge Engineer

FROM: Wayne J. Seger, Civil Engineering Manager 1 *MRA*
for

RE: 79-I40-5.09/ WOLF RIVER

As discussed, I am sending you this memo to confirm permission for removal of the cofferdams that are obstructing flow in the area of these structures.

It is the opinion of Mr. John Hewitt from the permitting section that neither the Tennessee Department of Environment and Conservation nor the United States Army Corps of Engineers requires a permit to remove a structure from an area. Further, as long as you are not in "waters of the State", we will be in full compliance with both agencies' regulations.

If you find that you are unable to get to one or both of these structures without bringing machinery or equipment directly into the stream flow, please stop work before this occurs.

If you have any further questions, please feel free to call me.

Wayne Jeger



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
NASHVILLE, TENNESSEE 37243-0339

February 5, 1998

Mr. Daniel C. Eagar
Tennessee Department of Environment and Conservation
Division of Water Pollution Control
Natural Resource Section
6th Floor L. & C. Annex
401 Church Street
Nashville, Tennessee 37243-1534

Subject: Emergency Road Repair
Interstates 40 and 240
At Wolf River
Shelby County

Dear Mr. Eagar:

In accordance with the Tennessee Department of Environment and Conservation's General Permit for Emergency Road Repair, this office is providing a portion of the USGS quad map for Northwest Memphis, TN (404-NE), showing the location of an emergency road repair at the Wolf River, where the riverbank has scoured to the point of endangering the bridge piers. Immediate repairs are necessary to protect the safety of the motoring public. This is a "no-plans" operation by TDOT maintenance forces.

The repair, which will be conducted the week of February 9, will consist of the placement of soil and rip-rap of approximately 15" diameter to temporarily repair approximately 590 linear feet of bank which have been severely scoured, and rip-rap covering the existing substrate adjacent to the bank. It is our understanding that the river flow has scoured the bank and moved several feet toward the pier within the last few months, endangering pier stability due to the relatively short piling used under these piers. We are enclosing aerial photographs taken in 1987 and January 1998, which show the movement of the bank toward pier # 7 on both bridges. We are also enclosing photographs taken at the site.

Please note that the current instability of the banks is resulting in considerable erosion and sedimentation. We believe this project will have an overall benefit to water quality.

Diagrams showing the approximate scope of work are enclosed. Due to the varying conditions at the site, and the highly erodible sediments in the river, the engineer on the site will determine the final scope of work at the time the rip-rap is placed. The thickness of the rip-rap layer will vary between 3' and 6', depending on conditions.

We also are preparing to develop project plans for a more permanent solution to the bank erosion problem, which appears to be caused in part by the configuration of the opposite bank

Mr. Daniel C. Eagar
February 5, 1998
Page 2

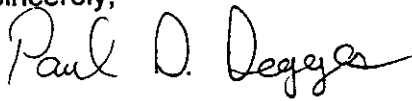
and an existing obstruction in the river upstream of this site. At that time, we will apply for the appropriate permits for the additional work.

By copy of this letter, we request the concurrence of the Corps of Engineers, Memphis District, that this emergency work fits the criteria of one of the Nationwide Permits.

We also request that the Corps inform us at their early convenience if they are planning any related corrective or preservation work for this area of the Wolf River in the future, which may impact or aid potential TDOT work.

Please advise us if you have any questions or if we can be of any assistance.

Sincerely,



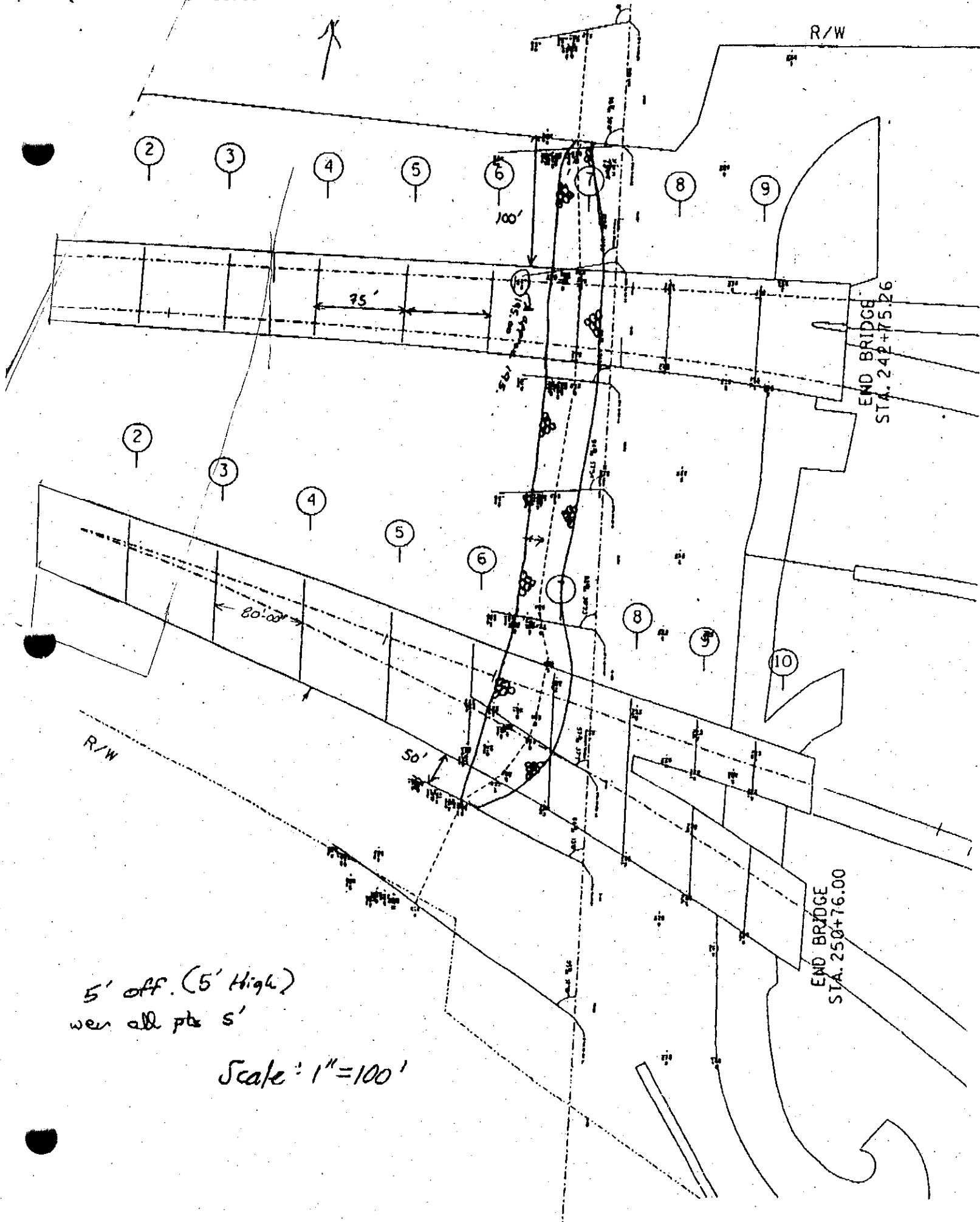
(for)

Edward P. Wasserman
Engineering Director, Structures

Enclosures

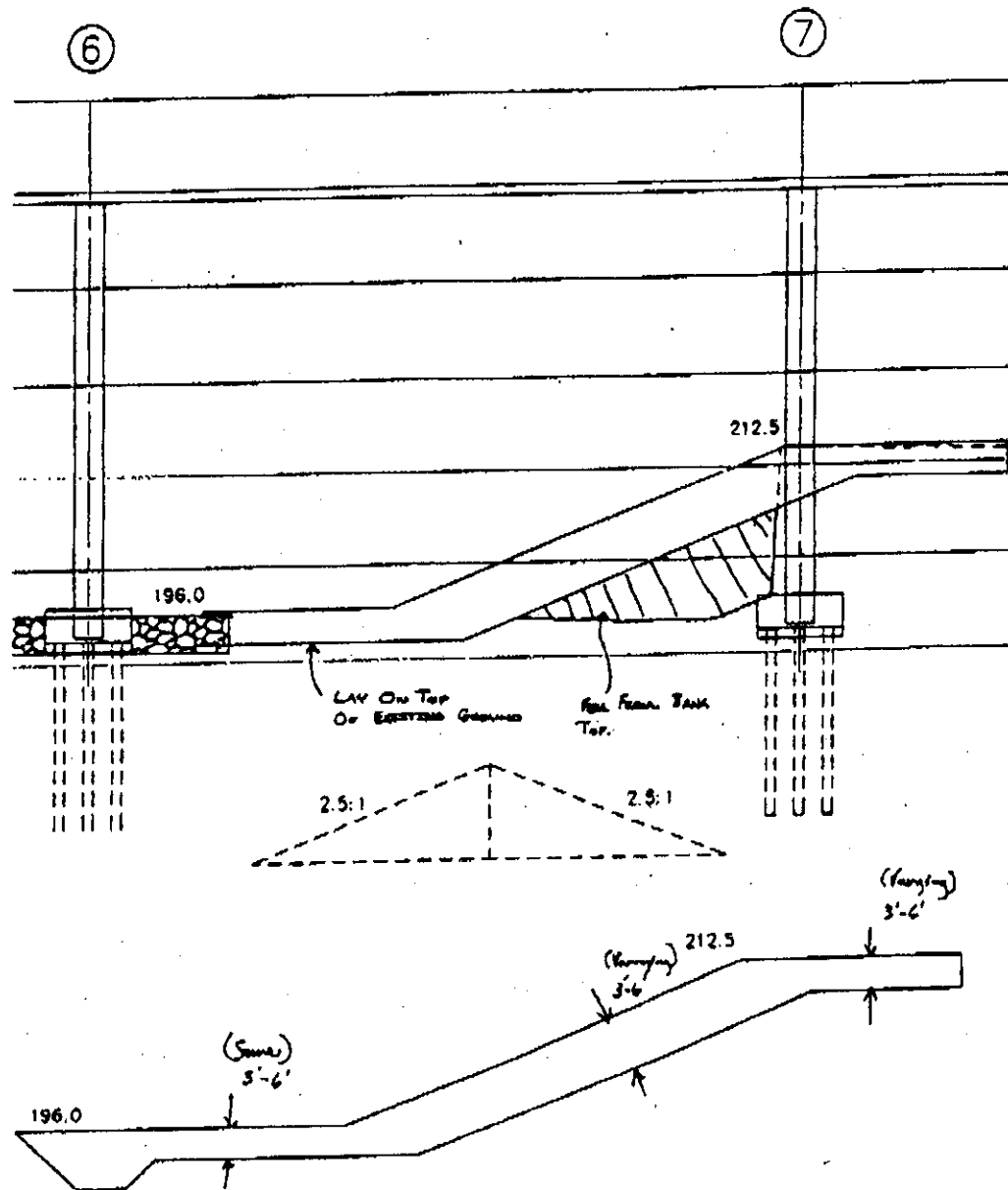
PDD:JLH:pc

cc: Col. Gregory Bean, USACE Memphis District
✓ Mr. Wayne Seger
Mr. Bill Hazlerig
Mr. Bob Englert

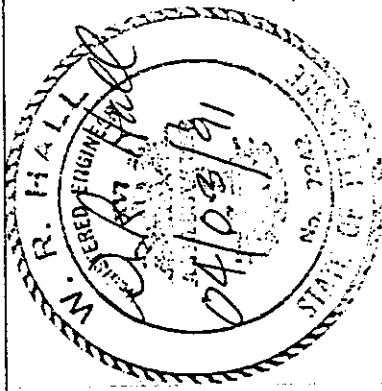
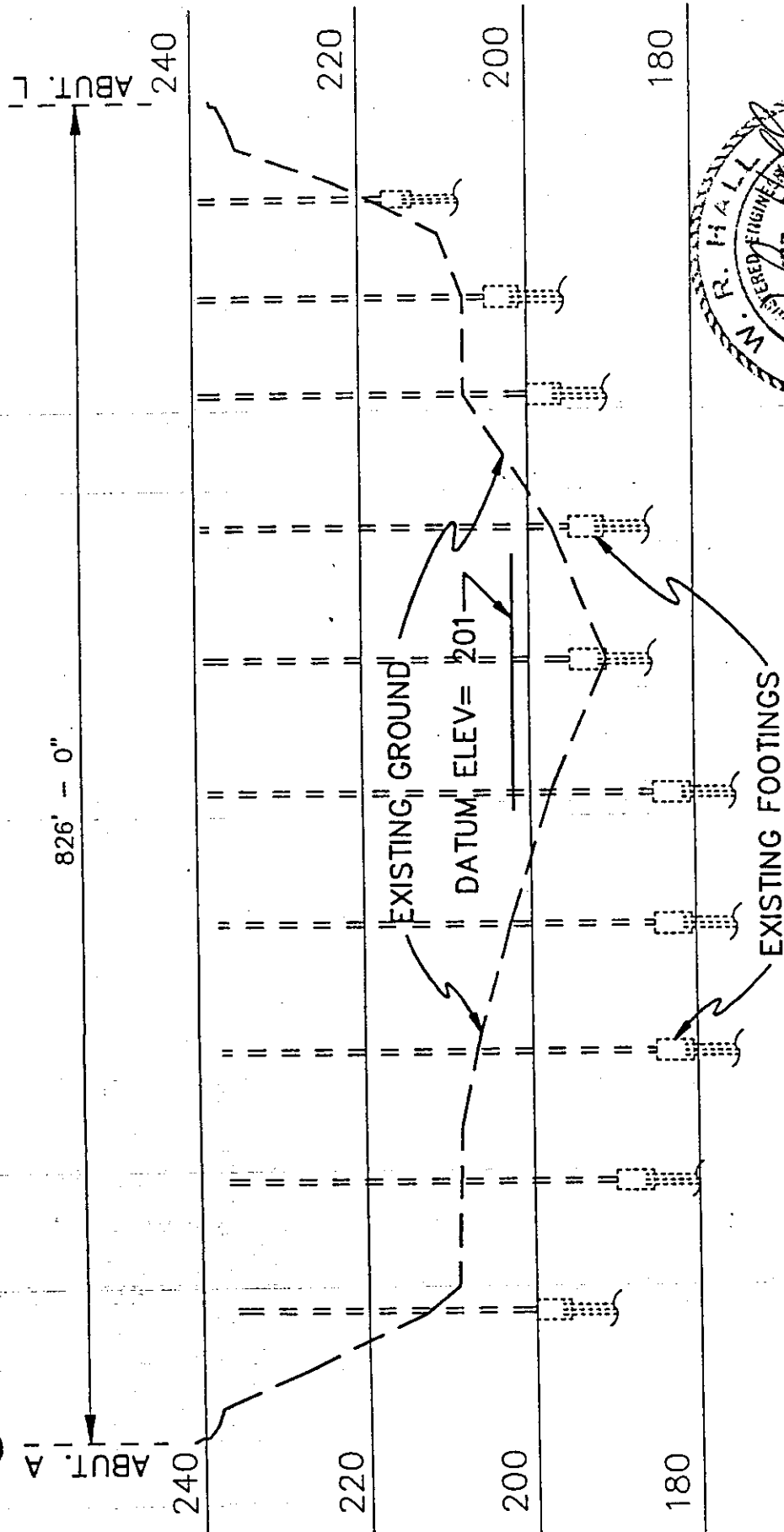


5' off. (5' High)
w/ all pts 5'

Scale: 1"=100'



Scale: Horiz. 1" = 20'
Vert. not to scale



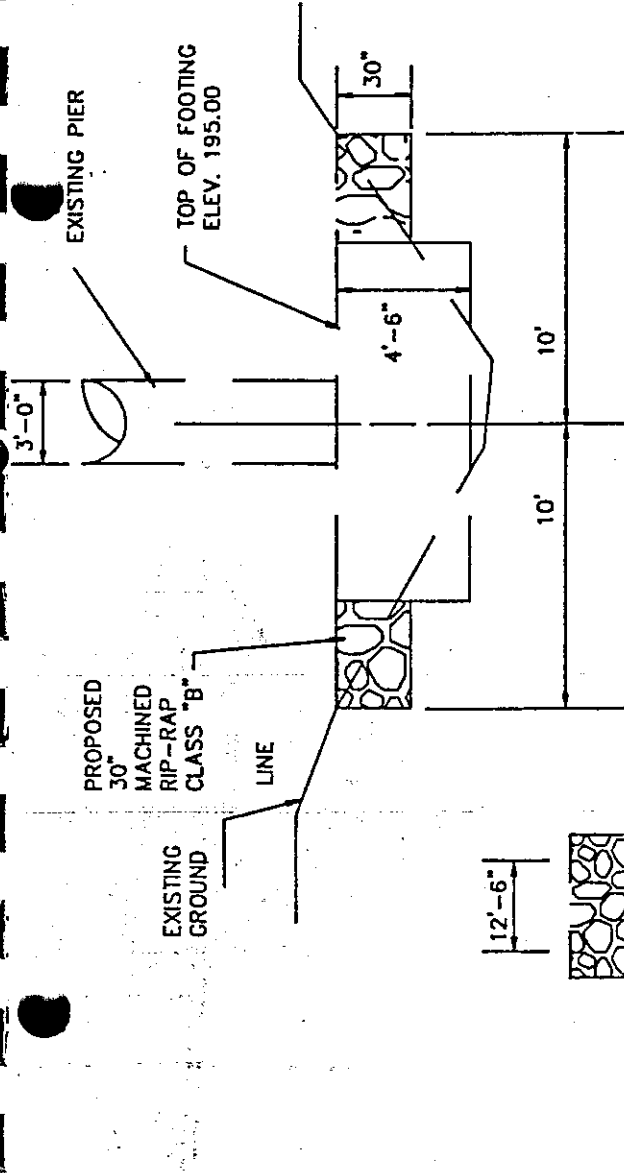
ELEVATION VIEW
 SCALE H: 1" = 100'
 V: 1" = 20'

HYDRAULIC DATA

DRAINAGE AREA = SQ. MI.
 DESIGN DISCHARGE (100 YR.) = 40510 C.F.S.
 100 YR. FLOW AREA = SQ. FT.
 DESIGN VELOCITY = 3.01 F.P.S.
 DESIGN BRIDGE BACKWATER = FT.
 ROADWAY OVERTOPPING ELEV. =

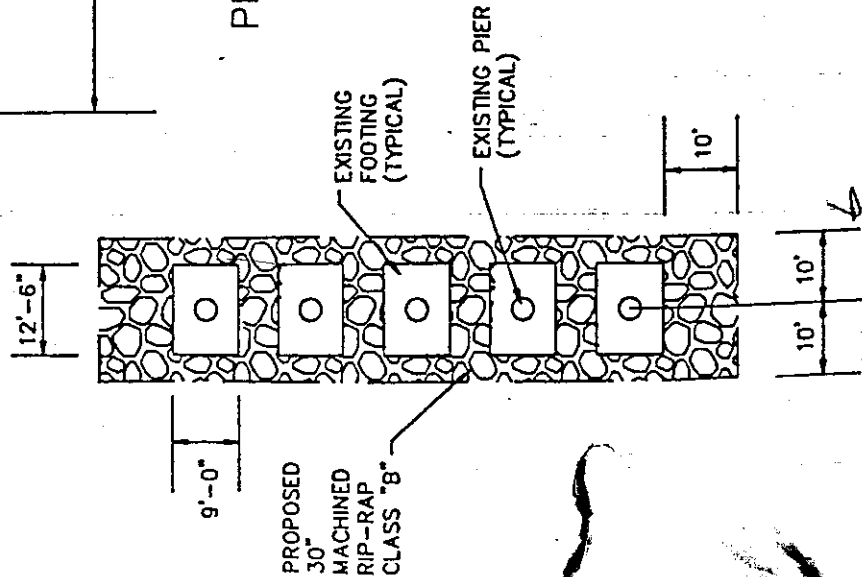
BRIDGE SCOUR REPAIR PROFILE

S.R. 140 OVER WOLF RIVER
 BRIDGE NO. 79-140-5.09 RT.
 SHELBY COUNTY
 1991

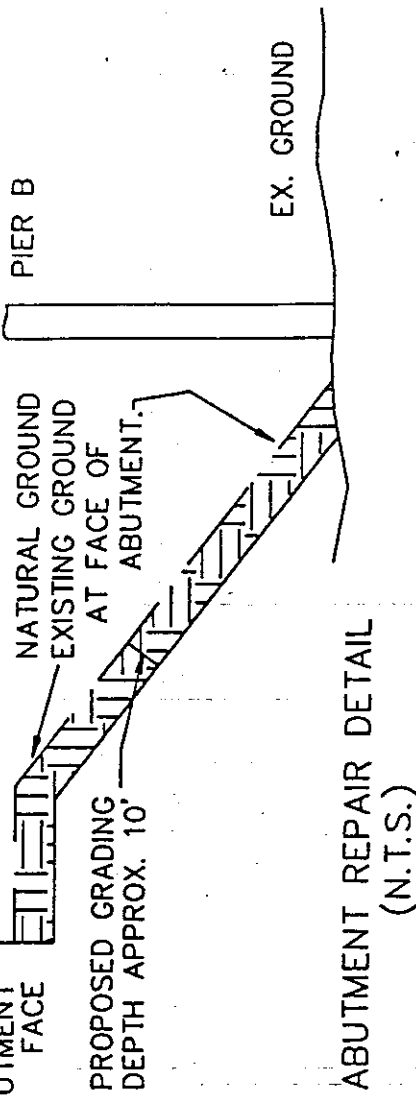


NOTE: LIMITS OF REPAIR
FOR ABUTMENT FACE
NOT TO DAMAGE EXIST.
PIER OR STRUCTURE.

**PIER G
PROFILE VIEW DETAIL
(N.T.S.)**



**EXISTING BRIDGE
EXISTING ABUTMENT
FACE**



BRIDGE SCOUR REPAIR DETAIL

I-40/240 OVER WOLF RIVER
BRIDGE NO. 79-140-5.09

SHELBY COUNTY
1991

**DETAILS FOR
BOTH STRUCTURES**

**PIER G
PLAN VIEW DETAIL
(N.T.S.)**

S T A T E

O F

T E N N E S S E E

SHEET 1 OF 2

Bids to be Received until 10:00 A.M. on June 14, 1991.

DESCRIPTION OF WORK

In the State of
Tennessee, in the County of Shelby:
consisting of the Contract Rip-Rap
Installation in Shelby County on I-
40-5.09 left and right over Wolf
River.

Project Ref. No.

Project No.

79007-4165-04Road 0.00 MilesBridge 0.00 MilesCounty ShelbyTOTAL 0.00 Miles

ESTIMATE OF QUANTITIES AND SCHEDULE OF PRICES
(Sequence Numbers are for Departmental Use Only)

S T A T E

O F

T E N N E S S E E

SHEET 2 OF 2

PROJECT NO. 79007-4165-04

COUNTY: Shelby

ITEM & SEQ. NO.	UNIT MEAS. AND QNTY.	ITEMS AND UNIT PRICES (SEQ. NO. FOR DEPT. USE ONLY)	UNIT PRICE	AMOUNT

203-01	CU. YD. 4500	ROAD & DRAINAGE EXCAVATION(UNCLASS) @ _____ DOLLARS		
20		_____ CENTS		
209-06	BALE 200	BALED HAY OR STRAW EROSION CHECKS @ _____ DOLLARS		
30		_____ CENTS		
209-08	L.F. 400	TEMPORARY SILT FENCES @ _____ DOLLARS		
40		_____ CENTS		
709-05.08	TONS 1765	MACHINED RIP-RAP [CLASS B] @ _____ DOLLARS		
50		_____ CENTS		
712-01	L.S. 1	TRAFFIC CONTROL @ _____ DOLLARS		
60		_____ CENTS		
717-01	L.S. 1	MOBILIZATION @ _____ DOLLARS		
70		_____ CENTS		

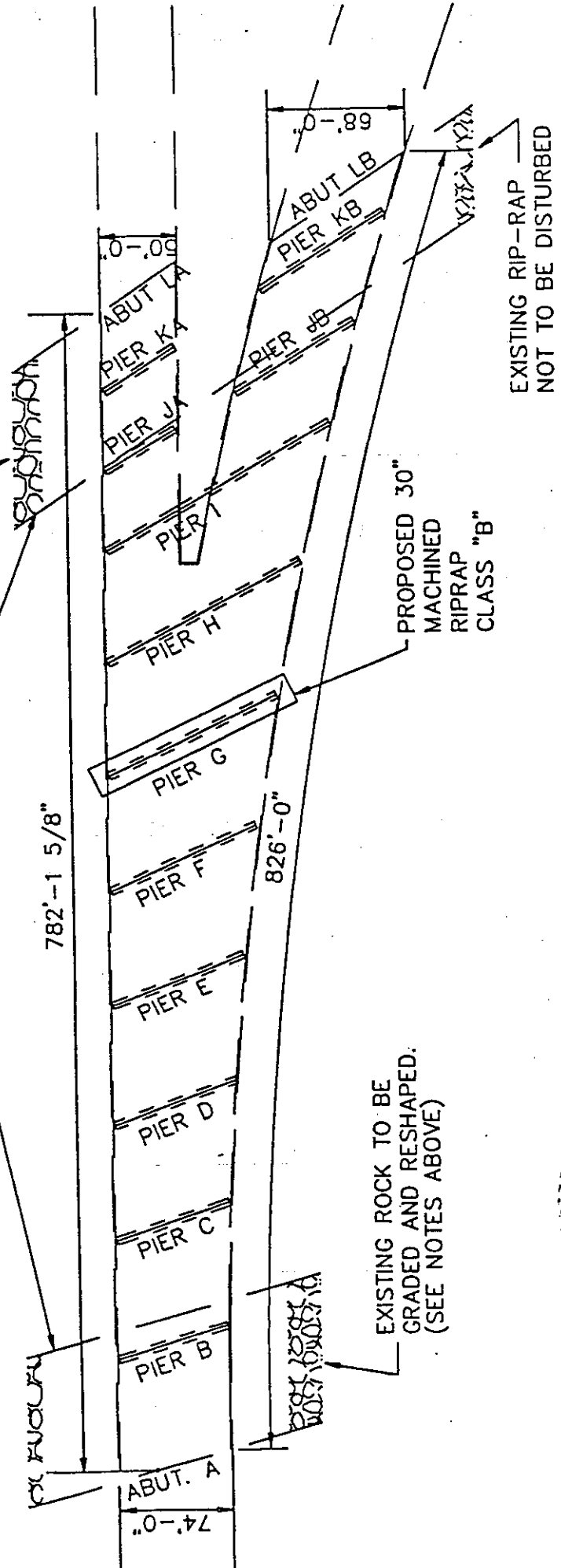


EXISTING ROCK TO BE STOCKPILED ON SITE TO BE PLACED IN EXCAVATION TO 30" BELOW NATURAL GROUND.
LIMITS OF REGRADE TO BE 25' FROM UPSTREAM AND DOWNSTREAM FACE OF BRIDGE, FROM FACE OF ABUTMENT TO PIER B AS SHOWN IN THE DETAILS FOR ABUTMENT REPAIR. THEN RECOMPACTED IN CONTROL LIFTS TO APPROXIMATELY 30" BELOW NATURAL GROUND. PLACE STOCKPILED ROCK. 30" MACHINED RIP-RAP CLASS "B" TO BE PLACED TO EXISTING GRADE.

FLOW

EXISTING RIP-RAP NOT TO BE DISTURBED

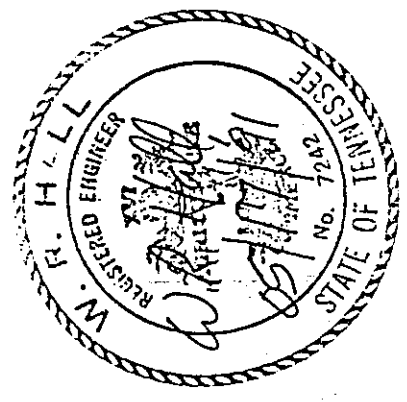
EXISTING TOE OF RIPRAP



EASTBOUND BRIDGE

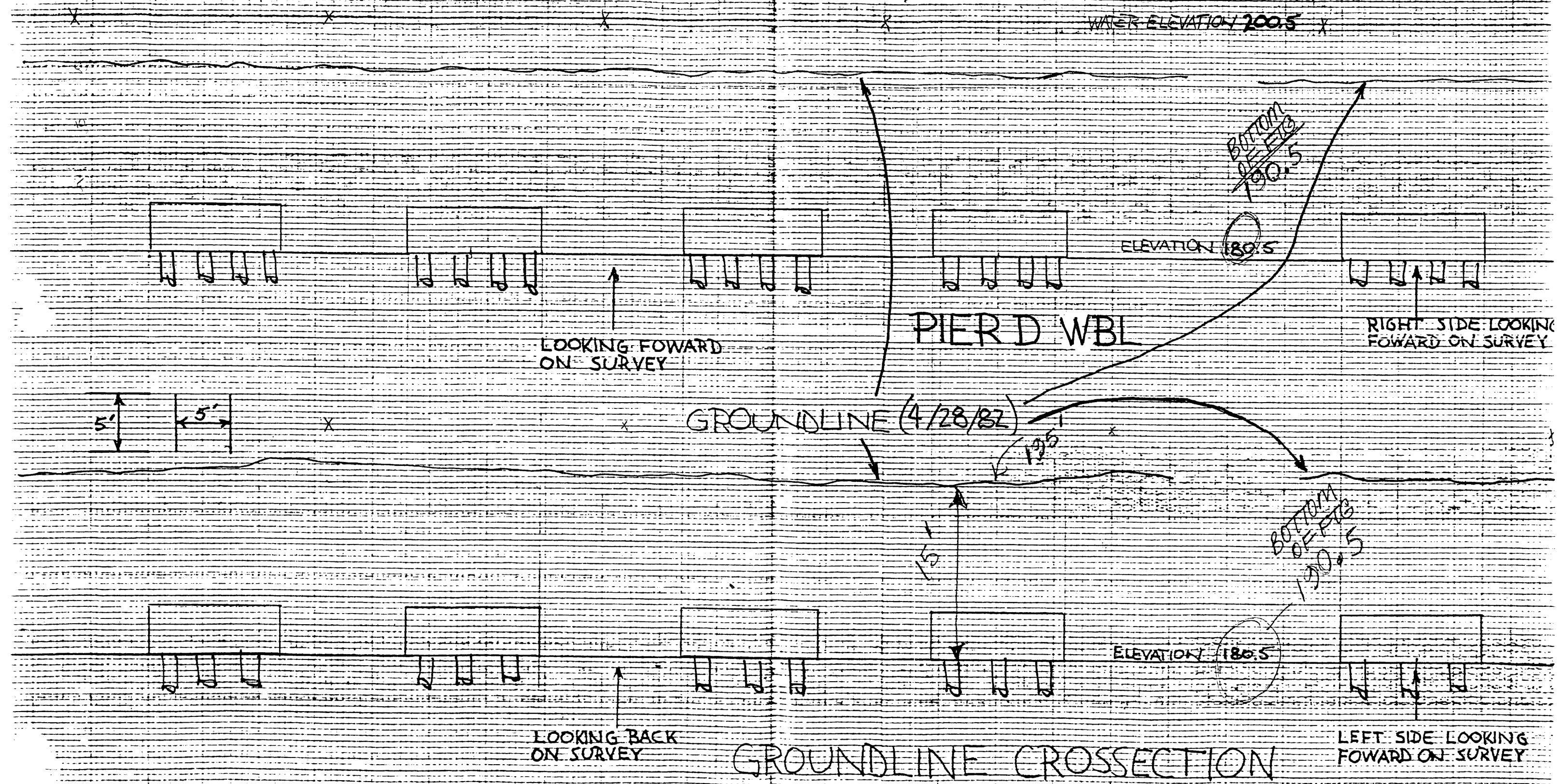
PLAN VIEW

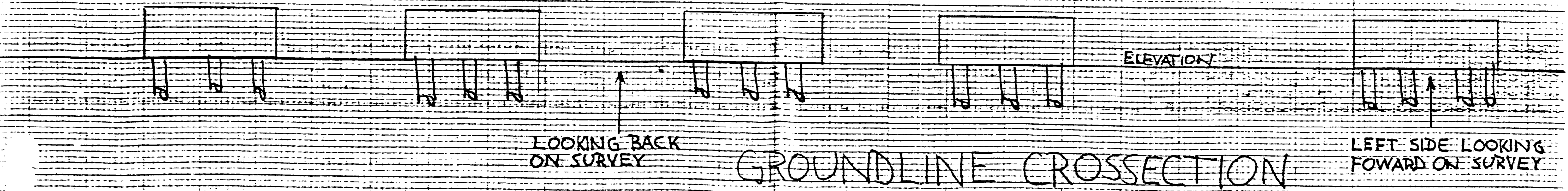
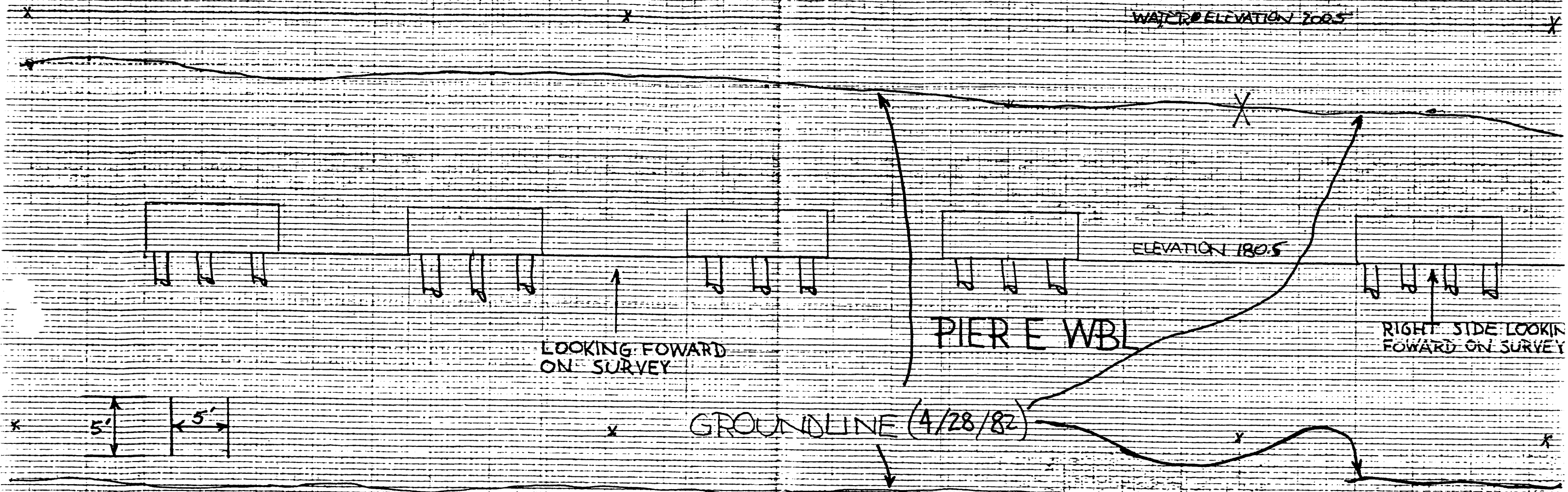
SCALE: 1" = 100'

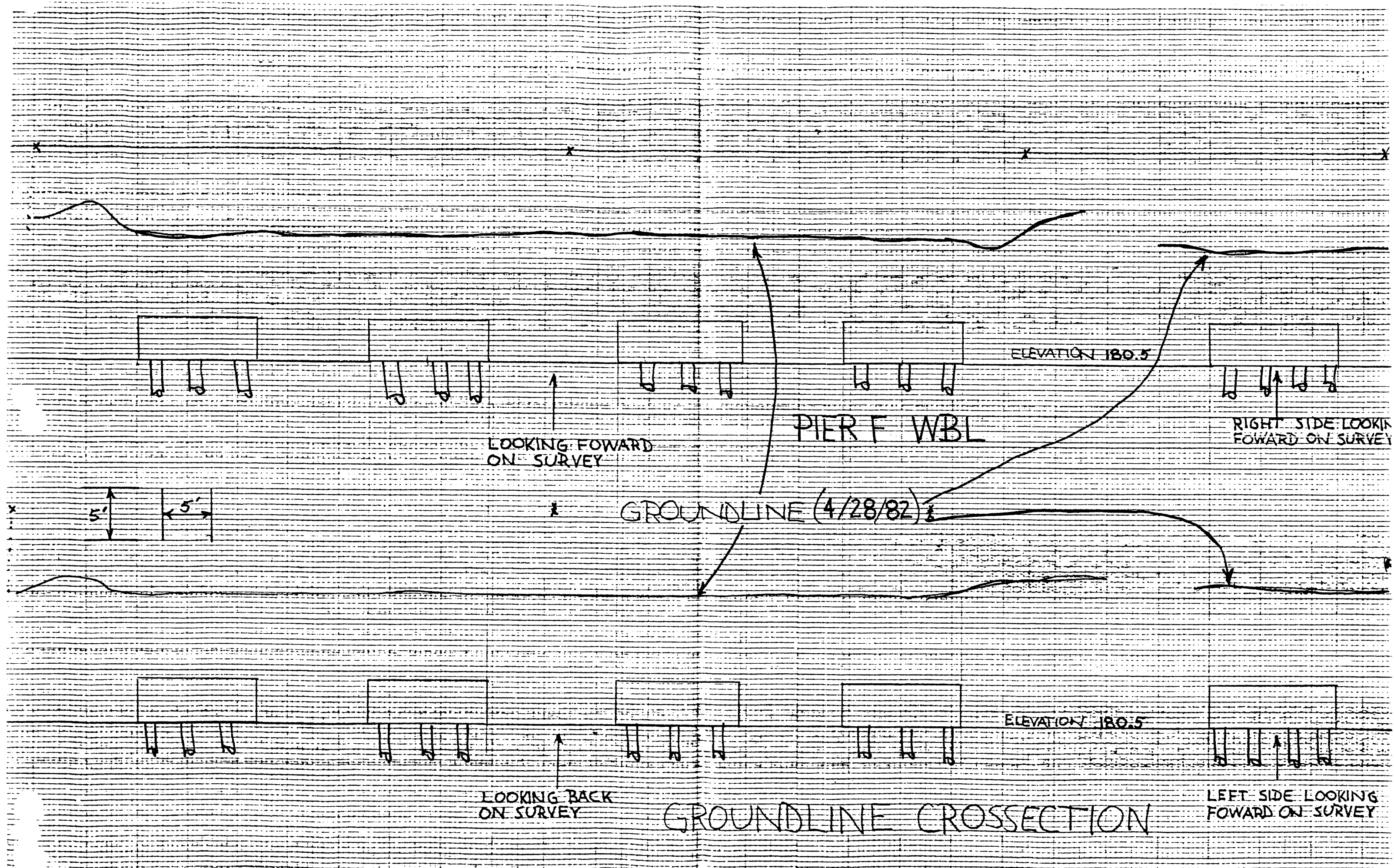


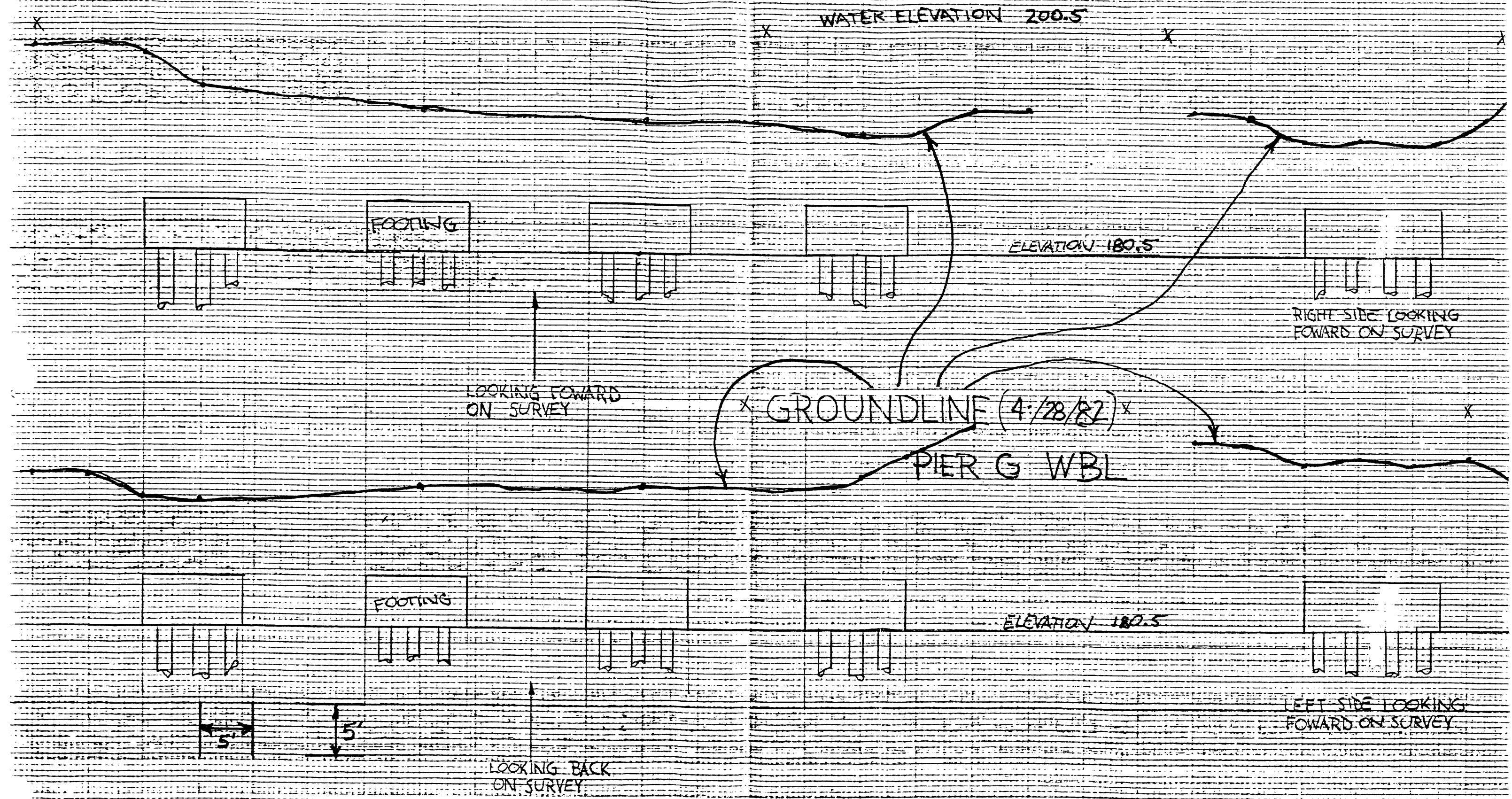
BRIDGE SCOUR REPAIR PLAN

I-40/240 OVER WOLF RIVER
BRIDGE NO. 79-140-5.09 RT.
SHELBY COUNTY
1991





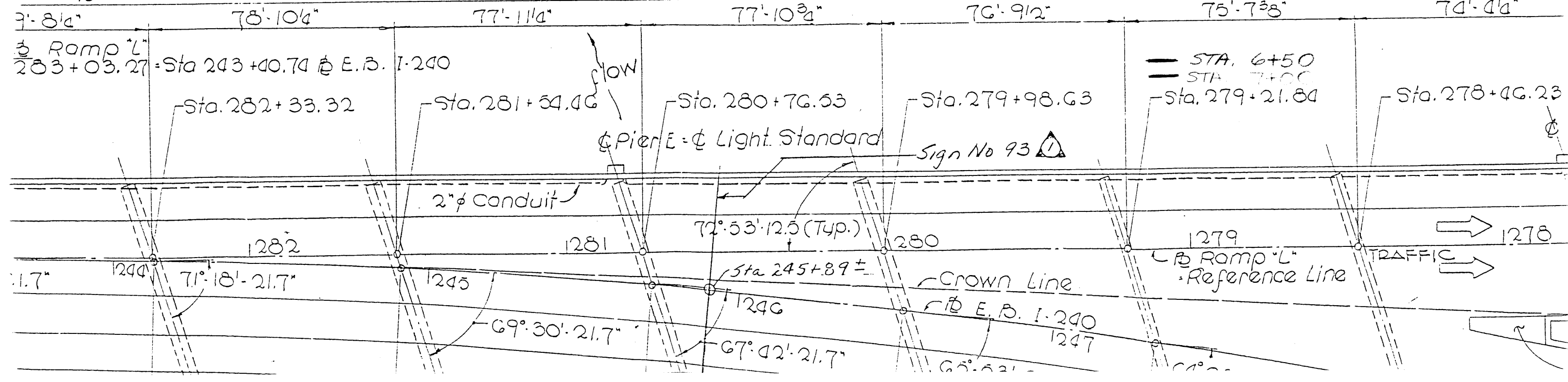
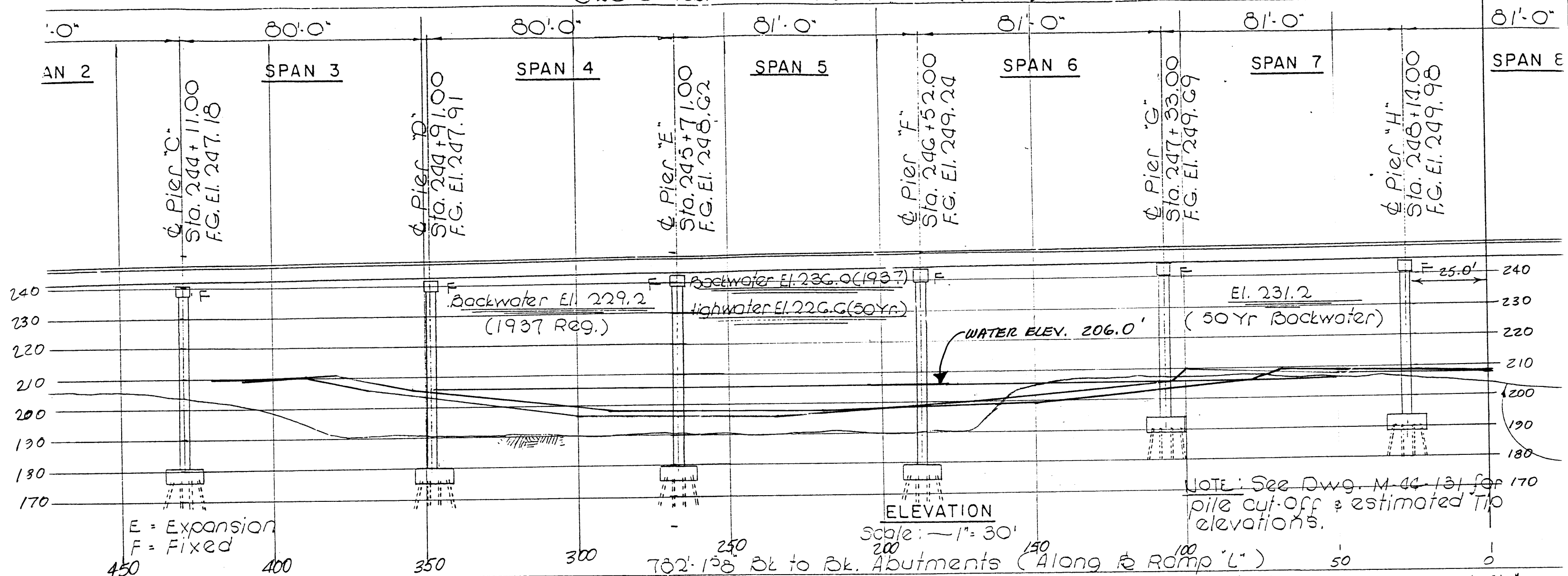


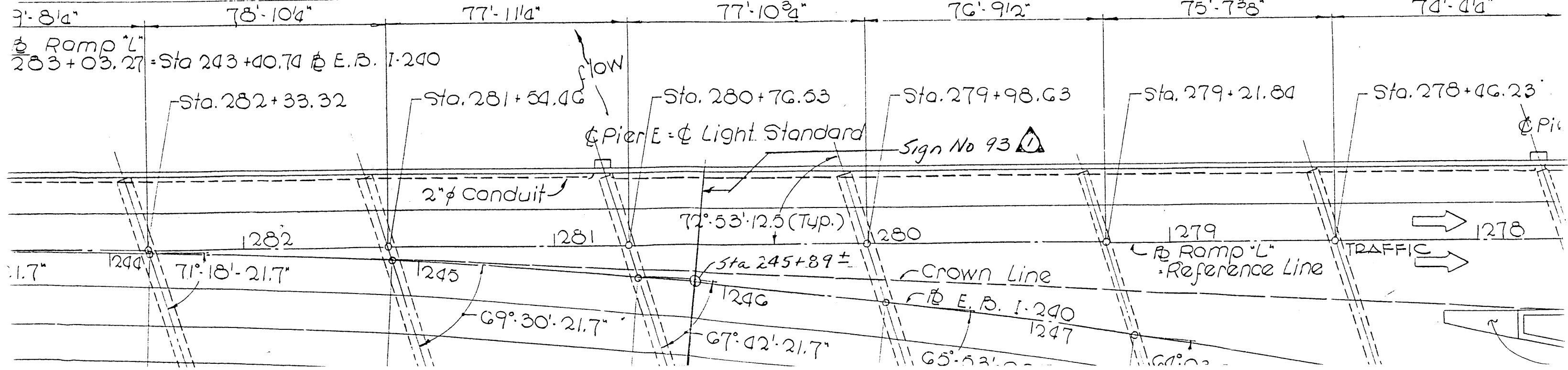
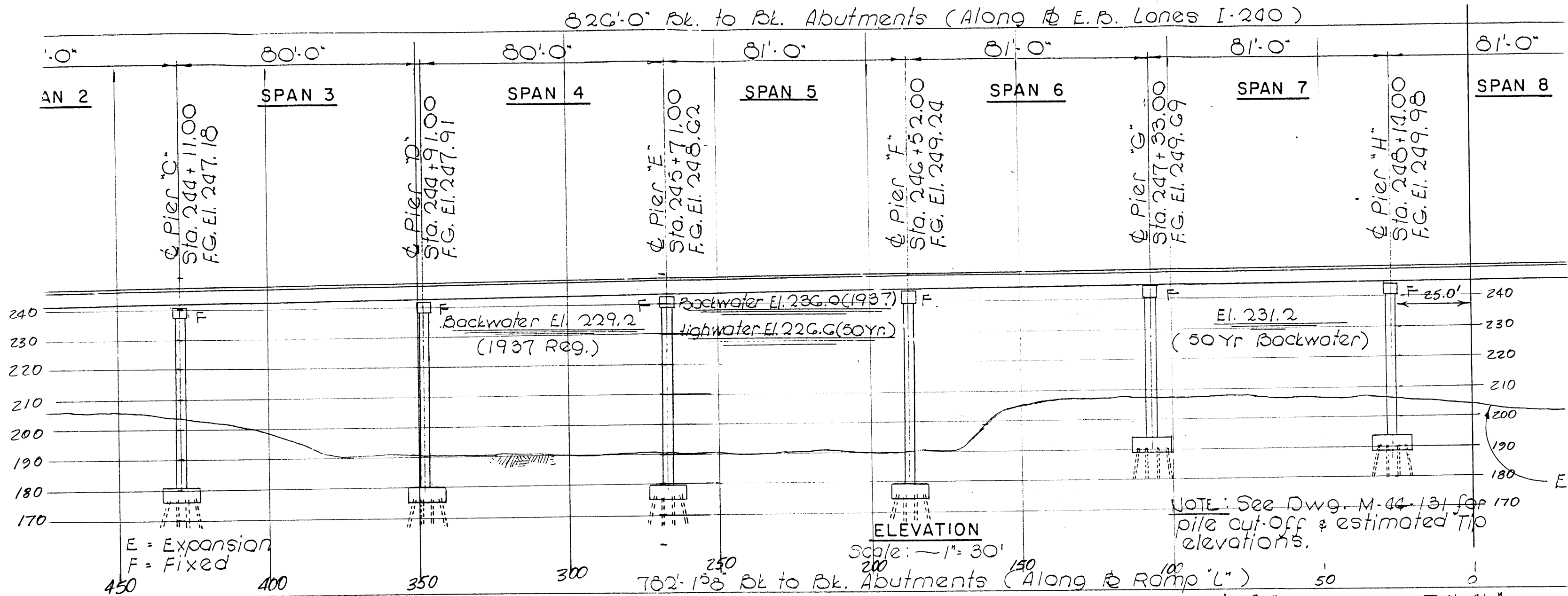


GROUNDLINE CROSSSECTION

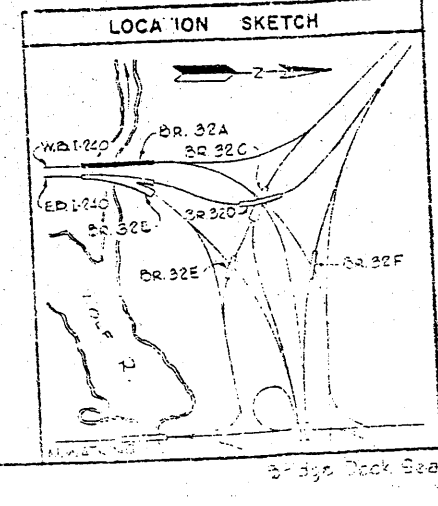
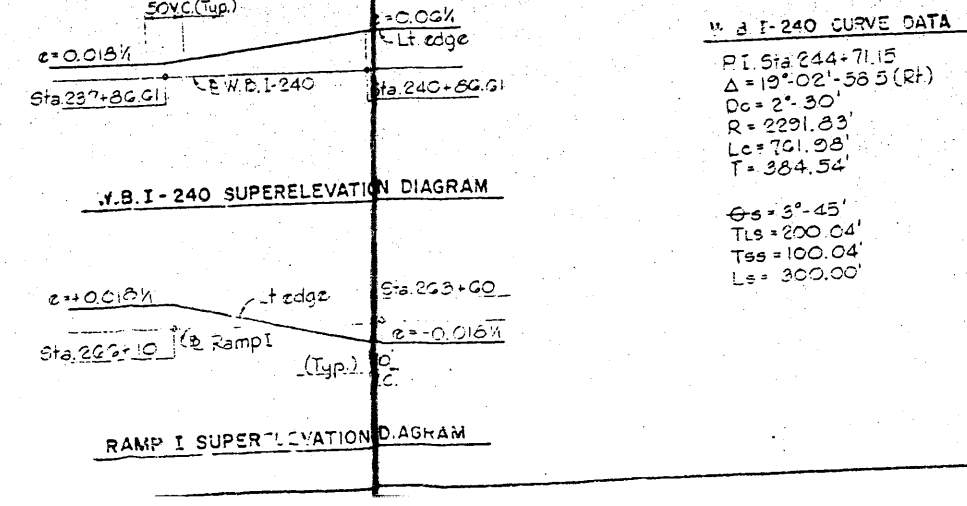
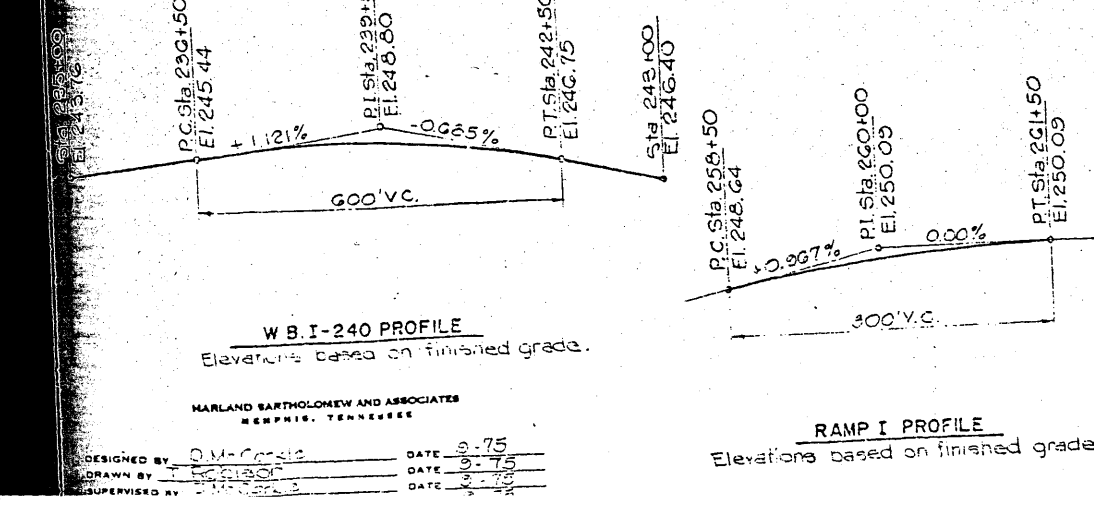
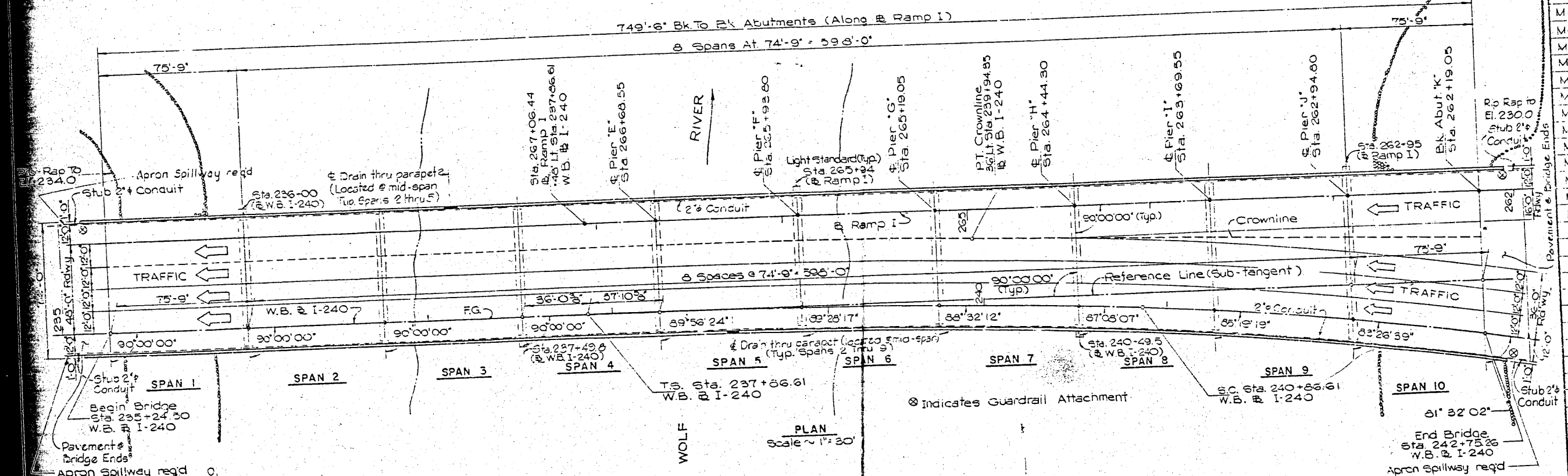
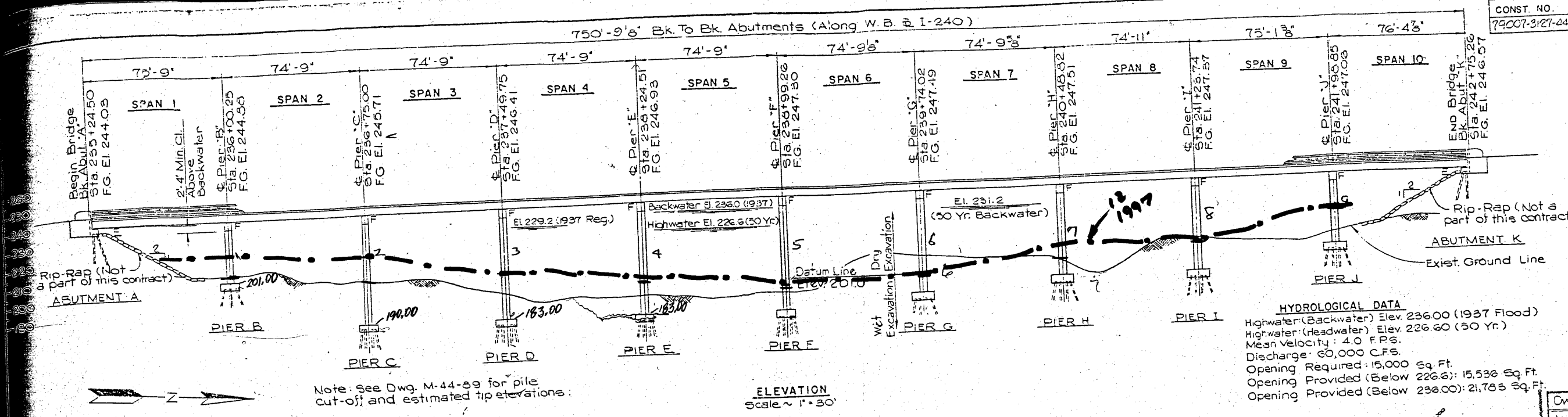
79-140-5.09 R.L.
E.B.L.

826'-0" Bl. to Bl. Abutments (Along E.B. Lanes I-240)





CONST. NO.	PROJECT NO.	YEAR	SHEET
79007-9127-44	EACI-240-11339	1975	3
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



Dwg. No.	Title
M-44-70	Bridge Layout.
M-44-71	General Notes & Estimated Qu.
M-44-72	Abutment A.
M-44-73	Abutment K.
M-44-74	Pier B.
M-44-75	Piers C, D, E & F.
M-44-76	Piers C, D, E & F - Details.
M-44-77	Pier G.
M-44-78	Pier H.
M-44-79	Pier I.
M-44-80	Pier J.
M-44-81	Superstructure - Typical Section
M-44-82	Slab Plan - Spans 1-6
M-44-83	Slab Plan - Spans 7-8
M-44-84	Slab Plan - Spans 9-10
M-44-85	Framing Plan - Spans 1-6
M-44-86	Framing Plan - Spans 7-10
M-44-87	Pre-stressed Beam & Bearing
M-44-88	Screed Elevations.
M-44-89	Foundation Data
M-44-90	Approach Slab.
M-25-1	Bridge Railing - Concrete
K-80-14	Reinf. Bar Support Data
K-80-144	Reinf. Concrete Pavement
H-5-111	Standard Pile Details.
K-85-150	Misc. Abutment & Drainage
K-80-130	Preformed Elastic Joint
M-8-149	Light Standard Support
P-8-9A	Standard Apron Spillway

79-I40-5.09.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS

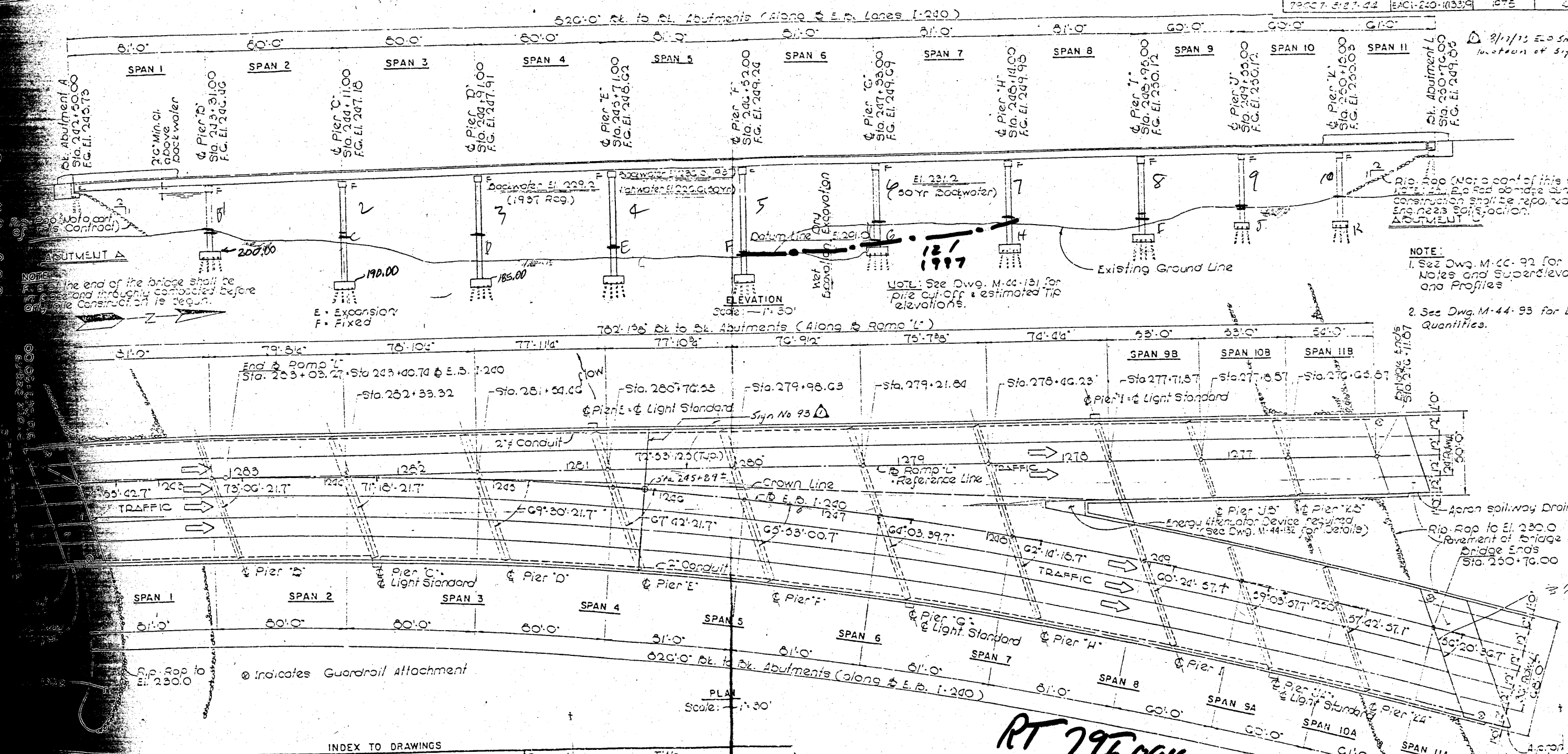
BRIDGE 31A
 W.D. I-240 OVER WOLF RIVER

BRIDGE LAYOUT
 STA 235+24.50

SHELBY COUNTY

CORRECT: [Signature]
 APPROVED: [Signature]

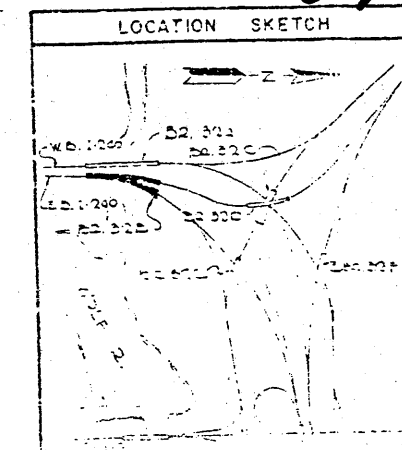
CONST. NO.	PROJECT NO.	YEAR	SHEET
72007-3-27-44	EACI-240-10339	1975	2



INDEX TO DRAWINGS			
Dwg. No.	Title	Dwg. No.	Title
M-44-104	Pier I	M-44-122	Framing Plan - Spans 1 & 2
M-44-105	Pier I	M-44-123	Framing Plan - Spans 3 & 4
M-44-106	Piers J & K	M-44-124	Framing Plan - Spans 5 & 6
M-44-107	Piers L & M	M-44-125	Framing Plan - Spans 7 & 8
M-44-108	Abutment LA	M-44-126	Framing Plan - Spans 9, 10A & 11A
M-44-109	Abutment LB	M-44-127	Framing Plan - Spans 10B & 11B
M-44-110	Typical Section - Spans 1 thru 11	M-44-128	Freshwater Exam Details
M-44-111	Typical Section - Spans 9B thru 11B	M-44-129	Bearing Details
M-44-112	Slab Plan - Spans 1 & 2	M-44-130	Screen Elevations
M-44-113	Slab Plan - Spans 3 & 4	M-44-131	Foundation Data
M-44-114	Slab Plan - Spans 5 & 6	M-44-132	Light Standard Support Details
M-44-115	Slab Plan - Spans 7 & 8	M-44-133	Storm Drain Details
M-44-116	Slab Plan - Spans 9 & 10A	M-44-134	Storm Drain Details
M-44-117	Slab Plan - Spans 10B & 11B	M-44-135	Storm Drain Details
M-44-118	Storm Drain Details	M-44-136	Storm Drain Details
M-44-119	Storm Drain Details	M-44-137	Storm Drain Details
M-44-120	Storm Drain Details	M-44-138	Storm Drain Details
M-44-121	Storm Drain Details	M-44-139	Storm Drain Details
M-44-122	Storm Drain Details	M-44-140	Storm Drain Details

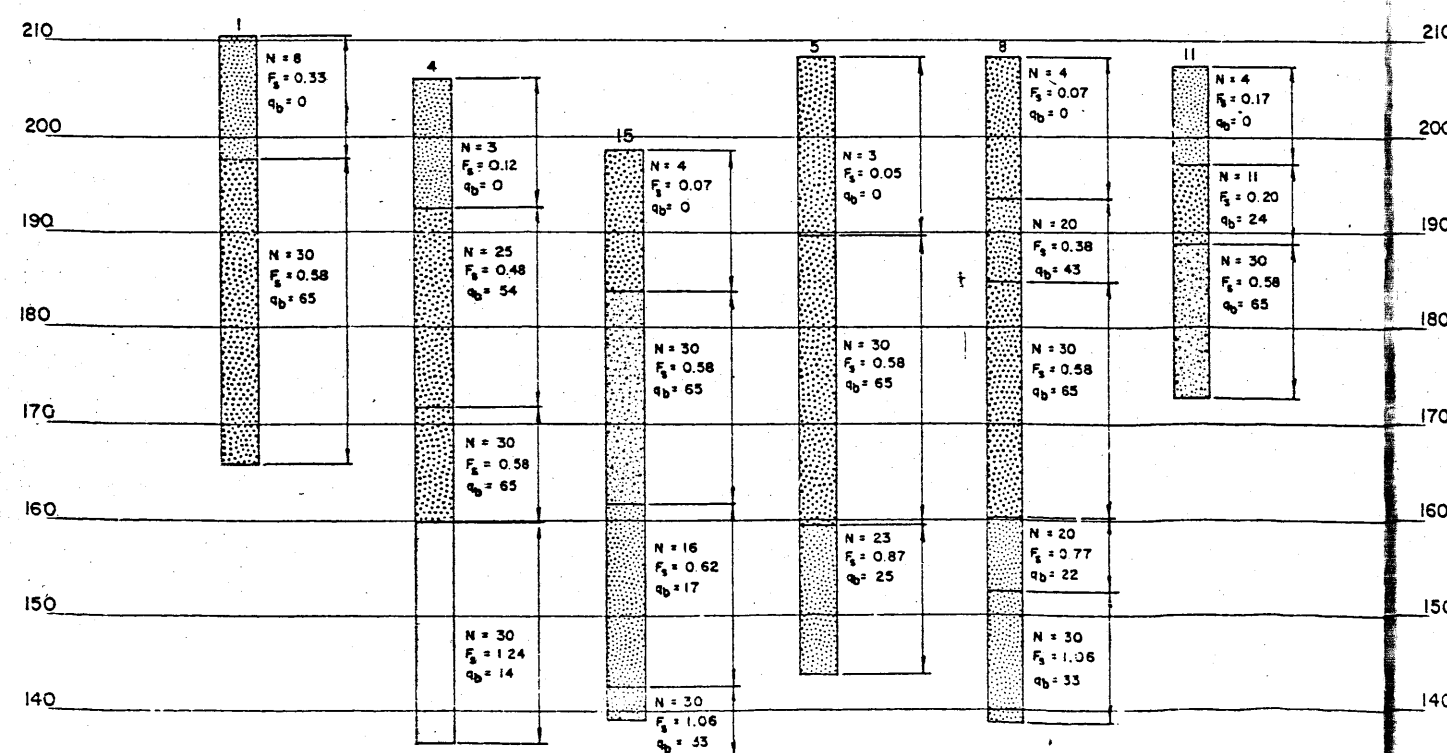
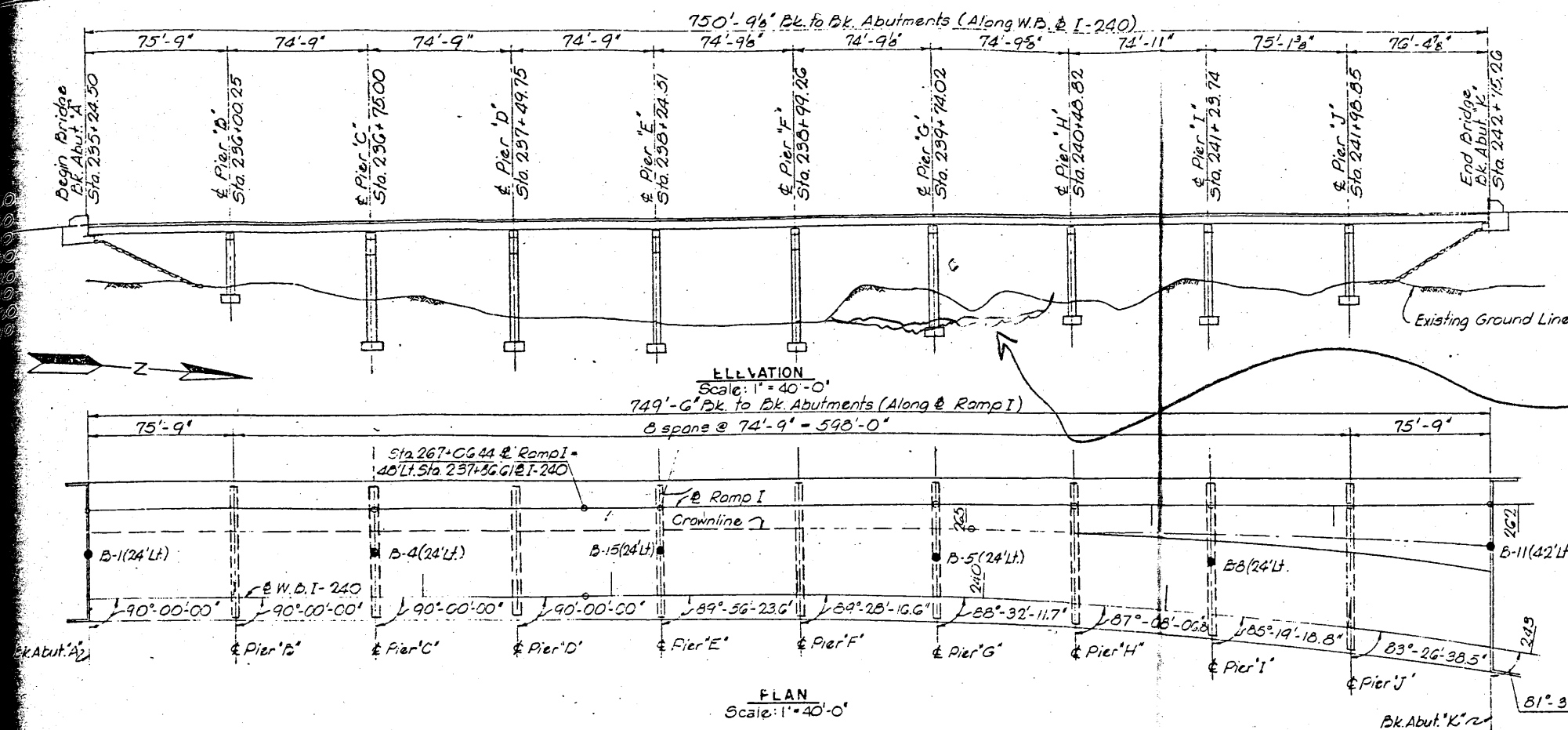
HYDROLOGICAL DATA
 Highwater (Backwater) Elev. 236.00 (1937 Flood)
 Highwater (Headwater) Elev. 226.00 (50 Yr.)
 Mean Velocity 4.0 F.P.S.
 Discharge 66,000 C.F.S. (50 Yr.)
 Opening Required 15,000 Sq. Ft.
 Opening Provided (Below 226.0) 16,500 Sq. Ft.
 Opening Provided (Below 236.0) 21,700 Sq. Ft.

E.B. I-240 CURVE DATA
 PI Sta. 240+92.57
 L=44' 00" 33' (21)
 Ch. 2' 15"
 R=243.00'
 L=243.00'
 T=102.90'
 E=3' 00"
 L=200.00'
 T=100.00'
 L=500.00'



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
 BRIDGE NO. 32 B
 E.B. I-240 OVER WOLF RIVER
 BRIDGE LAYOUT
 STA. 242+50.00
 SHELBY COUNTY
 79-I40-5.09 1A
 CORRECTED BY STRUCTURES
 APPROVED BY STRUCTURES

CONST. NO.	PROJECT NO.	YEAR	SHEET
79007-3127.44	EACI 240-1(133)9	1975	22



PILE DATA				
Location	Design Load ①	Cut-off Elev.	Est. Tip Elev.	Pile
Abut. A.	95 Tons	Varies	192.0	
Pier B.	100 Tons	200.0	130.0	
Pier C.	95 Tons	180.5	166.5	
Pier D.	95 Tons	181.5	162.5	
Pier E.	95 Tons	181.5	162.5	
Pier F.	95 Tons	181.5	166.5	
Pier G.	85 Tons	192.5	177.5	
Pier H.	85 Tons	192.5	177.5	
Pier I.	90 Tons	191.5	176.5	
Pier J.	100 Tons	193.5	173.5	
Abut. K.	95 Tons	Varies	183.0	

① Design loads based on factored loads.
② Pile lengths subject to change after reviewing results of load tests.

NOTE:
This drawing is not to be used as a layout.

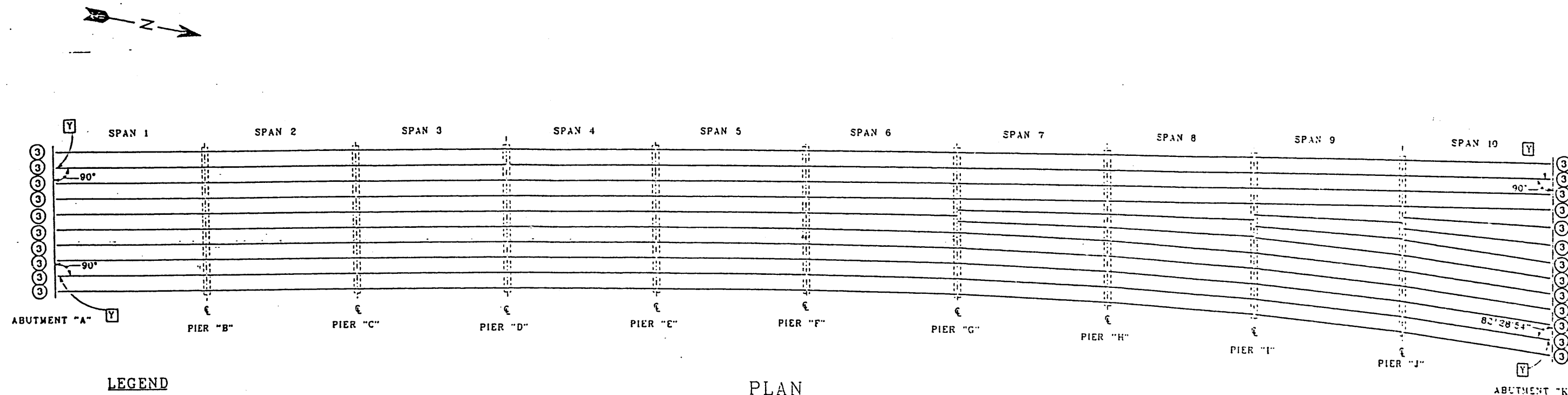
PROJECT I-240-1(83)4
FOUNDATION DATA
E.B. & W.B. I-240 OVER WOLF RIVER
BRIDGE NO. 32A
SHELBY COUNTY

LIST OF SPECIAL PROVISIONS			
** DENOTES CURRENT REVISION DATE, AS PER CONTRACT DOCUMENTS			
NO.	LAST REV. DATE	REGARDING	
105A	**	APPROVAL OF SHOP DRAWINGS	

LIST OF REFERENCE DRAWINGS	
(TO BE PRINTED WITH PLANS)	
DRAWING NO.	DRAWING
M-44-70, M-44-72 THRU 75, M-44-77 THRU 81 AND M-44-85 THRU 87.	EXISTING BRIDGE DRAWINGS

LIST OF DRAWINGS		
DRAWING NO.	LAST REV. DATE	DRAWING
BR-33-30	5-15-13	SEISMIC RESTRAINER LAYOUT
BR-33-29	5-15-13	ESTIMATED QUANTITIES AND GENERAL NOTES
BR-33-40		TYPICAL SECTIONS
BR-33-44		SEISMIC RESTRAINER DETAILS TYPE 3) (17)
BR-33-48		LATERAL RESTRAINER DETAILS TYPE Y
BR-33-49	5-8-96	BOLT INSTALLATION

PROJECT NO.	YEAR	SHEET NO.
79957-4152-04	1993	
REVISIONS		
NO.	DATE	BY
1	5-15-13	SAE
2	5-15-13	SAE
3	5-15-13	SAE



LEGEND

Y DENOTES LATERAL RESTRAINT TYPE Y SEE BR-33-48 FOR DETAILS.
 3 DENOTES SEISMIC RESTRAINT TYPE 3 SEE BR-33-44 FOR DETAILS.
 SEE BR-33-40 FOR TYPICAL CROSS-SECTIONS.

PLAN
 (WEST BOUND LANE)
 DIRECTION OF SURVEY

GENERAL SCOPE OF WORK

- 1) PROVIDE WIRE ROPE SEISMIC RESTRAINTS AT ABUTMENTS (TYPE 3). REFER TO LEGEND AND PLAN VIEW FOR DESCRIPTION AND LOCATION, THIS SHEET.
- 2) PROVIDE LATERAL SEISMIC RESTRAINTS AT ABUTMENTS REFER TO LEGEND AND PLAN VIEW FOR DESCRIPTION AND LOCATION, THIS SHEET.

DESIGNED BY BRIAN ELLIOTT DATE NOVEMBER 1991
 DRAWN BY JIM KIMMEL DATE NOVEMBER 1991
 SUPERVISOR BY M. LAWRENCE T. LUTHELIANSON DATE NOVEMBER 1991
 CHECKED BY RICK LARSON & BRIAN ELLIOTT DATE FEBRUARY 1994

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 SEISMIC RESTRAINER LAYOUT
 INTERSTATE 40 OVER WOLF RIVER
 BRIDGE NO. 79-140-5.09
 (WEST BOUND LANE)
 SHELBY COUNTY
 1993

BR-33-30

ESTIMATED QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	79-140-5.09 L&R OVER WOLF RIVER	79-2819-4.93 OVER 140-6.50	79-140-7.60 L&R OVER FAU 2821	79-140-8.25 L&R OVER I.C.G. RAILROAD	79-4186-2.11 OVER 140-9.36	79-140-9.50 L&R OVER WOLF RIVER	TOTAL QUANTITIES
602-10.39	STRUCTURAL STEEL BRIDGE (REPAIRS)	EACH				24			24
604-03.60	BRIDGE JOINT SEISMIC MODIFICATION	EACH	48	36	36	28	40	30	218
604-10.42	CONCRETE REPAIRS	C.F.	36						36
712-01	TRAFFIC CONTROL	L.S.	0.17	0.16	0.16	0.17	0.17	0.17	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	500	440		620	460	400	2420
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH			10	18			28
712-05.01	WARNING LIGHTS (TYPE A)	EACH		6	2	4	6		18
712-06	SIGNS (CONSTRUCTION)	S.F.		116	44	120	116		396
712-05.03	WARNING LIGHTS (TYPE C)	EACH				8			8
712-06.10	NEW SIGNS (CONSTRUCTION)	S.F.	232					232	464
712-06.16	SIGNS (CONSTRUCTION) (REDUCE SPEED WARNING)	EACH	4	2			2	4	12
717-01	MOBILIZATION	L.S.	0.17	0.16	0.16	0.17	0.17	0.17	1
712-08.03	ARROW BOARDS (TYPE C)	EACH			1	2			3

- 1 INCLUDES COST OF ALL LABOR AND MATERIALS NECESSARY TO PROVIDE STRUCTURAL STEEL LATERAL SEISMIC RESTRAINTS.
- 2 INCLUDES COST OF ALL LABOR AND MATERIALS NECESSARY TO PROVIDE WIRE ROPE SEISMIC RESTRAINTS.
- 3 INCLUDES COST OF ALL LABOR AND MATERIALS NECESSARY TO PROVIDE CONCRETE LATERAL SEISMIC RESTRAINTS.
- 4 TO BE USED ON FLEXIBLE DRUMS THROUGH TAPERS.
- 5 INCLUDES THE INSTALLATION AND MAINTENANCE OF A NEW SIGN PANEL, SHEETING AND SUPPORTS.
- 6 ITEM TO BE USED ONLY WHEN CONTRACTOR ESTABLISHES A REDUCED SPEED LIMIT WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS AND TWO (2) TYPE "B" FLASHING LIGHTS AS PER THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER PRESENT IN THE CONSTRUCTION WORK ZONE.

UTILITY NOTES

THE LOCATION OF UTILITIES SHALL BE FIELD LOCATED BY THE CONTRACTOR, AND BY CONTACTING THE UTILITY COMPANIES INVOLVED. SOME UTILITIES CAN BE LOCATED BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC. AT 1-800-351-1111.

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO CO-OPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT.

THE CONTRACTOR SHALL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR CONTACTING ALL AFFECTED UTILITIES PRIOR TO SUBMITTING HIS BID. IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF THE WORK FOR THE PROJECT, SOME UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. WHILE SOME WORK MAY BE REQUIRED "AROUND" UTILITY FACILITIES THAT WILL REMAIN IN PLACE, IT IS UNDERSTOOD AND AGREED THAT THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR ANY DELAYS OR INCONVENIENCE CAUSED BY THE UTILITY ADJUSTMENTS.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY.

CONST. WORK ZONE TRAFFIC CONTROL

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06. SIGNS (CONSTRUCTION) S.F. AND 712-06.10. NEW SIGNS (CONSTRUCTION) S.F.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS FLAGGER SIGNS MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE ENGINEER SHALL APPROVE ALTERNATE LOCATIONS.

THE CONTRACTOR WILL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LAKE IS OPEN TO TRAFFIC, UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE ENGINEER SHALL APPROVE ALTERNATE LOCATIONS.

GALVANIZING OF NEW STEEL

ALL NEW STEEL SHALL BE GALVANIZED TO ASTM A123 STANDARDS.

NOTE: ROADSIDE BANKS/SLOPES USED BY THE CONTRACTOR FOR WORK ACCESS, PARKING, AND ANY OTHER OPERATIONS THAT ARE DISTURBED BY HIS OPERATIONS SHALL BE REPAIRED BY REGRADING, RESEEDING, MULCHING OR WHATEVER MEANS ARE NECESSARY TO RESTORE THE BANKS/SLOPES TO THE ORIGINAL CONDITION. ALL RESTORATION WORK SHALL MEET THE FULL SATISFACTION OF THE ENGINEER. COST OF ALL RESTORATION WORK SHALL BE INCLUDED IN ITEMS BID ON.

DESIGNED BY: BRIAN EGLI DATE: 01/13/99
DRAWN BY: SCOTT NELSON DATE: 01/13/99
SUPERVISED BY: M. LARSON & T. CHRISTIANSON DATE: 01/13/99
CHECKED BY: M. LARSON & B. EGLI DATE: 02/13/99

PROJECT NO.	YEAR	SHEET NO.
7995J-4152-J4	1998	2
REVISIONS		
NO.	DATE	DESCRIPTION
1	3-2-99	REVISED QUANTITIES AND ADDED NOTE
2	5-8-99	ADDED GENERAL NOTE
3	5-9-99	ADDED GENERAL NOTE

GENERAL NOTES

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION, (MARCH 1, 1995 EDITION)

DESIGN SPECIFICATIONS: AASHTO 1992 EDITION WITH ADDENDA.

STRUCTURAL STEEL: SHALL CONFORM TO AASHTO M270 GRADE 36 (ASTM A709 GRADE 36) UNLESS OTHERWISE NOTED.

REINFORCING STEEL: SEE THE STANDARD SPECIFICATIONS.

GROUTED BARS IN DRILLED HOLES: HORIZONTALLY DRILLED HOLES SHALL BE DRILLED 1/2" IN DIAMETER LARGER THAN THE BAR. CLEANED, PACKED WITH NON-SHRINK GROUT AND BAR DRIVEN TO ITS SEAT. VERTICALLY DRILLED HOLES SHALL BE DRILLED 1/4" IN DIAMETER LARGER THAN THE BAR. CLEANED, PACKED WITH EPOXY GROUT AND BAR DRIVEN TO ITS SEAT. ALL GROUTING MATERIAL SHALL BE APPROVED BY T.O.O.T. MATERIALS AND TESTS.

SHOP DRAWINGS: SHALL BE SUBMITTED ACCORDING TO SPECIAL PROVISION NO. 105A. EXCEPT SHOP DRAWINGS SHALL BE SUBMITTED TO THE HEADQUARTERS BRIDGE INSPECTION AND REPAIR OFFICE IN LIEU OF THE DIVISION OF STRUCTURES.

BOLTS: SHALL BE HIGH TENSILE STRENGTH BOLTS (ASTM-A325), UNLESS OTHERWISE NOTED. SIZE TO BE AS NOTED ON PLANS. SEE AASHTO SPECIFICATIONS, ARTICLE 11.5.6 DIVISION II. EXISTING CONTRACT SURFACES SHALL BE CLEANED TO SSPC-10 SPECIFICATIONS PRIOR TO ATTACHMENT OF NEW MEMBERS.

CONCRETE: TO BE CLASS "A" CONCRETE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

WELDING: ANSI/AASHTO/AAS D1.5-98 BRIDGE WELDING CODE AND THE STANDARD SPECIFICATIONS.

SPECIAL NOTE TO CONTRACTOR: CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO INSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVING AND DISPOSING OF DEBRIS SHALL BE INCLUDED IN ITEMS BID ON.

NOTE: ALL STRUCTURAL STEEL FOR SEISMIC RESTRAINER AND LATERAL RESTRAINTS, EXCEPT FOR NON-CORROSIVE WIRE ROPE AND TIMBLES, SHALL BE FABRICATED BY AISC, SIMPLE SPAN BRIDGES CATEGORY, CERTIFIED SHOP.

SPECIAL NOTE CONCERNING DRILLED ANCHORS: AT ALL LOCATIONS WHERE A DRILLED MAXI-BOLT OR ANCHOR IS SPECIFIED, A WILLIAMS UNDERCUTTING ANCHOR SHALL BE ACCEPTABLE AS WELL AS THE DRILLED MAXI-BOLT.

WIRE ROPE: WIRE ROPE SHALL BE AS SPECIFIED IN AASHTO DESIGNATION M277-S1(1990).

WIRE ROPE CLIPS: EACH CONNECTION SHALL HAVE A MINIMUM OF FOUR (4) WIRE ROPE CLIPS AND CUMULATIVELY DEVELOP 125% OF THE YIELD STRESS OF THE WIRE ROPE. THE YIELD STRESS SHALL BE VERIFIED BY TENNESSEE DEPARTMENT OF TRANSPORTATION MATERIALS AND TEST.

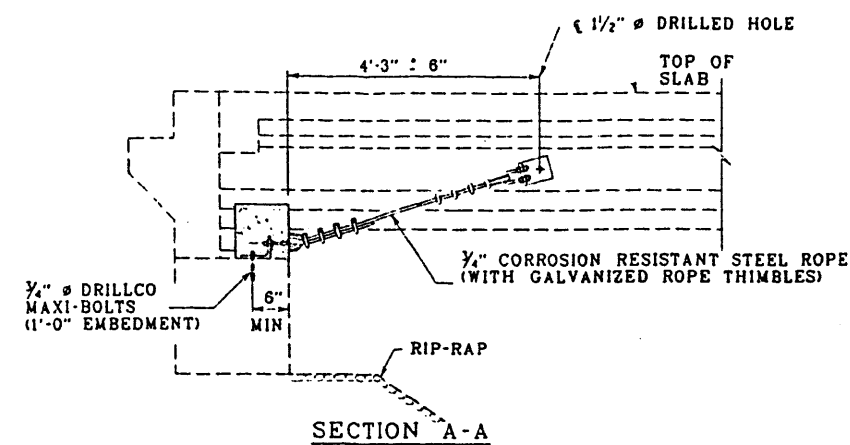
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ESTIMATED QUANTITIES
AND

GENERAL NOTES

BRIDGE NO. 79-140-5.09 (L&R)
BRIDGE NO. 79-2819-4.93
BRIDGE NO. 79-140-7.60 (L&R)
BRIDGE NO. 79-140-8.25 (L&R)
BRIDGE NO. 79-4186-2.11
BRIDGE NO. 79-140-9.50 (L&R)
SHELBY COUNTY

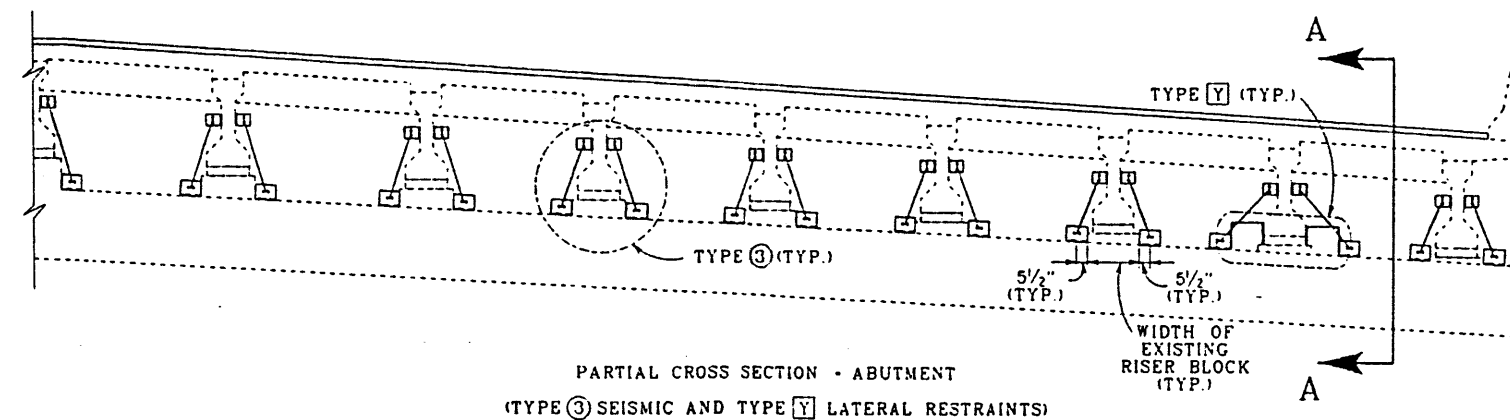
1998

SR-33-29



TYPE ③ SEISMIC RESTRAINER
TYPE Y LATERAL RESTRAINER
@ ABUTMENTS 'A', 'LA' AND 'LB'

(TYPE ③ - 23 REQ'D. WEST BOUND LANE)
 (TYPE Y - 4 REQ'D. WEST BOUND LANE)



TYPICAL CROSS SECTION

NOTE DETAILS FOR TYPE Y SHOWN ON BR-33-48.
 DETAILS FOR TYPE ③ SHOWN ON BR-33-44.

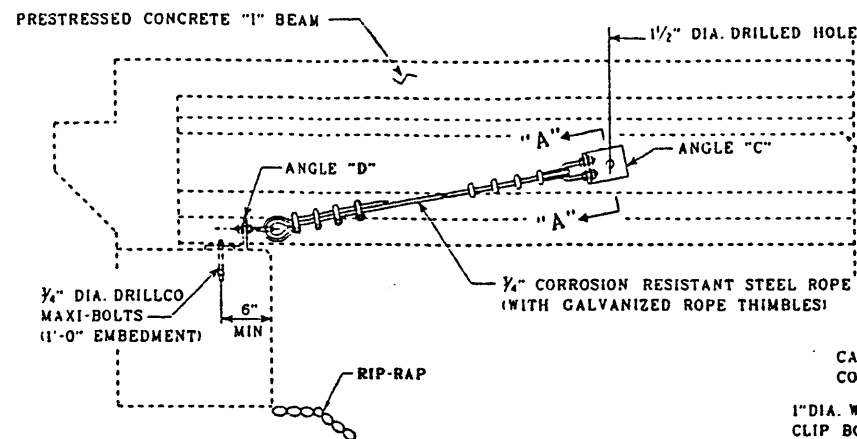
DESIGNED BY BRIAN EGGL DATE JANUARY 1998
 DRAWN BY DON KIMMER DATE JANUARY 1998
 SUPERVISED BY M. LARSON & T. CHRISTIANSON DATE JANUARY 1998
 CHECKED BY MIKKY LARSON & BRIAN EGGL DATE FEBRUARY 1998

PROJECT NO.	YEAR	SHEET NO.
70959-4152-04	1998	
REVISIONS		
NO.	DATE	BY



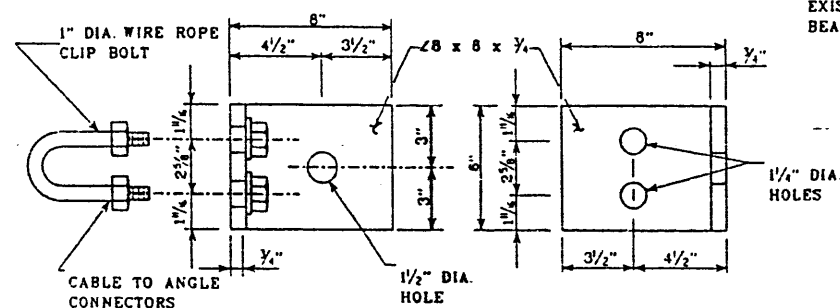
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 INTERSTATE 40 OVER WOLF RIVER
 BRIDGE NO. 79-140-5.09
 (WEST BOUND LANE)
 SHELBY COUNTY
 1998

BR-33-40

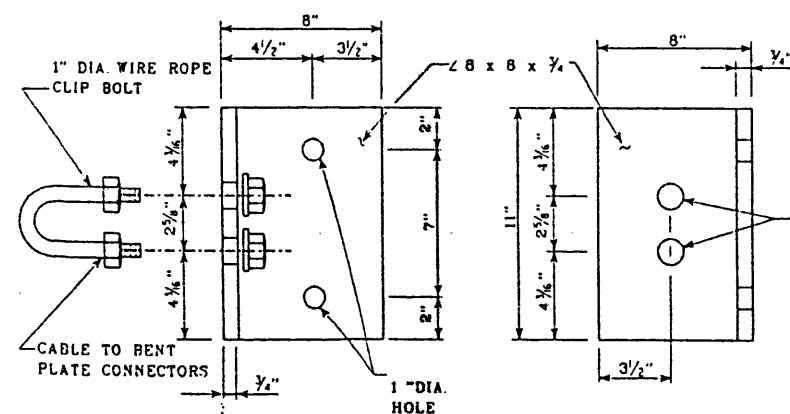


ELEVATION

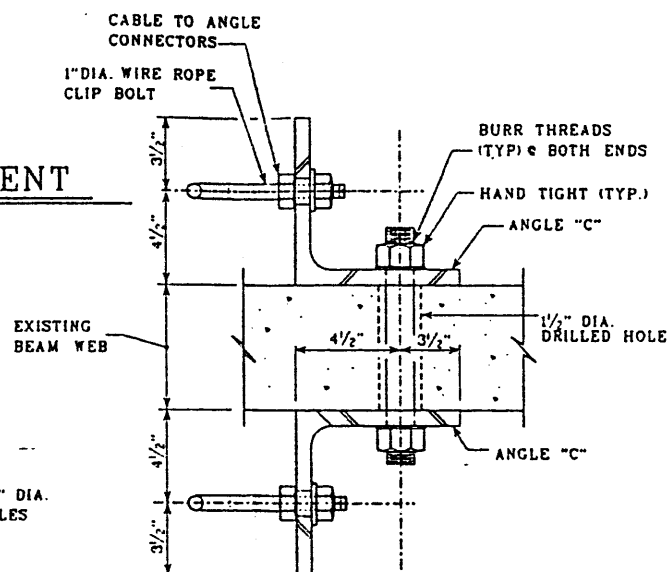
SEISMIC RESTRAINER @ ABUTMENT



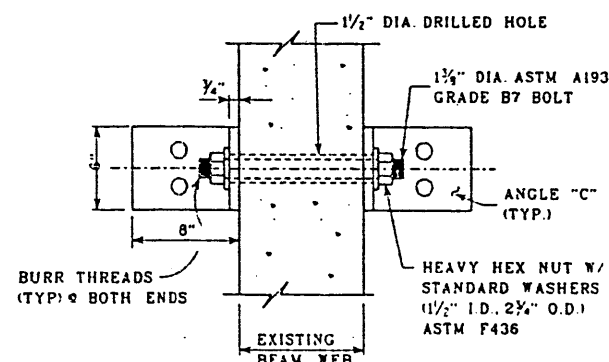
ANGLE "C" DETAILS
(2 REQ'D PER ASSEMBLY)



ANGLE "D" DETAILS
(2 REQ'D PER ASSEMBLY)



PLAN VIEW @ FITTING
(ANGLE "C")



SECTION A-A
(ANGLE "C")

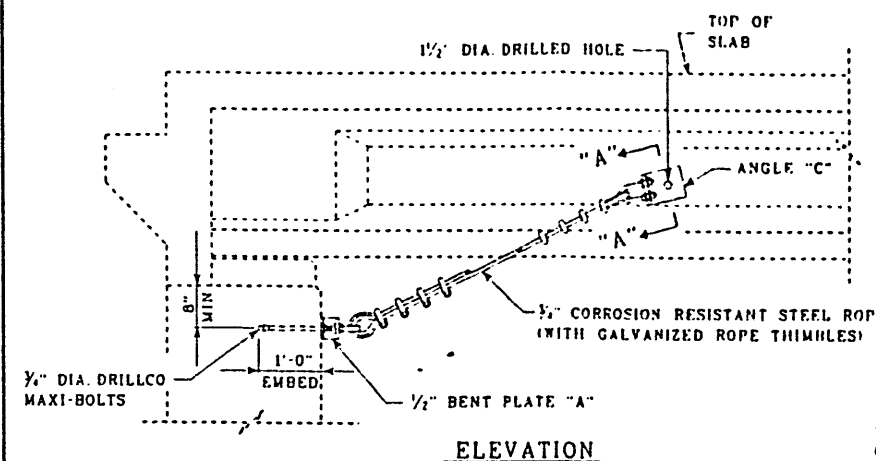
NOTES:

A PAIR OF STEEL ROPES AND ACCOMPANYING PLATES, BOLTS, NUTS, AND WASHERS IS CONSIDERED ONE ASSEMBLY.

SEE DWG. NO. BR-33-29 FOR GENERAL NOTES AND DWG. NO. BR-33-49 FOR PROCEDURE FOR INSTALLATION OF ANCHOR BOLTS.

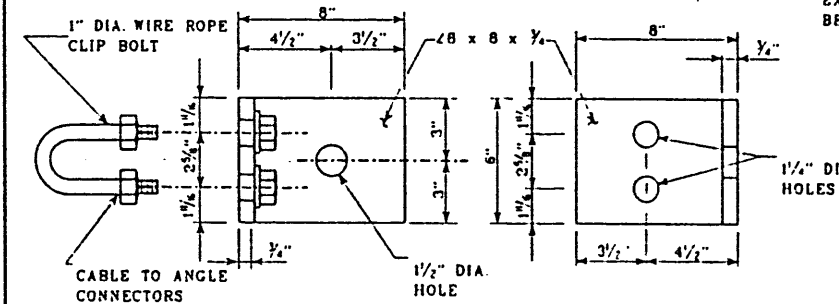
SEISMIC RESTRAINER TYPE ③

DESIGNED BY: Brian Egle DATE: September 1997
 DRAWN BY: Cory Hefner DATE: December 1997
 SUPERVISED BY: Mike Johnson / J. Christensen DATE: December 1997
 CHECKED BY: Mike Johnson / Brian Egle DATE: January 1998

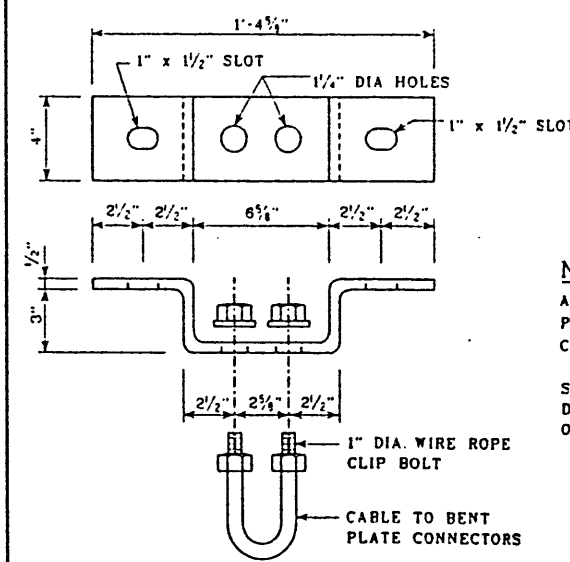


ELEVATION

SEISMIC RESTRAINER @ ABUTMENT



ANGLE "C" DETAILS
(2 REQ'D PER ASSEMBLY)



BENT PLATE "A"
(2 REQ'D PER ASSEMBLY)

NOTES:

A PAIR OF STEEL ROPES AND ACCOMPANYING PLATES, BOLTS, NUTS, AND WASHERS IS CONSIDERED ONE ASSEMBLY.

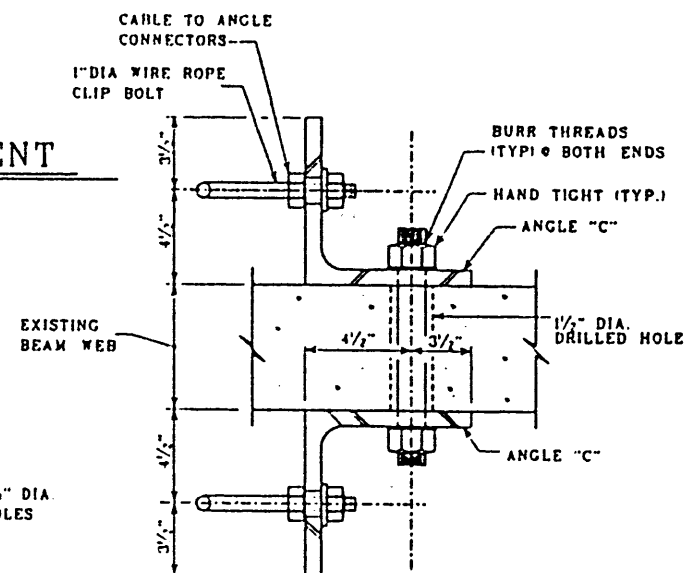
SEE DWG. NO. BR-33-29 FOR GENERAL NOTES AND DWG. NO. BR-33-49 FOR PROCEDURE FOR INSTALLATION OF ANCHOR BOLTS.

SEISMIC RESTRAINER TYPE ①⑦

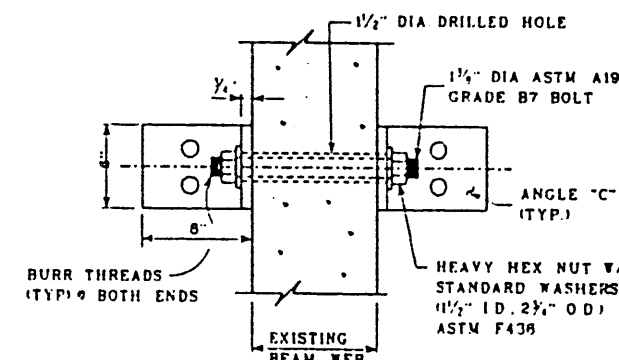
PROJECT NO.	YEAR	SHEET NO.
77359-4152-04	1998	

REVISIONS

NO.	DATE	BY	DESCRIPTION



PLAN VIEW @ FITTING
(ANGLE "C")



SECTION A-A
(ANGLE "C")



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SEISMIC RESTRAINER DETAILS
TYPE ③ AND ①⑦
SHELBY COUNTY
1998

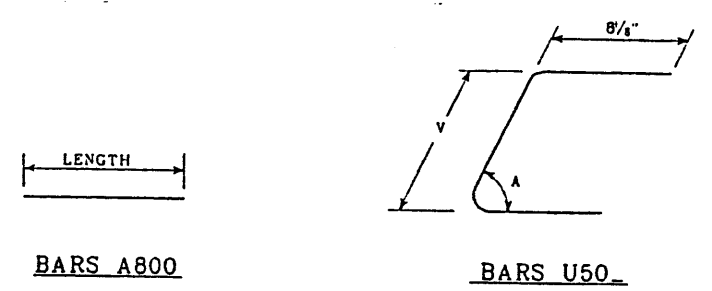
BR-33-44

NOTE: COST OF DRILLING, GROUTING, BITUMINOUS FILLER, FORMING, REINFORCING STEEL, CONCRETE, LABOR, AND ANY MISCELLANEOUS MATERIALS TO CONSTRUCT THE LATERAL SEISMIC RESTRAINTS AS SHOWN IN DETAILS THIS SHEET SHALL BE INCLUDED IN ITEM NUMBER 604-10.42, CONCRETE REPAIRS, C.F.

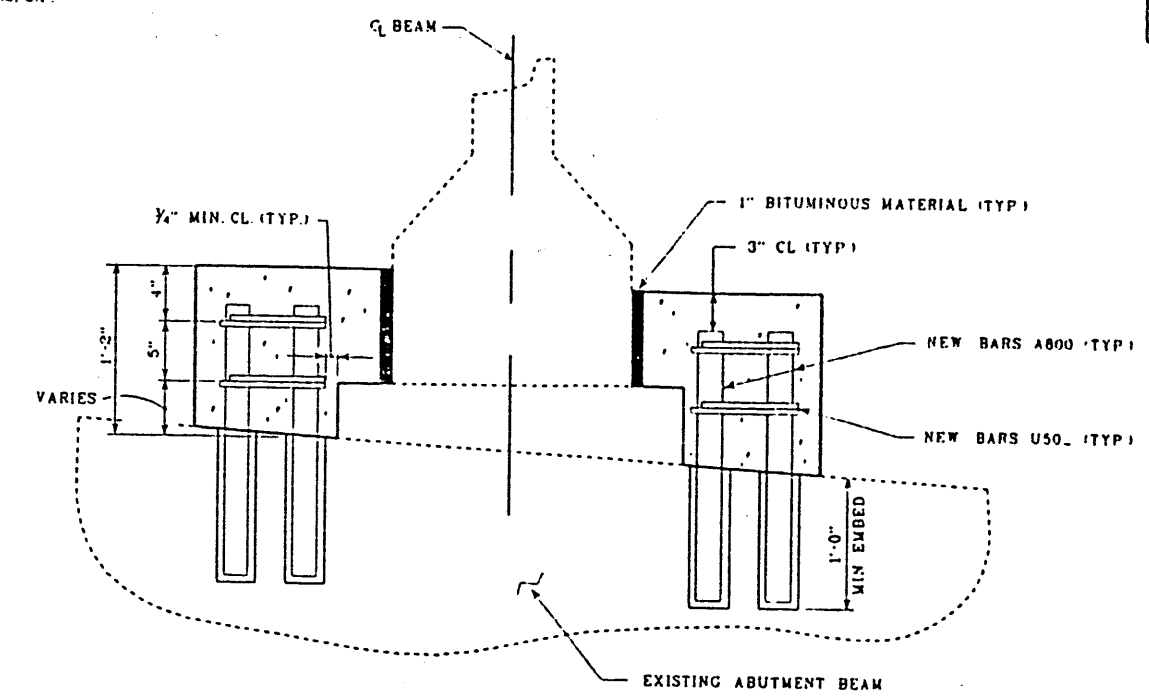
PROJECT NO.	YEAR	SHEET NO.	
79259-4152-04	1998		
REVISIONS			
NO	DATE	BY	DESCRIPTION

LATERAL BLOCK REINFORCEMENT						
BRIDGE NO.	TYPE	BARS	SIZE	"A"	NO. REQUIRED	V LENGTH
79-140-5.09 EAST & WEST BOUND	PRESTRESSED I BEAM	A800	8		36	1'-11"
		U501	5	90.82	32	7 1/8" 1'-11 1/8"
		U502	5	74.73	24	7 1/2" 1'-11 1/4"
		U503	5	57	16	8 1/4" 2'-0 1/4"

DIMENSIONS ARE OUT TO OUT ALONG SKEW



BAR BENDS

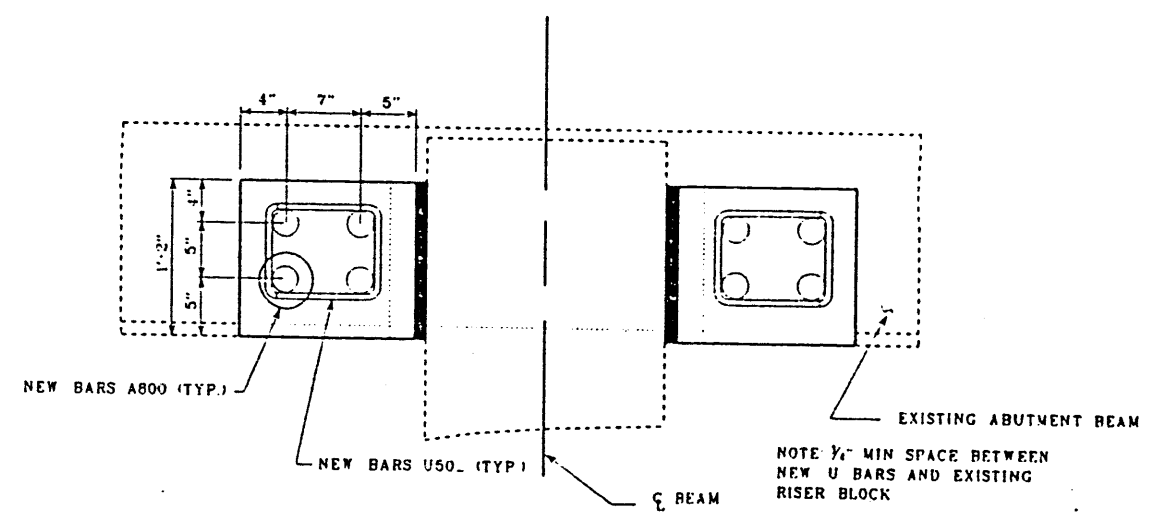


TYPICAL CROSS SECTION @ ABUTMENT (TYPE "Y")

NOTE: CONTRACTOR TO KEEP CONCRETE OFF THE EXISTING BEARING PADS

NOTE: CONTRACTOR TO KEEP BLOCK AT LEAST 1" FROM FACE OF DIAPHRAGM OR BACKWALL

NOTE: CONTRACTOR TO REMOVE 1/2" OF CONCRETE IN BLOCK LOCATIONS FOR BONDABLE SURFACE



PLAN

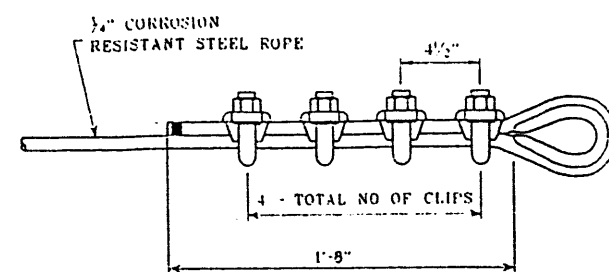
LATERAL RESTRAINER TYPE Y

DESIGNED BY Brian Lyle DATE September 1997
 DRAWN BY Cory Hargis DATE December 1997
 SUPERVISED BY Mike Jensen DATE December 1997
 CHECKED BY Mike Jensen DATE February 1998

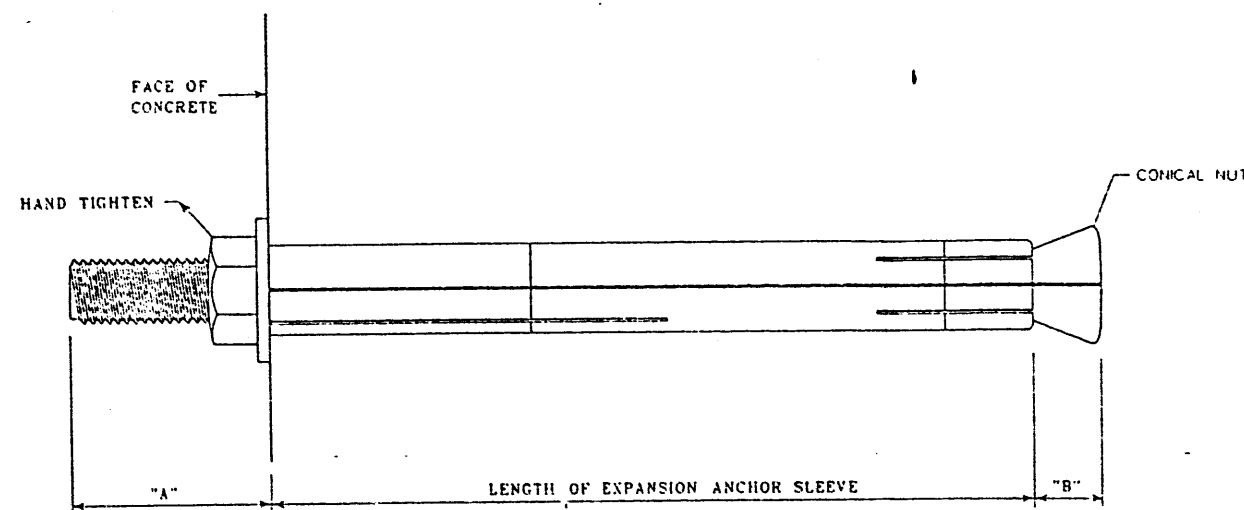


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

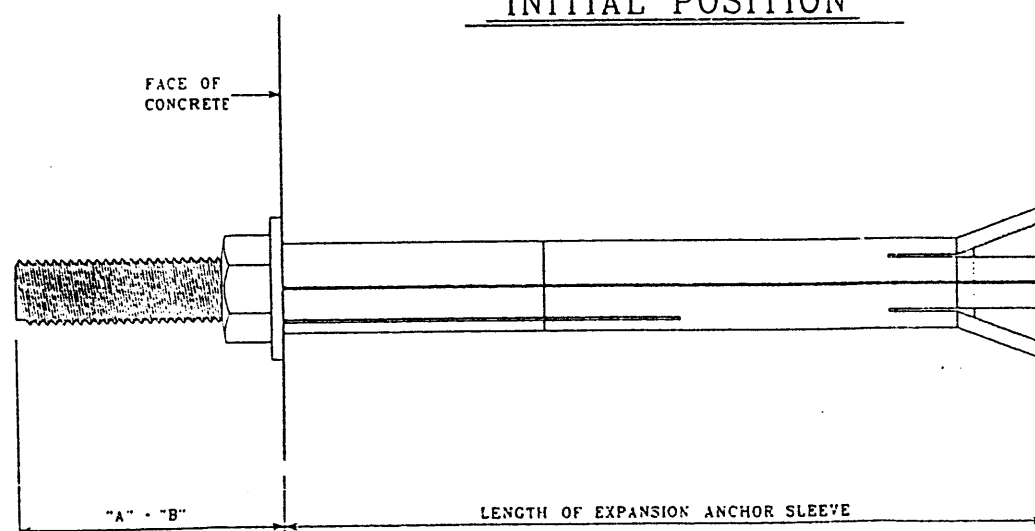
LATERAL RESTRAINER DETAILS
 TYPE Y
 SHELBY COUNTY
 1998



WIRE ROPE CLIPS



INITIAL POSITION



SET POSITION

ANCHOR SETTING DETAILS

PROCEDURE FOR INSTALLATION OF ANCHOR BOLTS:

(3/4" DIAMETER DRILLCO MAXI-BOLT OR WILLIAMS BOLTS UNDERCUTTING REQUIRED)

1. LOCATE PLACEMENT OF EXISTING REBAR IN VICINITY OF ANCHORS WITH A REBAR LOCATING DEVICE AND MAKE NECESSARY CORRECTIONS IN LOCATIONS OF ANCHORS ON CONCRETE. ANCHOR LOCATION MAY VARY PLUS OR MINUS 3 INCHES IN ANY DIRECTION BUT THE HOLE SHALL BE DRILLED WITHIN 6 DEGREES OF PERPENDICULAR TO THE NOMINAL CONCRETE SURFACE. CUTTING OF REBAR WILL BE ALLOWED.
2. HOLES SHALL BE DRILLED WITH A CARBIDE PERCUSSION DRILL BIT, A "REBAR EATER" BIT OR A DIAMOND CORE BIT.
3. THE DRILL BIT DIAMETER AND HOLE DEPTHS ARE SPECIFIED AS FOLLOWS:
 - a) THE MAXIMUM DRILL BIT DIAMETER SHALL NOT EXCEED 1 1/2 INCH DIAMETER.
 - b) THE HOLE DEPTH SHALL NOT BE LESS THAN THE ANCHOR EMBEDMENT PLUS 1/2 INCHES BUT MAY EXCEED THE SPECIFIED HOLE DEPTH BY NOT MORE THAN 1 INCH.
4. IF AN ANCHOR MUST BE RELOCATED AND A NEW HOLE DRILLED, THE OLD HOLE SHALL BE REPAIRED WITH A NON-SHRINKAGE PACK GROUT.
5. UNDERCUT IN PRIMARY HOLE SHALL BE AS SPECIFIED BY THE MANUFACTURER OF THE UNDERCUTTING TOOL.
6. CLEAN THE HOLE OF CONCRETE DUST AND DEBRIS USING OIL FREE COMPRESSED AIR OR BY VACUUMING. PLACE BEARING SLEEVE FLUSH WITH THE CONCRETE SURFACE.
7. THE EXPANSION SLEEVE IS TO EXPAND INTO THE UNDERCUT CREATED BY THE UNDERCUTTING TOOL THEREFORE THE ANCHOR TUBE MUST TERMINATE AT THE BASE OF THE UNDERCUT SECTION.
8. TO SET THE ANCHOR, IT IS NECESSARY TO DRAW THE CONICAL NUT OF THE STUD BOLT INTO THE ANCHOR SLEEVE. AFTER THE ASSEMBLY IS INSERTED INTO THE DRILLED HOLE, THE ANCHOR WILL BE CONSIDERED SET WHEN THE DIMENSION "A" (SEE ANCHOR SETTING DETAILS) HAS INCREASED BY AN AMOUNT EQUAL TO DIMENSION "B". AFTER THE STEEL PLATES ARE IN PLACE THE FINAL TENSION LOAD OF 28400 LBS. SHALL BE APPLIED. THE ANCHOR LOADS MAY BE APPLIED BY MANUAL TORQUING OR HYDRAULIC TENSIONING.
9. BECAUSE OF CLOSE TOLERANCE BETWEEN CONICAL NUT OD AND HOLE ID IT MAY BE NECESSARY TO LIGHTLY HAMMER THE ANCHOR INTO THE HOLE. IF HAMMERING IS NECESSARY, STEPS SHALL BE EMPLOYED WHICH WILL PREVENT DAMAGE TO THE STUD BOLT THREADS.
10. INSTALLATION PROCEDURES REQUIRED BY THE ANCHOR MANUFACTURER IN ADDITION TO THE INSTRUCTIONS LISTED ABOVE SHALL BE FOLLOWED.
11. BENT PLATES SHALL BE ASTM A709 (GRADE 36) MATERIAL GALVANIZED TO ASTM A123 STANDARD.
12. POSITION OF PLATE OR ANGLE ON BEAM: ADJUSTMENTS THE PLATE OR ANGLE SHALL BE POSITIONED ON THE BEAM WITH CABLE IN THE FULL EXTENDED POSITION AND PLATE OR ANGLE POSITION MARKED THE PLATE OR ANGLE SHALL THEN BE SHIFTED TOWARD THE ABUTMENT 3" AND THE ANCHOR BOLT LOCATIONS MARKED THROUGH THE PLATE OR ANGLE ANCHOR HOLES BENTS (BEAM TO BEAM) AFTER ONE ANCHOR HAS BEEN ATTACHED THE ANGLE OF THE OTHER SHALL BE POSITIONED ON THE BEAM WITH CABLE IN THE FULL EXTENDED POSITION ANGLE POSITION MARKED. THE PLATE OR ANGLE SHALL THEN BE SHIFTED TOWARD THE BENT 3" AND THE ANCHOR BOLT LOCATIONS MARKED THROUGH THE ANGLE ANCHOR HOLE.

PROCEDURE FOR INSTALLATION OF ANCHOR BOLTS:

(3/4" HILTI BOLTS OR EQUIVALENT UNDERCUTTING REQUIRED)

1. INSTALLATION TO BE IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDED PROCEDURES.

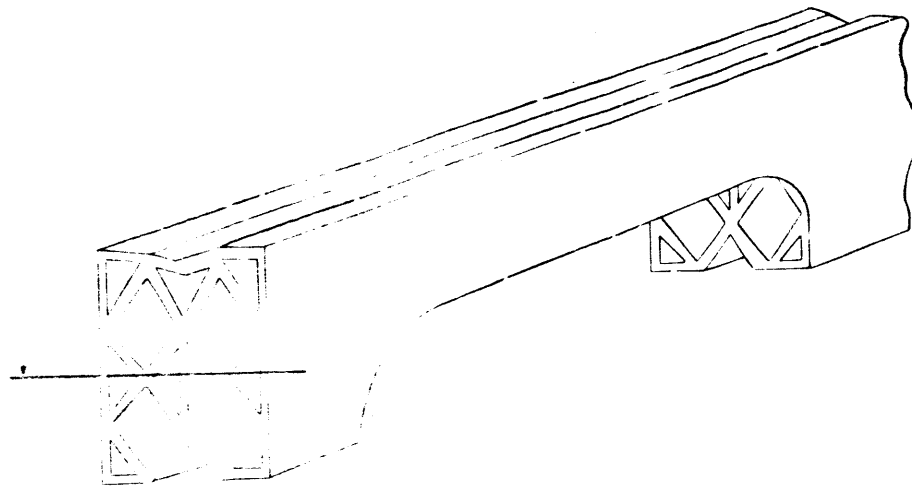
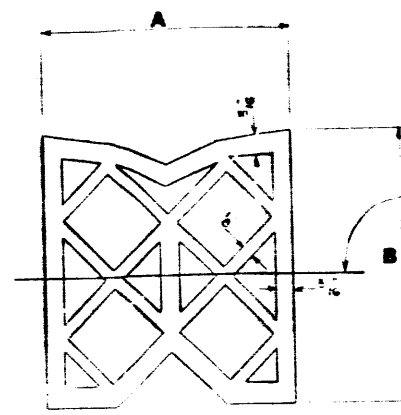
PROJECT NO.	YEAR	SHEET NO.	
1111111111111111	1998	1	
REVISIONS			
NO.	DATE	BY	REVISION
1	11-11-98	W.A.E.	REVISION 1



DEPARTMENT OF TRANSPORTATION
BOLT INSTALLATION
SHELBY COUNTY
1998

DESIGNED BY: Brian E. G. DATE: January 1998
 DRAWN BY: C. J. H. DATE: March 1998
 SUPERVISED BY: Mike J. J. DATE: March 1998
 CHECKED BY: Mike J. J. DATE: April 1998

NO.	DATE	REVISIONS
1	2-4-70	W.B. ADDED NEW SIZE
2	1-16-71	W.B. ADDED NEW SIZE
3	2-27-71	W.B. ADDED NEW SIZE
4	2-27-74	R.M.D. ADDED NOTE TO TABLE OF DIMENSIONS



Preformed Elastic Joint Sealer

The preformed elastic joint sealer is a synthetic rubber compound used for sealing joints in concrete and steel structures. The sealer is formed into a diamond-shaped cross-section and is designed to provide a permanent, flexible seal. The sealer is installed by sawing a groove in the concrete or steel and inserting the sealer into the groove. The sealer is then compressed to fill the groove and provide a tight seal. The sealer is designed to last for many years and to resist weathering and chemical attack.

TABLE OF CONSTANT DIMENSIONS

TYPE	DIMENSIONS (INCHES)				L CONCRETE	L STEEL
	A	B	C	Δ TOTAL		
I	3	3 3/8	3 3/4	1 1/2	25"	100"
II	3 1/2	3 3/8	4 1/4	1 3/4	34"	147"
III	4	4 3/8	4 3/4	2	35"	214"

- Δ TOTAL - MAXIMUM ALLOWABLE TOTAL MOVEMENT
- L CONCRETE - MAXIMUM ALLOWABLE LENGTH OF UNRESTRAINED EXPANSION FOR CONCRETE STRUCTURES AND IS BASED ON TEMPERATURE DIFFERENTIALS OF +60°F TO +95°F
- L STEEL - MAXIMUM ALLOWABLE LENGTH OF UNRESTRAINED EXPANSION FOR STEEL STRUCTURES AND IS BASED ON A TEMPERATURE DIFFERENTIAL OF 0°F TO +120°F
- ③ DIMENSIONS A, B & C MAY VARY WITH EACH MANUFACTURER OF PREFORMED ELASTIC JOINT SEALER. CONTRACTOR TO VERIFY DIMENSIONS BEFORE SAWING OPENING.

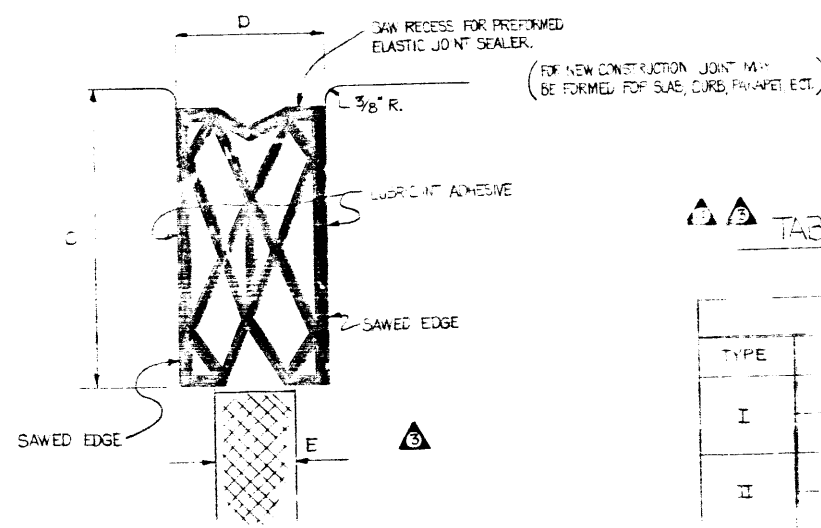
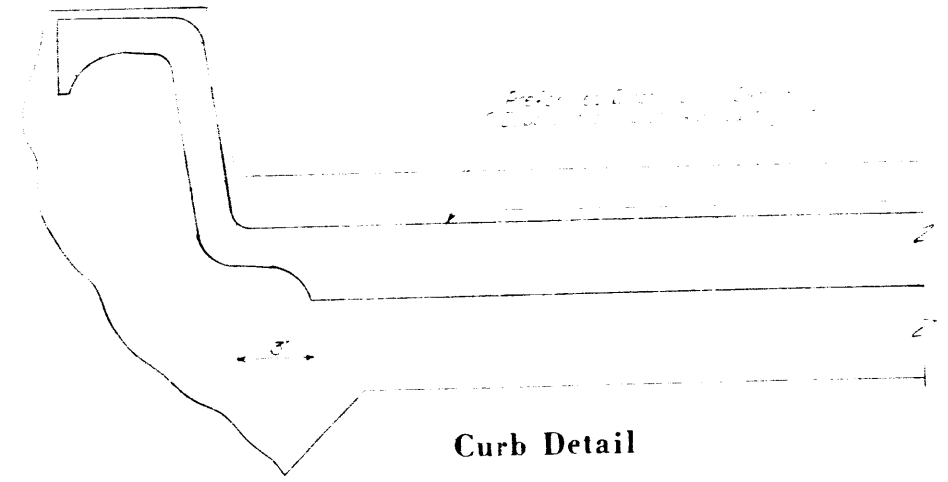


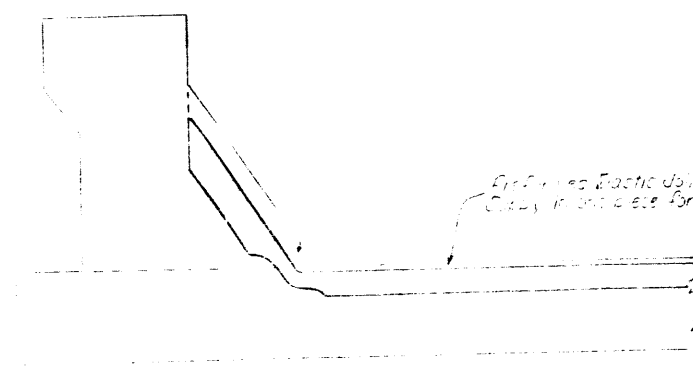
TABLE OF VARIABLE DIMENSIONS (INCHES)

TYPE	D.M.	STRUCTURE TEMPERATURE (°F)						Concrete	Steel
		25°	0°	40°	60°	75°	90°		
I	D	2 1/2"	2 1/8"	1 3/4"	1 3/8"	1"	5/8"	1"	1"
	E	1 3/4"	1 3/8"	1"	5/8"	3/4"	3/8"	1/4"	1/4"
II	D	2"	2 1/2"	2 1/8"	1 3/4"	1 3/8"	1"	1 1/4"	1 1/4"
	E	1 1/2"	1 3/8"	1 1/4"	3/4"	3/4"	3/8"	3/8"	3/8"
III	D	2 1/2"	2 1/8"	1 3/4"	1 3/8"	1"	5/8"	1 1/8"	1 1/8"
	E	1 3/4"	1 3/8"	1"	5/8"	3/4"	3/8"	1/4"	1/4"

SECTION A-A

BOTH EDGES TO BE SAWED AT THE SAME TIME. CONTRACTOR SHALL USE DOUBLE ENDING BLADES. SAW WATER COOLED WITH AT LEAST A 50 H.P. MOTOR. JOINTS TO BE SAWED IN CURB ALSO.

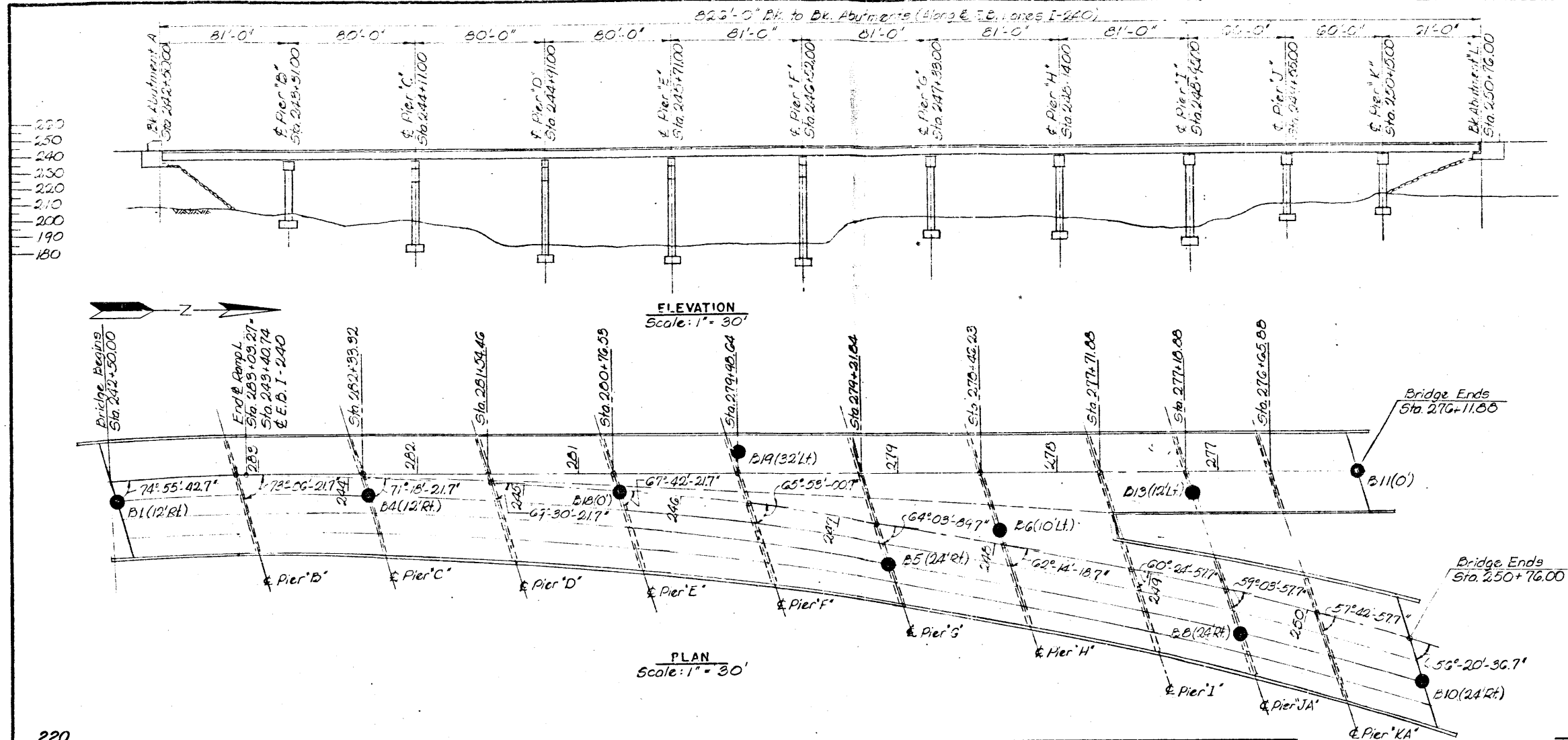
NOTE: DIMENSIONS D AND E ARE TO BE DETERMINED BY THE ENGINEER BASED ON THE TEMPERATURE OF THE STRUCTURE AT THE TIME OF INSTALLATION. IT SHOULD BE NOTED THAT THE TEMPERATURE OF THE STRUCTURE MAY VARY WITH THE TIME OF DAY AND SEASON.



Parapet Detail

79-140-509R

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
NASHVILLE
STANDARD
PREFORMED ELASTIC
JOINT SEALER



CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
19007-3127-60	101-240-11300	1975	65

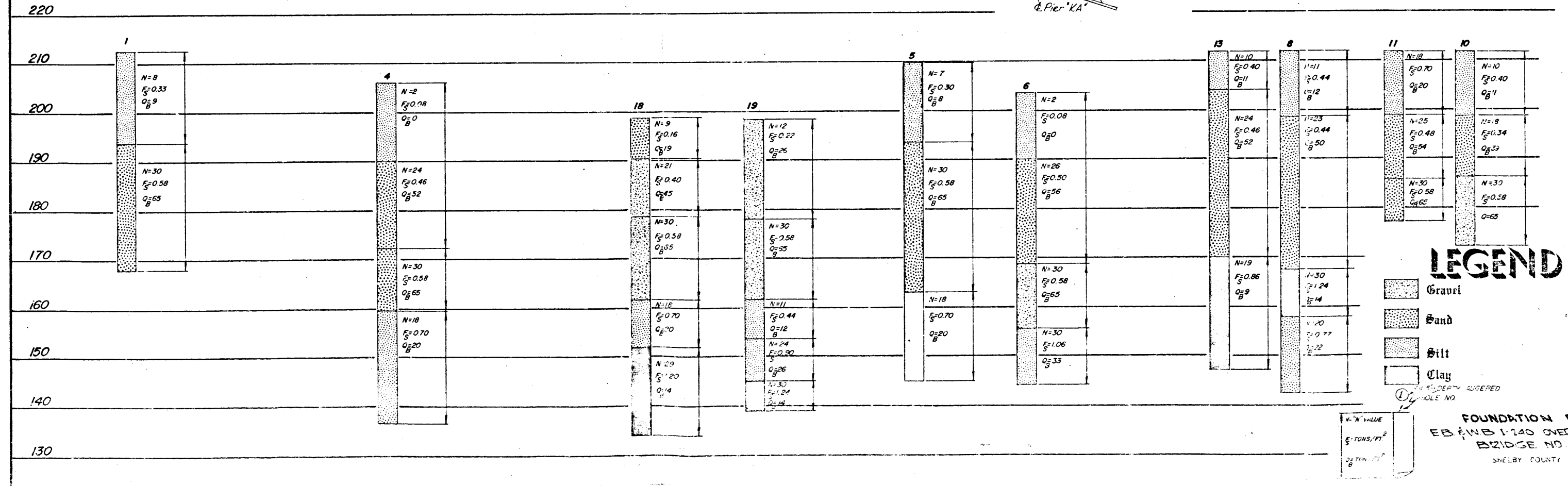
NO.	DATE	BY	DESCRIPTION
1	5-24-74	CEH	PILE DATA, PIER JA
2	9-9-74	CEH	PILE DATA, PIERS KA & KB

SUMMARY OF PILE DATA

	Pile Design Load ①	Pile Cut-off Elev.	Pile Tip Elev.	Pile Length ②
Abut A	90 Tons	Varies	190.0	50'
Pier B	85 Tons	196.75	181.75	15'
Pier C	85 Tons	186.50	166.50	20'
Pier D	85 Tons	181.50	166.50	15'
Pier E	90 Tons	181.50	166.50	15'
Pier F	80 Tons	181.50	166.50	15'
Pier G	95 Tons	191.50	176.50	15'
Pier H	85 Tons	191.75	176.75	15'
Pier I	85 Tons	196.75	181.75	15'
Pier JA	44 Tons	201.92	181.92	20'
Pier JB	75 Tons	202.75	187.75	15'
Pier KA	95 Tons	214.50	199.50	15'
Pier KB	75 Tons	211.50	196.50	15'
Abut LA	65 Tons	Varies	197.0	45'
Abut LB	65 Tons	Varies	197.50	50'

① Design Loads based on Factored Loads.
 ② Pile Lengths subject to change after reviewing results of load tests.

Note: This drawing not to be used as a layout.

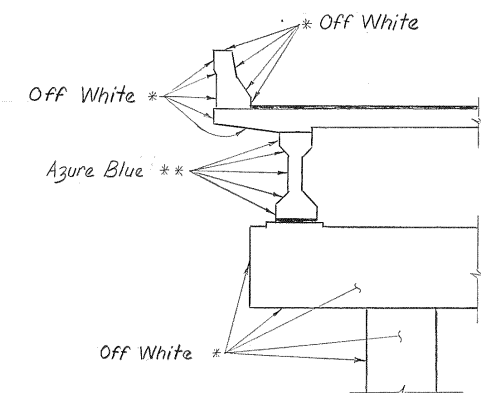


MICROFILMED

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-40	EACI-240-11339	1975	25

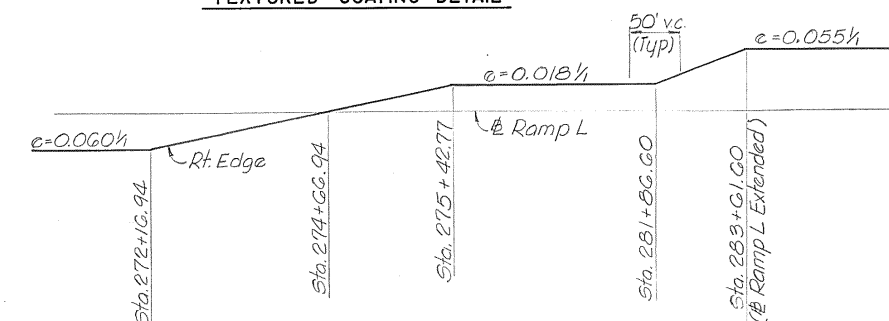
[illegible]

13. BRIDGE DECK FORMS: Bridge deck forms for concrete decks shall be constructed using either removable forms or permanent forms. In either case, forms shall be attached by means other than welding to support members. See Special Provision No. 450, "Special Provision Regarding Permanent Steel Bridge Deck Forms", Revised November 9, 1973.
14. COFFERDAM: Foundation Preparation, The lump sum bid for cofferdam items shall be full compensation to the contractor for preparation of foundations prior to pouring concrete for footings. The contractor shall be paid for excavation in accordance with the standard specifications and the contract unit bid price for each excavation item, except that no percent increase will be allowed for extra depth excavation. If cofferdams are required, they shall be in accordance with Section 204 of the Standard Specifications. Regardless of whether cofferdams are used the contractor shall be paid for Items 204-15.04, 204-15.05 & 204-15.06. The cost of seal concrete if required is to be included in the cofferdam lump sum bid.
15. LINSEED OIL PROTECTIVE TREATMENT: Surfaces receiving Textured Coated Finish shall not receive a linseed oil treatment. See Special Provision regarding Section 604 - Concrete Structures.
16. ELASTOMERIC BEARING PADS: In lieu of the bearing pads shown on these plans the contractor may submit shop plans and design calculations of alternate bearing devices to the Engineer of Structure for approval. Bearing seat elevations shall be adjusted to compensate for differences in bearing heights. The bearings shall be capable of providing the following minimum requirements under service loads. (Laminated pads - 50 durometer req'd., plain pads 70 durometer req'd.)
- | | |
|-------------------------|------------------|
| | E-2 |
| Total Movement Req'd. | 0.167 |
| Dead Load Reaction | 67 [±] |
| Live Load Reaction | 55 [±] |
| Total(DL+LL+I) Reaction | 136 [±] |
17. BAR DESIGNATION: The first number of all bar marks is the size of the bar. e.g. H400 = #4 bar ; H1100 = #11 bar

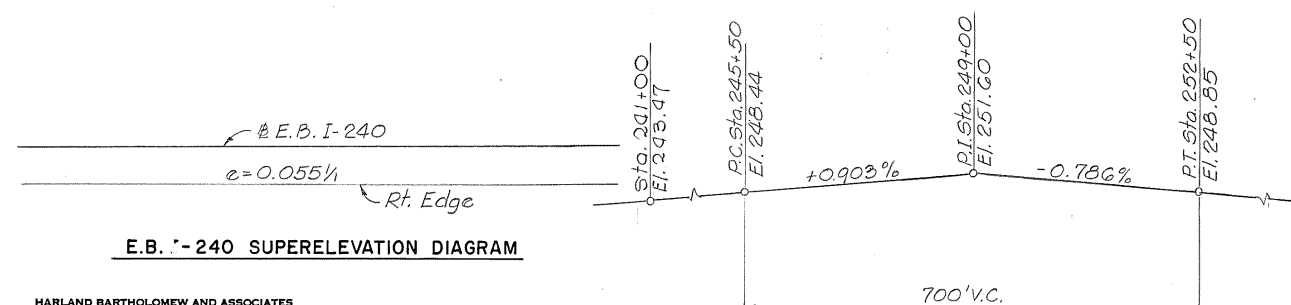


All surfaces marked with () shall have a textured coated finish similiar to Off White (Fed. Spec. No. 37778), in addition to the surfaces marked, all exposed abutment, endwall, wingwall and wingpost surfaces shall receive the same finish. Surfaces marked with (**) shall receive a textured coated finish similiar to Azure Blue (Fed. Spec. No. 35190.).*

TEXTURED COATING DETAIL ²



RAMP L SUPERELEVATION DIAGRAM



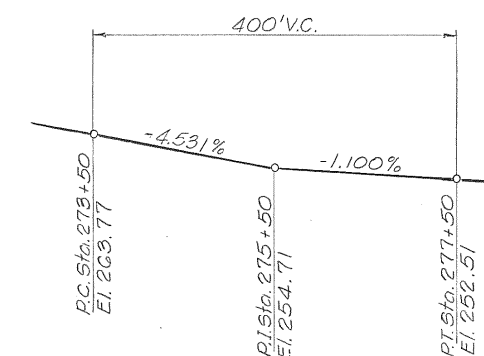
E.B. 1-240 SUPERELEVATION DIAGRAM

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY D. McCorkle DATE Oct. 1975
 DRAWN BY J. Starr DATE Oct. 1975
 SUPERVISED BY D. McCorkle DATE Oct. 1975
 CHECKED BY F. Hoffman DATE Oct. 1975

E.B. I-240 PROFILE

E.B. 1-240 PROFILE
Elevations Shown are based on finish Grade



RAMP L PROFILE

RAMP E PROFILE
Elevations Shown are based on finish Grade

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

GENERAL NOTES
STA. 242+50.00

SHELBY COUNTY

CORRECT _____
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

(4) The cost of furnishing and installing 1700 Lin. ft. of 2"Ø conduit, 4 junction boxes and expansion couplings, 20 anchor bolts, and all auxiliary equipment shall be included in the price bid for Structure Lighting Lump Sum.

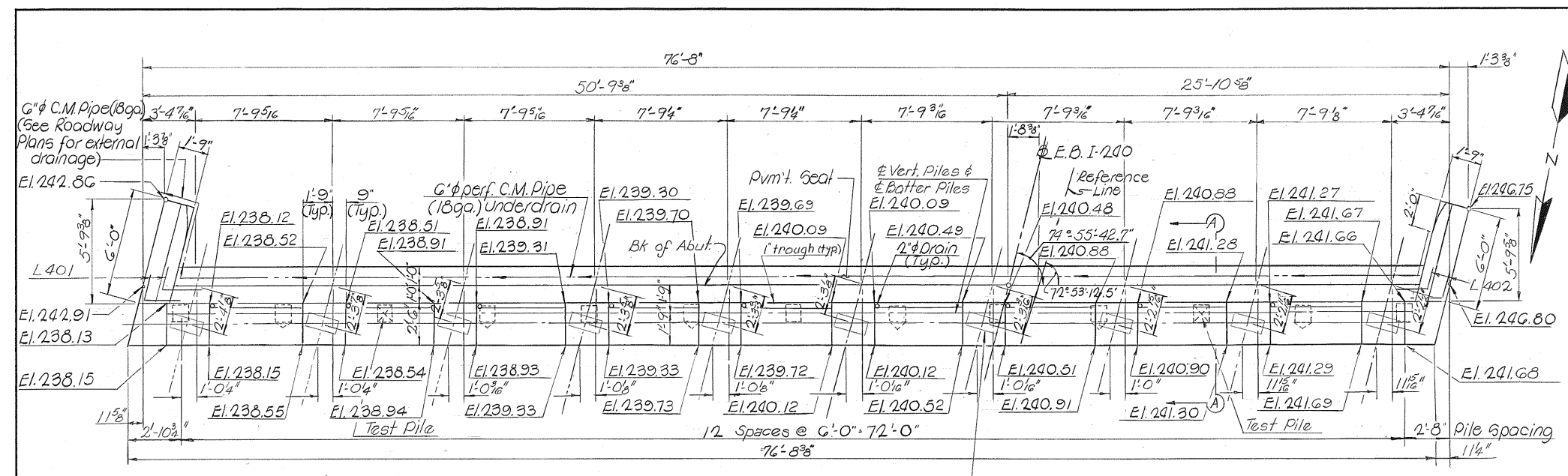
(10). The cost of 3 Apron Spillway Drains shall be included in the cost of Roadway Items.

SUMMARY OF ESTIMATED QUANTITIES																			
Item No.	204-02.01	204-03.01	204-15.04	204-15.05	204-15.06	602-05.07	604-03.01	604-03.02	604-03.03	604-25.04	606-09.01	606-09.02	606-09.03	615-01.03	616.08	710-10	710-11	714-01.02	908-21.02
Description	Dry Excavation (Bridges)(1)	Wet Excavation (Bridges)(1)	Cofferdam	Cofferdam	Cofferdam	Preformed Elastic Joint Sealer-Type III	Class "A" Concrete (Bridges)	Steel Bar Reinforcement (Bridges)	Linseed Oil Treatment	Textured Coated Finishing	Test Piles (Precast Conc. Size 1)	Loading Test (Precast Conc. Size 1.)	Precast Conc. Piles (Size 1)(2)	Prestressed Conc. T-Beam (Type III)(7)	Concrete Parapet (M-28-1)(6)	6"4perft.CMP (18ga.)1/porous Backfill(3)	6"4 CMP Underdrains (18 ga.)	Structure Lighting (4)	Bearings E-2
Unit	Cu.Yds.	Cu.Yds.	Lump Sum	Lump Sum	Lump Sum	Lin. Ft.	Cu.Yds.	Pound	Sq. Yds.	Sq. Yds.	Lin. Ft.	Each	Lin. Ft.	Lin. Ft.	Lin. Ft.	Lin. Ft.	Lin. Ft.	Lump Sum	Each
Abutment A	80					76	51.9	7890	30	25	100		550			82	4		10
Pier B	235	145					147.7	19060		260	30		690						
Pier C	175	534					189.1	30910		275	40		1240						
Pier D		706	1				197.3	32960		315	30		930						
Pier E		637		1			202.5	34170		330	30		930						
Pier F		754			1		247.9	39540		395	30		1170						
Pier G	310	434					213.2	31470		365	30		870						
Pier H	125	495					247.1	32400		465	30		1050						
Pier I	330	250					250.8	37920		485	30		1050						
Pier JA	299						143.8	18438		255	40		1160						
Pier KA	400						106.2	16,334		260	30		510						
Pier JB	165						84.6	12820		190	15	1	345						
Pier KB	300						78.4	11,748		190	30		345						
Abutment LA	85					52	54.7	8260	30	25	90		585			87	4		9
Abutment LB	60					81	36.4	5260	20	20	50		400			58	4		6
Pvmt. @ Bridge Ends							192.5	49180	515				200						
Superstructure							2075.8	595959	8485	3130				10539	2052			1	
Total	2564	3955	1	1	1	209	4519.9	984319	9080	6985	605	1	12,025	10539	2052	227	12	1	25

M-44-93

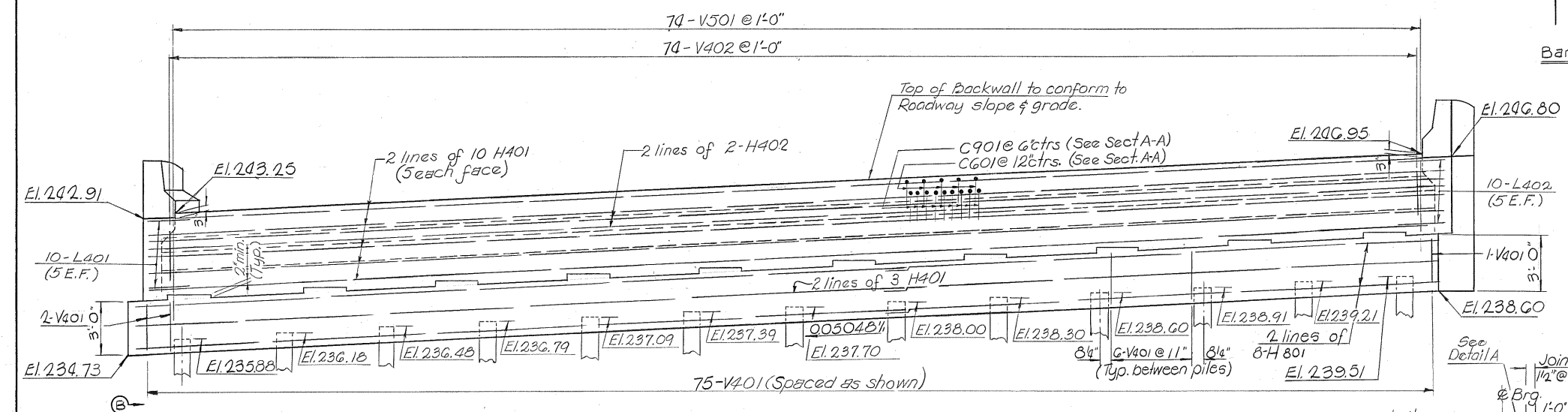
SCANNED

MICROFILMED



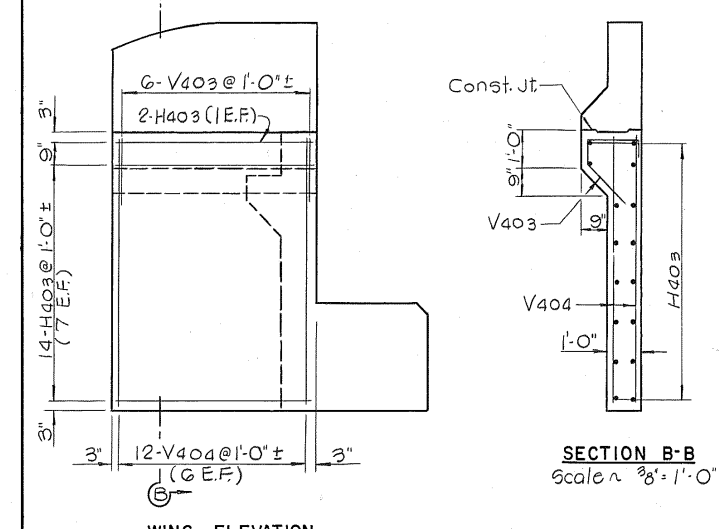
PLAN
Scale 1/4"=1'-0"

Elastomeric Bearing Pad E-2
(5'8" x 16" x 1'-10") (Typ. each bearing)



ELEVATION
Scale 1/4"=1'-0"

Note: Drive all piles to an ultimate capacity of 90 tons. 13 piles required. See Dwg. M-44-127 for additional pile data.
Denotes direction of batter.



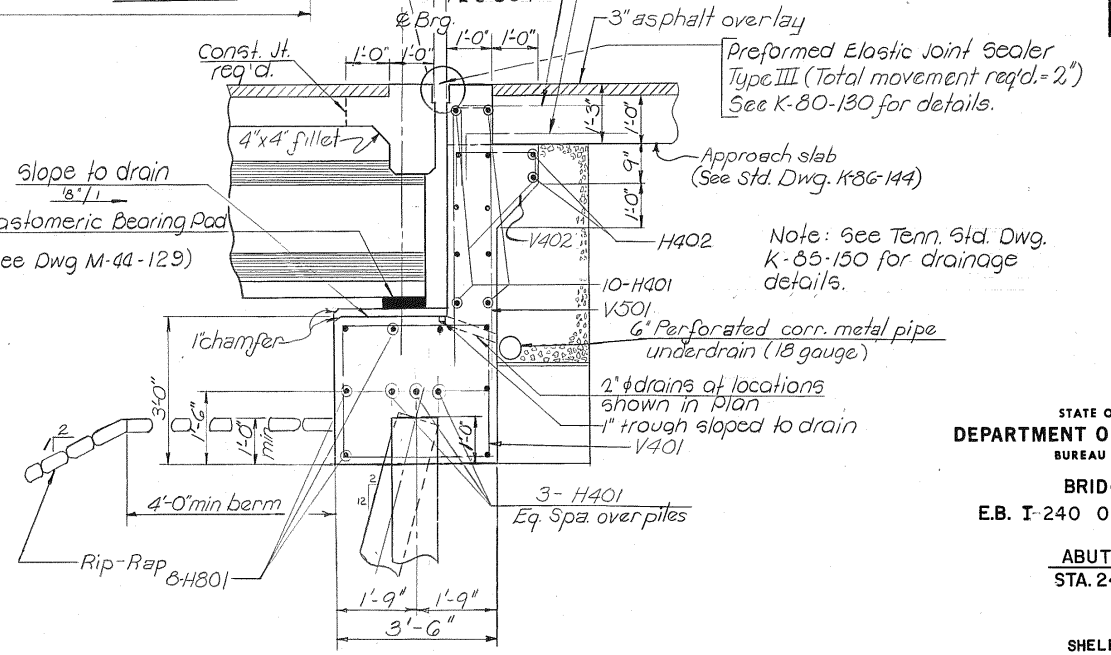
SECTION B-B
Scale 1/8"=1'-0"

WING ELEVATION
Scale 1/8"=1'-0"

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

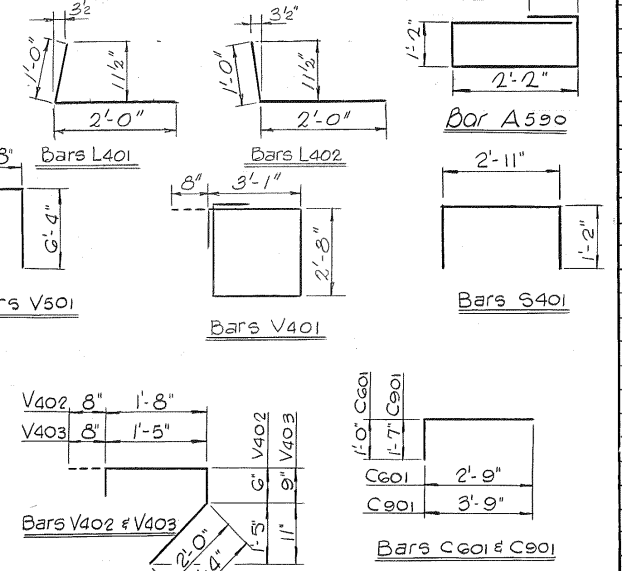
DESIGNED BY D. McCorkle DATE 10-75
DRAWN BY J. Milam DATE 10-75
SUPERVISED BY D. McCorkle DATE 10-75
CHECKED BY F. Hoffman DATE 10-75

SUMMARY OF ESTIMATED QUANTITIES				
Item No.	Item	Unit	Quantities	
			Abut. A	Approach Slab
204-02.01	Dry Excavation	Cu. Yds.	80	—
604-03.01	Class A Concrete (Bridge)	Cu. Yds.	51.9	74.1
104-03.02	Steel Bar Reinforcement (Bridge)	Lbs.	7890	18340
606-09.01	Test Piles (Precast Concrete Size 1)	Lin. ft.	100	—
406-09.03	Precast Concrete Piling - Size 1	Lin. ft.	550	90
710-10	G.4 Perf. C.M.P. (18Ga) Porous Backfill	Lin. ft.	82	—
710-11	G.4 C.M.P. Underdrain (18Ga)	Lin. ft.	4	—



SECTION A-A
Scale 1/2"=1'-0"

- NOTES: Riser Blocks to be poured monolithically w/ Abut. beam Riser Block, bearing pad surface to conform to bottom of beam grade.
- The backwall shall not be poured until the beams are in place. The end 2 feet of slabs at expansion joints and top 15' of the abutment backwall shall be poured concurrently, and are to be poured after all other pours are complete.
 - For bearing Details see Dwg. M-44-129.
 - See Std. Dwg. M-28-1 for Details of Wingpost and Reinforcement to be placed in wingwalls.
 - See Std. Dwg. K-86-144 for Detail of Approach.
 - All drainage pipes to have a minimum downward slope of 1/8" / F.
 - Minimum Bar Laps shall be as follows
H400 = 1'-4" H801 = 3'-0"
 - Cost of Wingpost is to be included in the cost of the Bridgerail system.



CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EACI-240-1(133)9	1975	27

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

BILL OF STEEL			
Mark	No. Required	Length	Shape
H401	26	38'-10"	—
H402	4	37'-0"	—
H403	3/2	5'-8"	—
H801	16	39'-8"	—
V401	75	12'-10"	□
V402	74	4'-10"	□
V403	12	4'-2"	□
V404	24	7'-2"	—
V501	74	13'-4"	□
S401	30	5'-3"	□
C601	74	3'-9"	□
C901	147	5'-4"	□
L401	10	3'-0"	□
L402	10	3'-0"	□
APPROACH SLAB			
A490	100	37'-3"	—
A590	132	7'-6"	—
A690	71	24'-0"	—
A790	16	38'-2"	—
A990	141	24'-0"	—

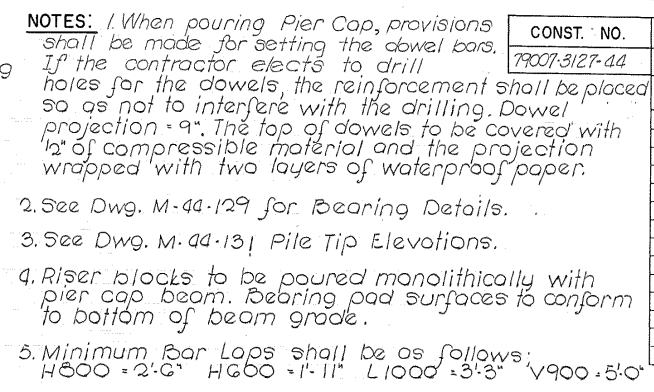
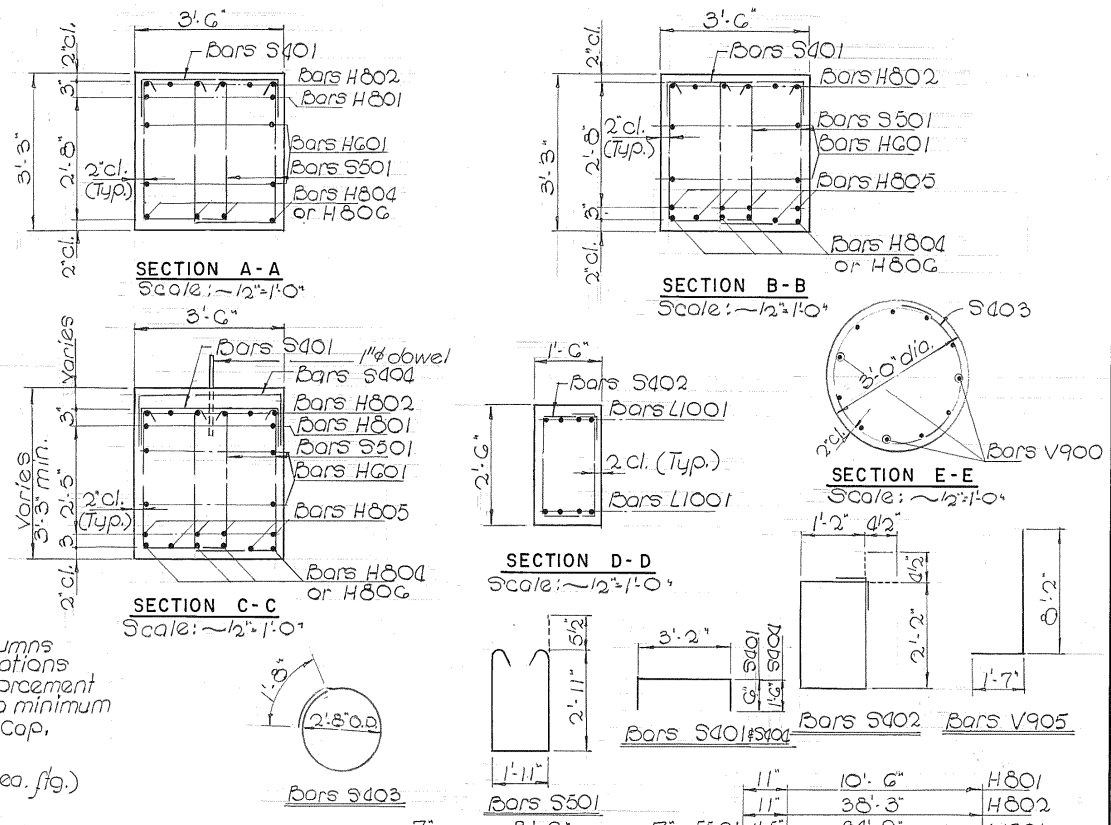
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE 32B
E.B. I-240 OVER WOLF RIVER

ABUTMENT A
STA. 242+50.00

SHELBY COUNTY

CORRECT _____
ENGINEER OF STRUCTURES
APPROVED _____
DIRECTOR OF HIGHWAYS

MICROFILMED

[illegible]

BILL OF STEEL			
Bar	No.	Length	Shape
H801	4	11'-5"	C
H802	12	39'-2"	C
H803	8	11'-6"	—
H804	4	51'-7"	—
H805	6	60'-0"	—
H806	4	25'-1"	—
H807	8	37'-8"	—
V901	11	41'-11"	—
V902	11	40'-11"	—
V903	11	39'-11"	—
V904	11	38'-11"	—
V905	44	9'-9"	J
L1001	16	36'-2"	C
S501	148	8'-8"	M
S401	38	4'-2"	┌
S402	54	7'-5"	□
S403	174	10'-0"	○
S404	30	6'-2"	└
FG01	72	13'-4"	C
F501	76	9'-8"	C

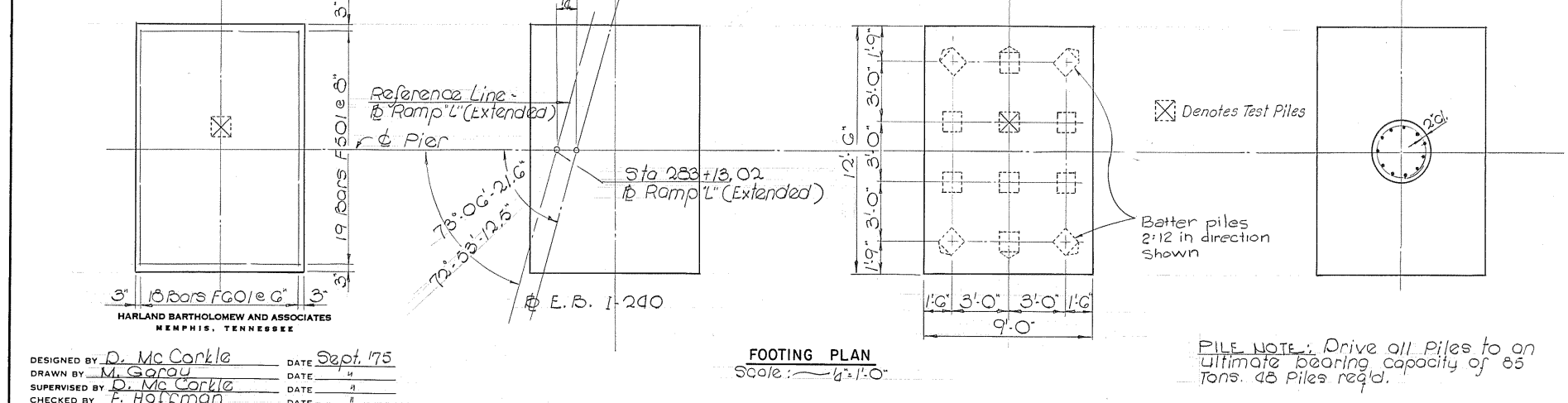
ESTIMATED QUANTITIES			
Item	Description	Unit	Quantity
G04-03.01	Class A Concrete	Cu.Yd	147.7
G04-03.02	Steel Bar Reinforcement	Lbs.	19060
204-02.01	Dry Excavation	Cu. Yd	235
204-03.01	Wet Excavation	Cu. Yd.	145
G06-09.01	Test Piles(Precast Conc. Size 1)	Lin.Ft.	30
G06-09.03	Precast Conc. Piles Size 1	Lin.Ft.	690

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

PIER 6
STA. 242 + 50.00

SHELBY COUNTY



PILE NOTE: Drive all Piles to an ultimate bearing capacity of 85 Tons. 48 Piles req'd.

DESIGNED BY D. Mc Corkle DATE Sept. 175
DRAWN BY M. Gorau DATE "
SUPERVISED BY D. Mc Corkle DATE "
CHECKED BY F. Hoffman DATE "

CORRECT _____
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

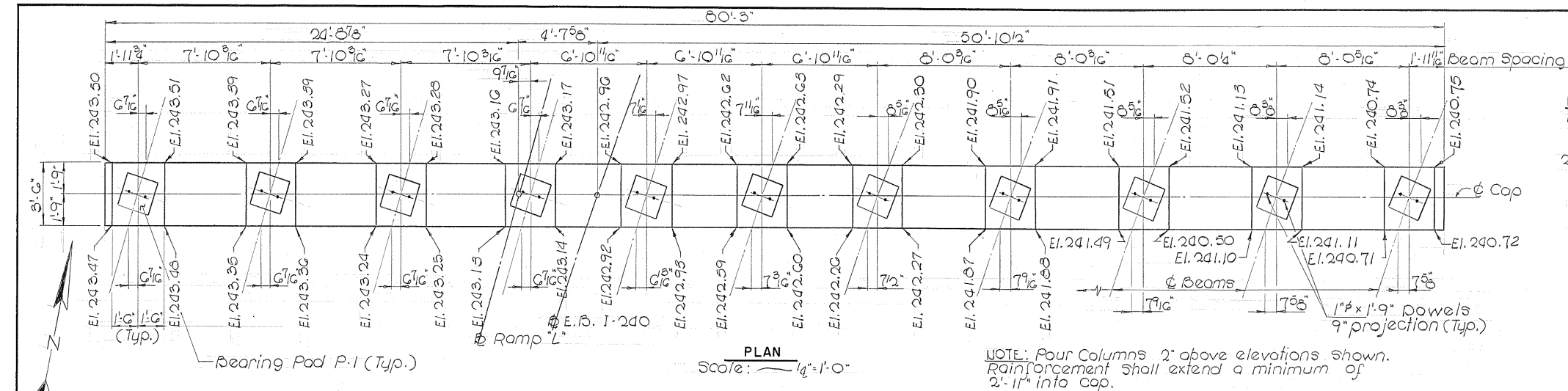
CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007 3127.41	EACI-240-1(133)9	1975	30

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

NOTES:

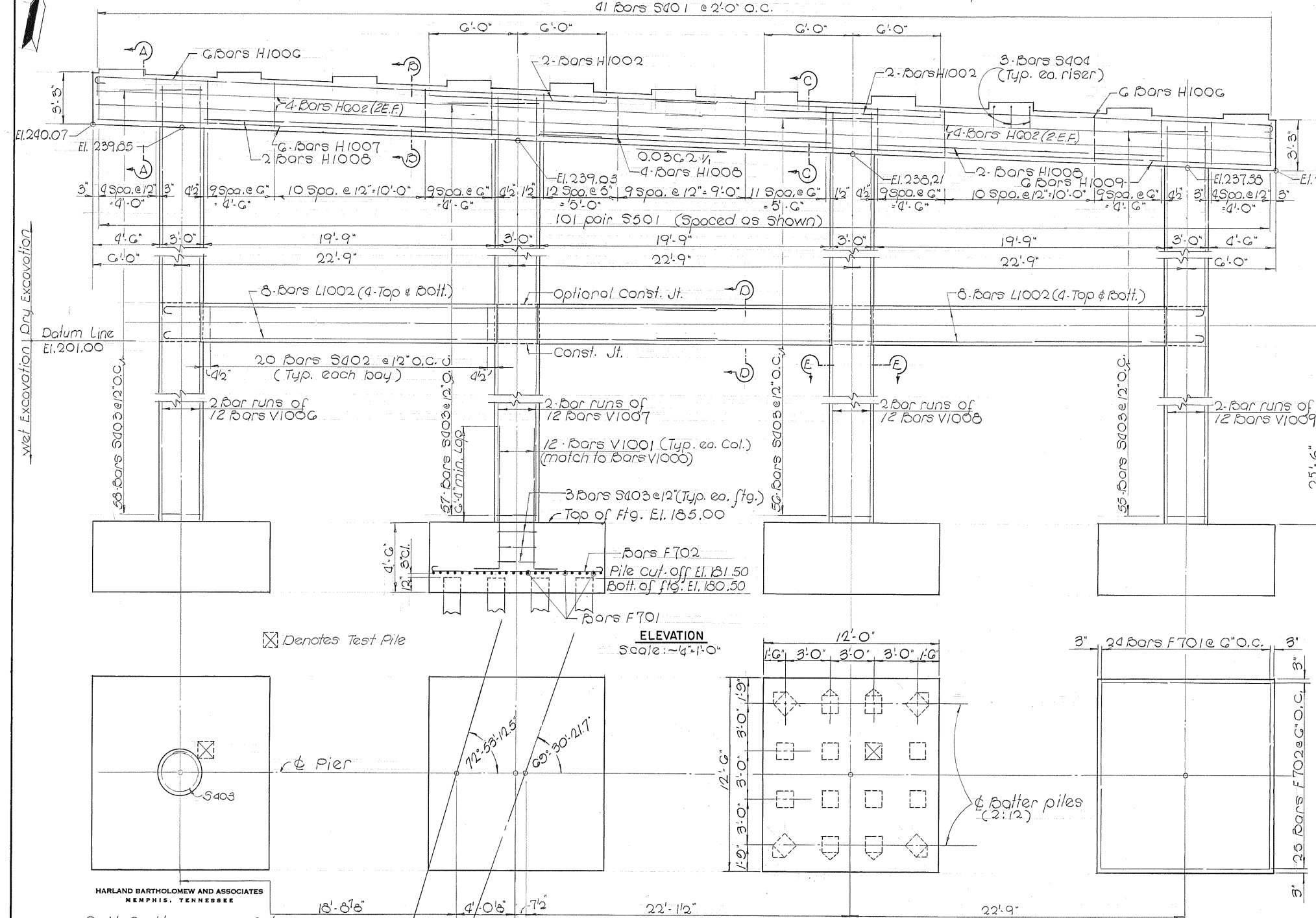
- Notes 1 thru 5 on Dwg. M-44-96 apply to this Dwg. also.
- Minimum Bar Laps shall be as follows:
H1000 = 3'-3"; H600 = 1'-11"; L1000 = 3'-3"
V1000 = 6'-4"



NOTE: Four Columns 2' above elevations shown. Reinforcement shall extend a minimum of 2'-11" into cap.

PLAN

Scale: 1/4" = 1'-0"

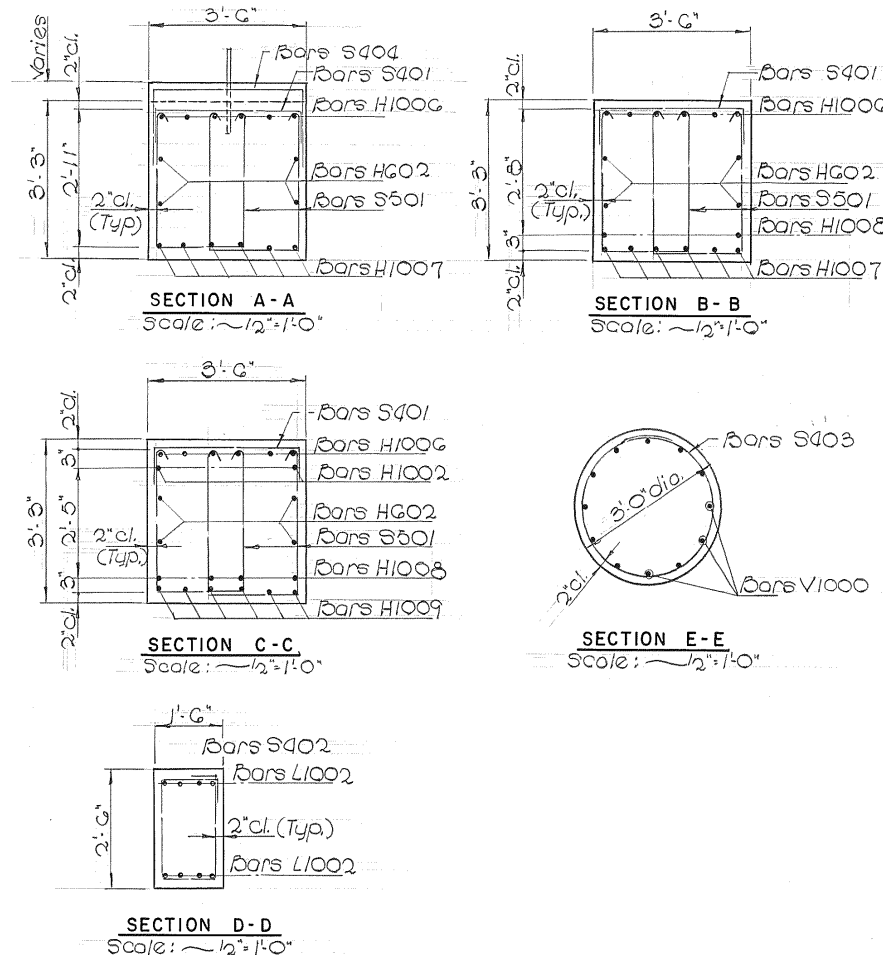


ELEVATION

Scale: 1/4" = 1'-0"

FOOTING PLAN

Scale: 1/4" = 1'-0"



SECTION E-E

Scale: 1/2" = 1'-0"

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

PIER D
STA. 242 + 50.00

SHELBY COUNTY

CORRECT ENGINEER OF STRUCTURES

APPROVED DIRECTOR OF HIGHWAYS

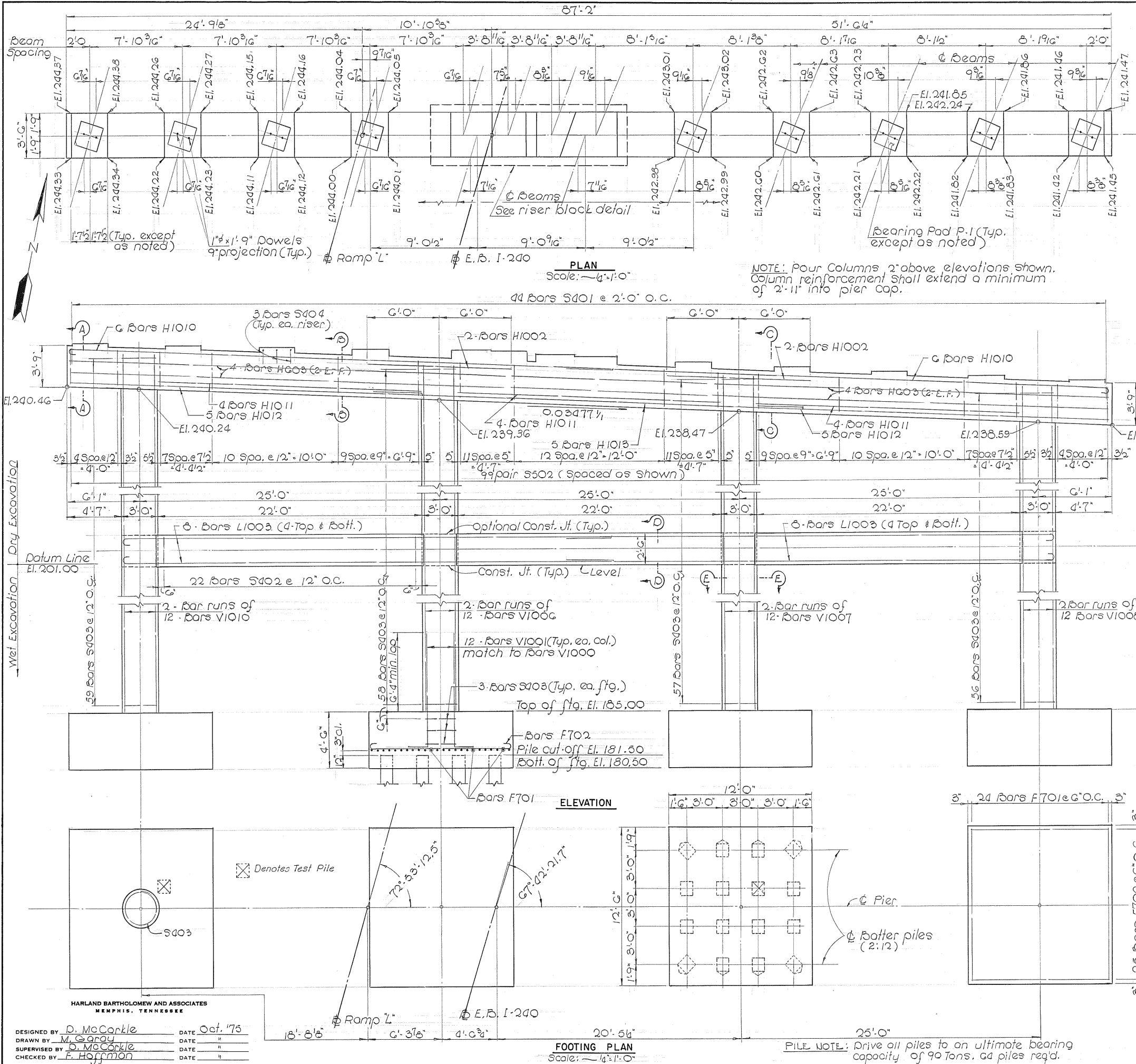
PILE NOTE: Drive all piles to an ultimate bearing capacity of 85 Tons. 64 piles req'd.

DESIGNED BY D. McCorkle
DRAWN BY M. Gandy
SUPERVISED BY D. McCorkle
CHECKED BY F. Hoffman

DATE Oct. '75
DATE
DATE
DATE

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

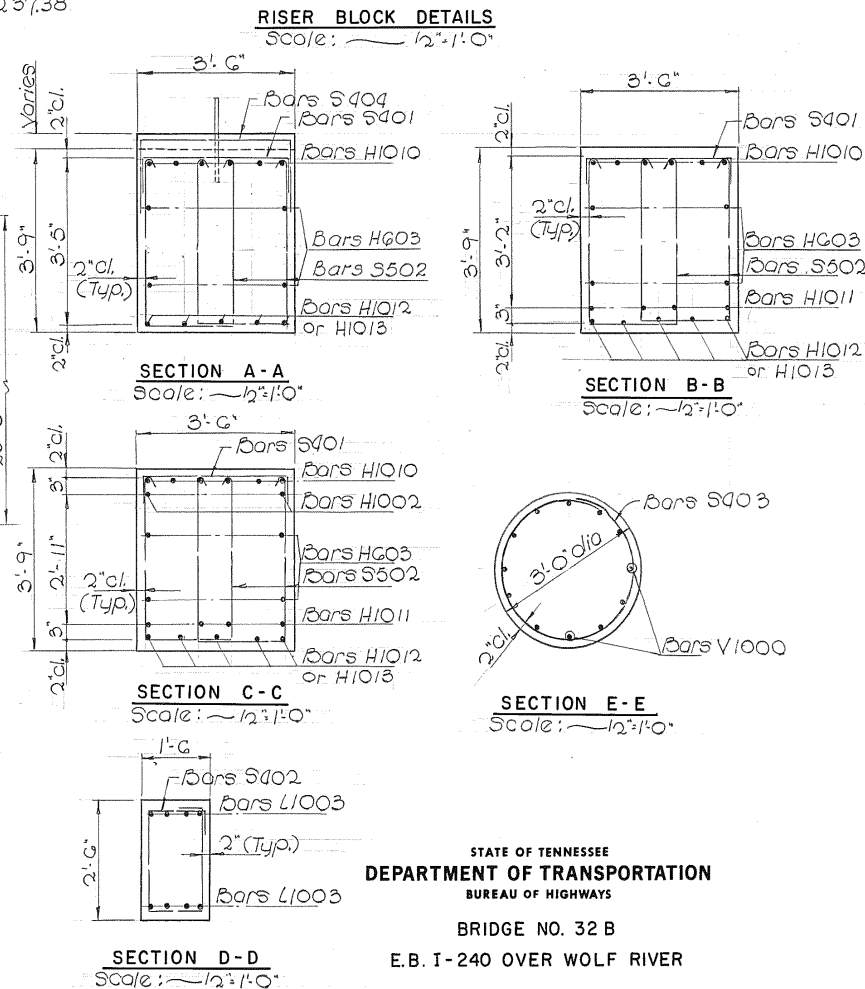
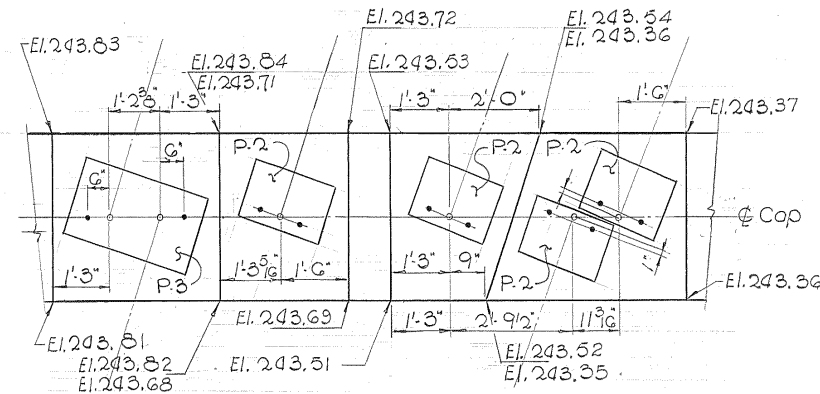
MICROFILMED



- NOTES:**
- Notes 1 thru 5 on Dwg. M-44-96 apply to this Dwg. also.
 - Minimum Bar Laps shall be as follows:
H1000 - 3'-3" H600 - 1'-11" L1000 - 3'-3"
V1000 - 6'-4"

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
T9007-3127-44	EACI-240-1(133)9	1975	31

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 32 B
E.B. 1-240 OVER WOLF RIVER
PIER E
STA. 242 + 50.00
SHELBY COUNTY

DESIGNED BY D. McCorkle
DRAWN BY M. Gentry
SUPERVISED BY D. McCorkle
CHECKED BY F. Hoffman

DATE Oct. '75
DATE
DATE
DATE

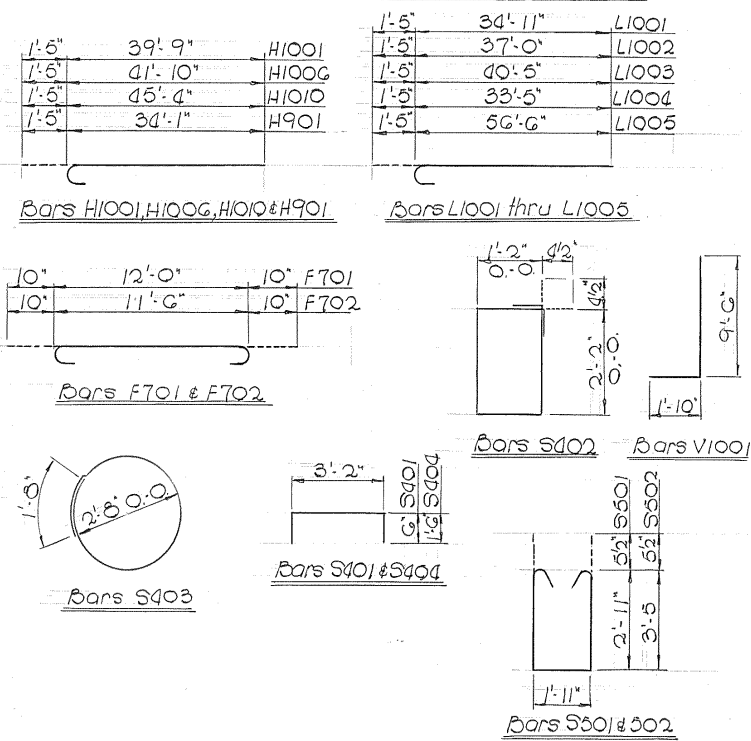
CORRECT
ENGINEER OF STRUCTURES
APPROVED
DIRECTOR OF HIGHWAYS

M-44-98

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	6-16-76	ret	Removed Load Test, Pier C

BILL OF STEEL

Bar	No. required				Length	Shape	Bar	No. required				Length	Shape
	Pier "C"	Pier "D"	Pier "E"	Pier "F"				Pier "C"	Pier "D"	Pier "E"	Pier "F"		
H1001	12	—	—	—	41'-2"	C	S501	168	202	—	206	8'-8"	M
H1002	4	4	4	—	12'-0"	—	S502	—	—	198	—	9'-8"	M
H1003	6	—	—	—	32'-5"	—							
H1004	6	—	—	—	47'-1"	—							
H1005	8	—	—	—	18'-4"	—	S401	39	41	44	49	3'-8"	□
H1006	—	12	—	—	43'-3"	C	S402	57	60	66	76	7'-3"	□
H1007	—	6	—	—	33'-10"	—	S403	203	226	230	291	10'-0"	○
H1008	—	8	—	—	19'-9"	—	S404	36	33	39	42	4'-8"	□
H1009	—	6	—	—	49'-10"	—							
H1010	—	—	12	—	46'-9"	C							
H1011	—	—	12	—	22'-0"	—	F701	96	96	96	120	13'-8"	C
H1012	—	—	10	—	29'-5"	—	F702	100	100	100	125	13'-2"	C
H1013	—	—	5	—	35'-6"	—							
H901	—	—	—	12	40'-4"	C							
H902	—	—	—	—	12'-0"	—							
H903	—	—	—	6	24'-2"	—							
H904	—	—	—	12	31'-5"	—							
H905	—	—	—	8	18'-3"	—							
H906	—	—	—	6	39'-6"	—							
H601	8	—	—	—	38'-6"	—							
H602	—	8	—	—	40'-7"	—							
H603	—	—	8	—	44'-1"	—							
H604	—	—	—	8	38'-8"	—							
H605	—	—	—	4	22'-11"	—							
V1001	48	48	48	60	11'-4"	—							
V1002	24	—	—	—	29'-3"	—							
V1003	24	—	—	—	28'-11"	—							
V1004	24	—	—	—	28'-6"	—							
V1005	24	—	—	—	28'-0"	—							
V1006	—	24	24	24	32'-1"	—							
V1007	—	24	24	24	31'-8"	—							
V1008	—	24	24	—	31'-3"	—							
V1009	—	24	—	—	30'-10"	—							
V1010	—	—	24	—	32'-6"	—							
V1011	—	—	—	24	33'-0"	—							
V1012	—	—	—	24	32'-8"	—							
V1013	—	—	—	24	32'-5"	—							
L1001	16	—	—	—	36'-4"	C							
L1002	—	16	—	—	38'-5"	C							
L1003	—	—	16	—	41'-10"	C							
L1004	—	—	—	8	34'-10"	C							
L1005	—	—	—	8	57'-11"	C							



ESTIMATED QUANTITIES

Item	Description	Unit	Quantities			
			Pier "C"	Pier "D"	Pier "E"	Pier "F"
004-03.01	Class A Concrete	Cu. Yd.	189.1	197.3	202.5	247.9
004-03.02	Steel Bar Reinforcement	Lbs.	30,910	32,960	34,170	39,540
204-02.01	Dry Excavation	Cu. Yd.	175	—	—	—
204-03.01	Wet Excavation	Cu. Yd.	315	550	550	690
006-09.01	Test Piles (Precast Conc. Size 1)	Lin. Ft.	40	30	30	30
006-09.03	Precast Conc. Piles Size 1	Lin. Ft.	1240	930	930	1170

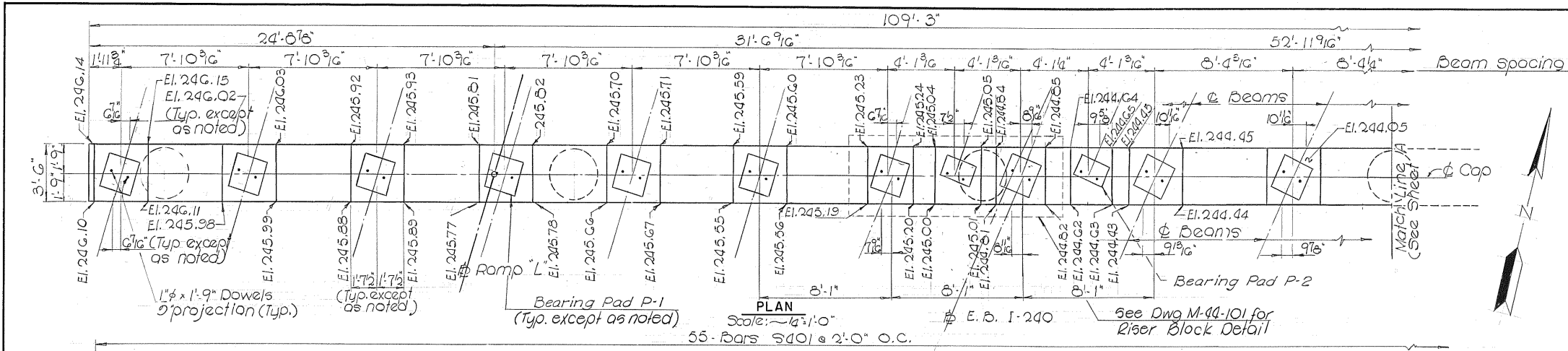
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER
PIERS C THRU F - BILL OF STEEL
STA. 242 + 50.00
SHELBY COUNTY

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE
DESIGNED BY F. Hoffman DATE Oct. '75
DRAWN BY M. Gentry
SUPERVISED BY D. McCortie
CHECKED BY D. McCortie

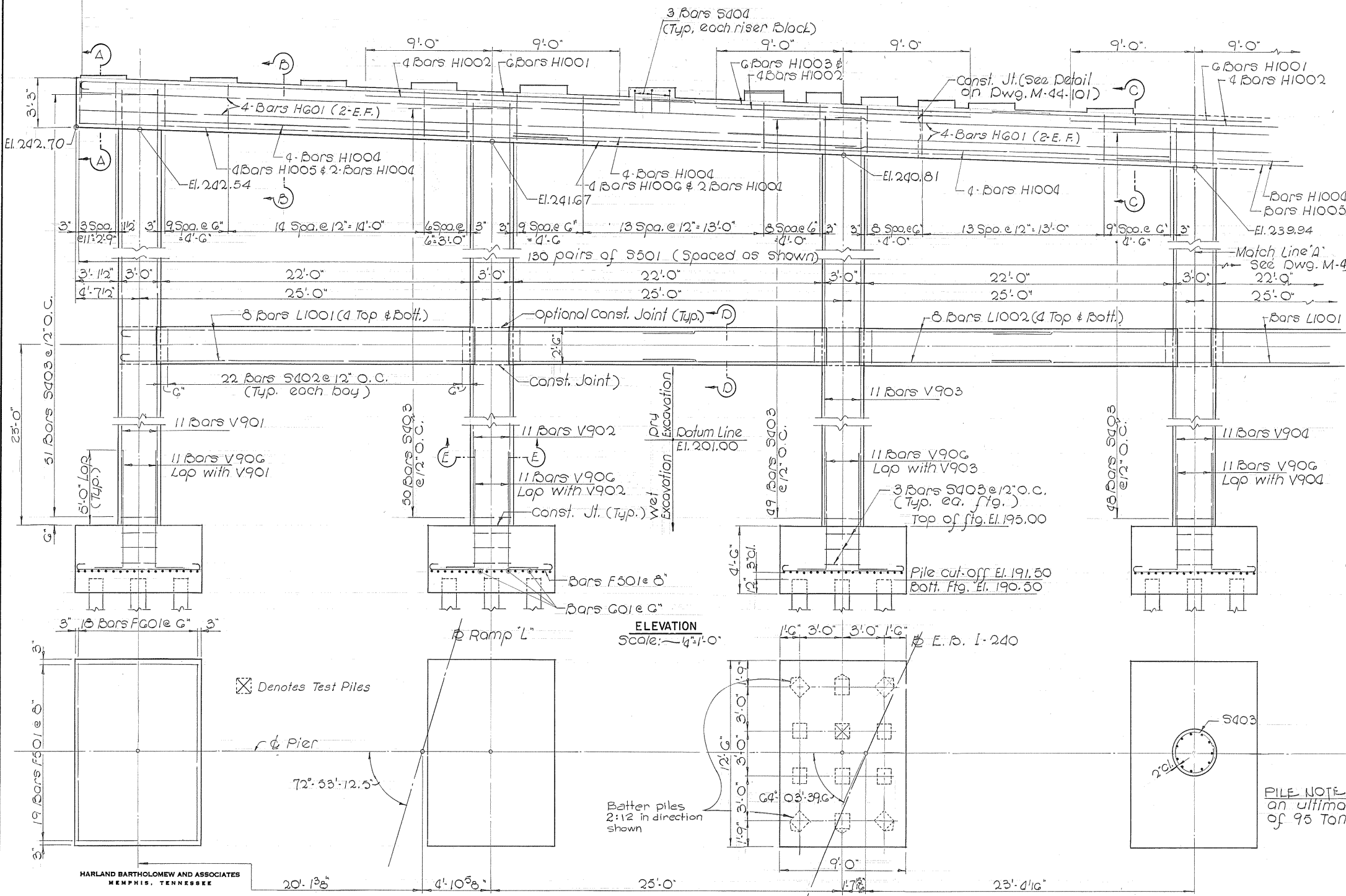
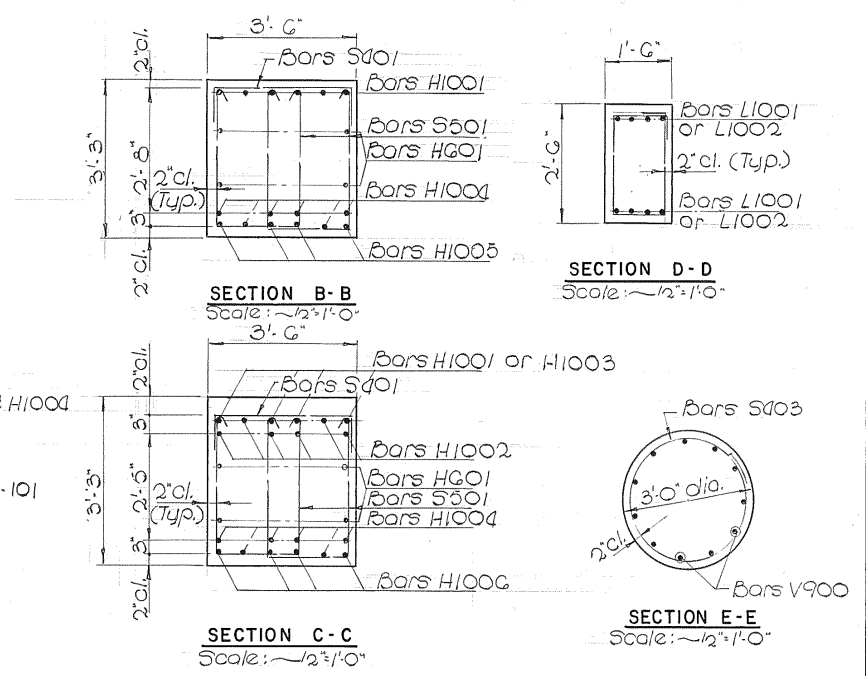
CORRECT
ENGINEER OF STRUCTURES
APPROVED
DIRECTOR OF HIGHWAYS

SCANNED

MICROFILMED



CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EACI-240-1(133)9	1975	34
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



NOTE: Four Columns 2' above elevations shown. Column reinforcement shall extend a minimum of 2'-11" into Pier cap

- NOTES:
- Notes 1 thru 4 on Dwg. M-44-95 apply to this sheet.
 - Minimum Bar Laps shall be as follows:
H1000 = 3'-3"
L1000 = 3'-3"
H900 = 1'-11"
V900 = 3'-0"
 - See Dwg. M-44-101 for Bill of Steel and Estimated Quantities.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

PIER G
STA. 242 + 50.00

SHELBY COUNTY

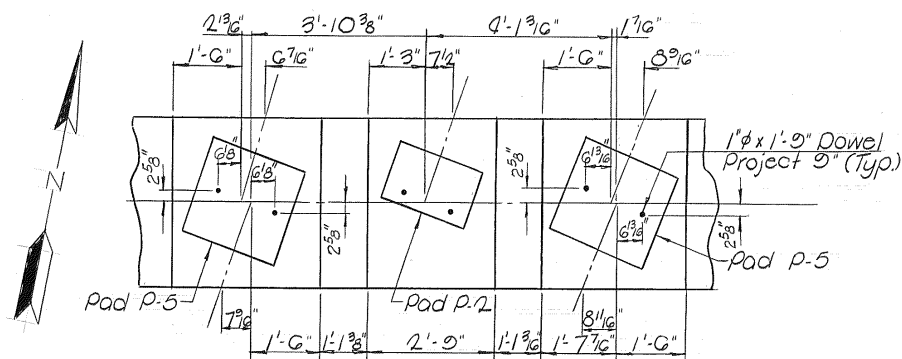
DESIGNED BY D. McCorkle DATE Sept. '75
DRAWN BY M. Garay DATE "
SUPERVISED BY D. McCorkle DATE "
CHECKED BY E. Hoffman DATE "

FOOTING PLAN
Scale: 1/4" = 1'-0"

CORRECT
ENGINEER OF STRUCTURES

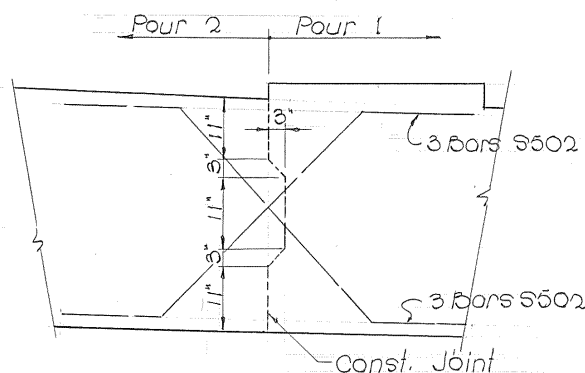
APPROVED
DIRECTOR OF HIGHWAYS

M-44-100

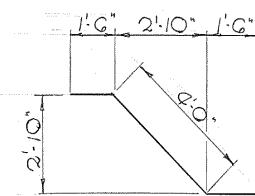


3. Minimum Bar Laps shall be as follows:
H1000 = 3'-3" L1000 = 3'-3" H600 = 1'-11"
V900 = 5'-0"

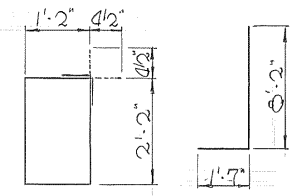
RISER BLOCK DETAIL
5c9/e ~ 1/2" = 1'-0"



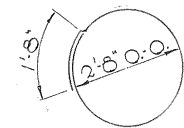
CONSTRUCTION JOINT - DETAIL
Scale: 3/4" = 1'-0"



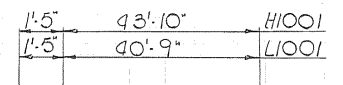
Bars S502



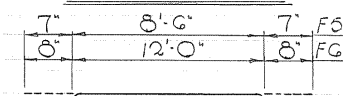
Bors 5402 Bors V900



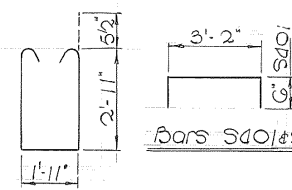
Bars S403


























Bars H1001 & L1001



Bars F501 & F601



Bars 5501

BILL OF STEEL			
Bar	No.	Length	Shape
H1001	12	45' 3"	
H1002	12	18' 0"	
H1003	6	28' 9"	
H1004	24	22' 0"	
H1005	8	34' 8"	
H1006	4	47' 0"	
H601	8	55' 5"	
V901	11	50' 6"	
V902	11	49' 8"	
V903	11	48' 10"	
V904	11	47' 11"	
V905	11	47' 1"	
V906	55	9' 9"	
L1001	16	42' 2"	
L1002	8	28' 9"	
S501	260	8' 8"	
S502	6	7' 0"	
S401	55	4' 2"	
S402	88	7' 5"	
S403	260	10' 0"	
S404	48	6' 2"	
F601	90	13' 4"	
F501	95	9' 8"	

ESTIMATED QUANTITIES			
Item	Description	Unit	Quantity
G04-03.01	Class A Concrete	Cu. Yd.	213.2
G04-03.02	Steel Bar Reinforcement	Lbs.	31470
204-02.01	Dry Excavation	Cu. Yd.	310
204-03.01	Wet Excavation	Cu. Yd.	365
G06-09.01	Test Piles (Precast Conc. Size 1)	Lin. Ft.	30
G06-09.03	Precast Conc. Piles Size 1)	Lin. Ft.	870

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

PIER G
STA. 242 + 50.00

SHELBY COUNTY

CORRECT _____
ENGINEER OF STRUCTURES

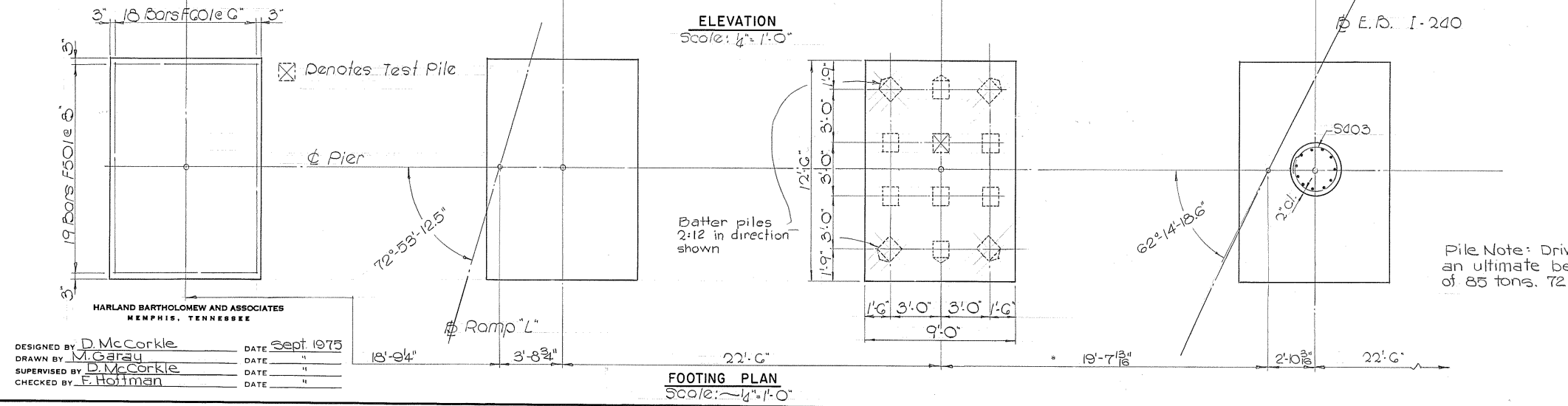
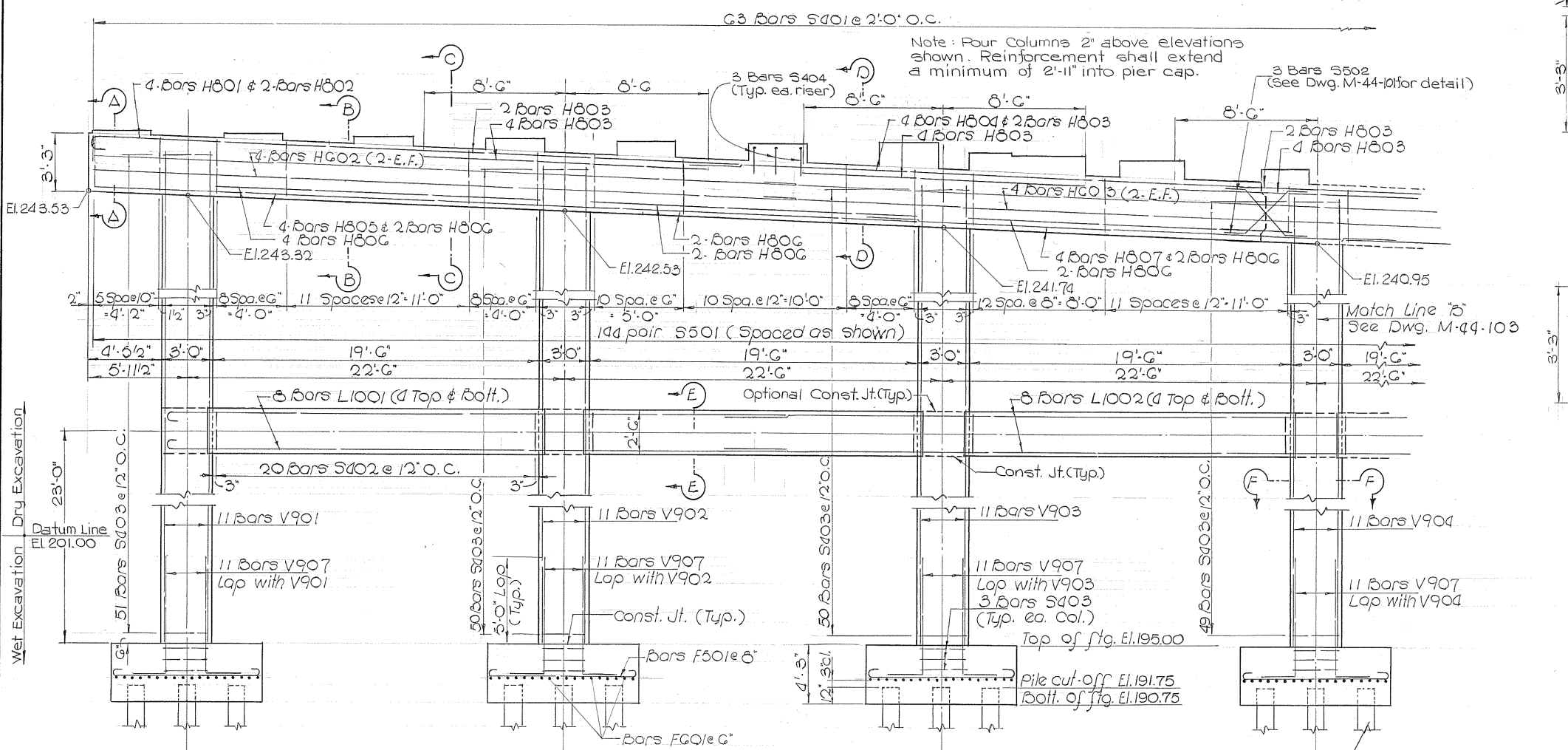
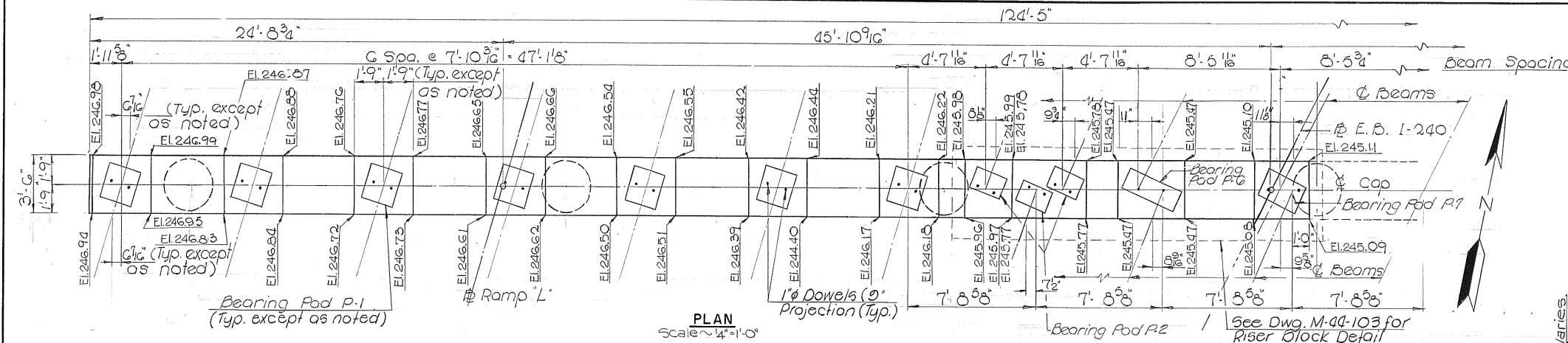
APPROVED _____
DIRECTOR OF HIGHWAYS

M-44-101

DESIGNED BY D. McCorkle DATE Oct. 175
DRAWN BY M. Gargy DATE "
SUPERVISED BY D. McCorkle DATE "
CHECKED BY F. Hoffman DATE "

FOOTING PLAN
Scale: $\frac{1}{4}'' = 1'-0''$

MICROFILMED

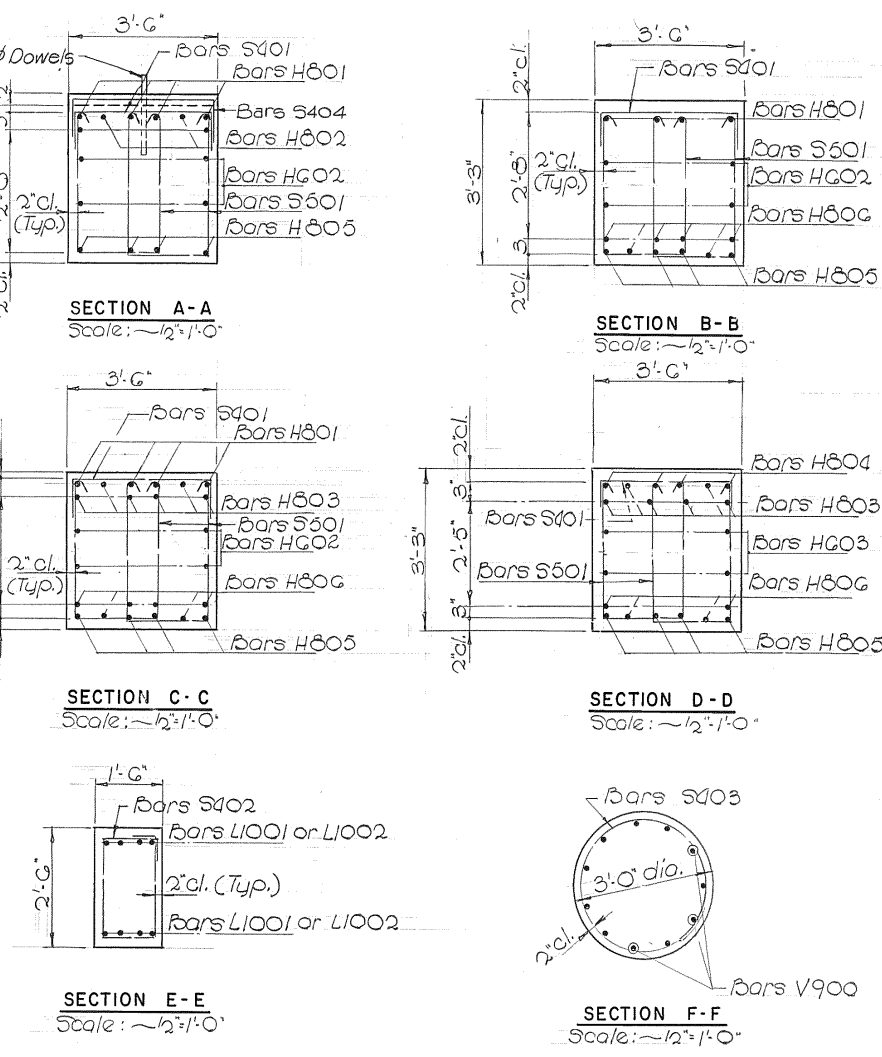


NOTES

- Notes 1 thru 4 on Dwg. M-44-95 shall apply to this sheet.
- See Dwg. M-44-103 for Bill of Steel & Estimated Quantities.
- Minimum Bar Laps shall be as follows:
H800 = 2'-0" H601 = 1'-11"
L1000 = 3'-3" V900 = 5'-0"
- See Dwg. M-44-129 for details of bearing pads.

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EAC1-240-1(133)9	1975	36

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

PIER H
STA. 242 + 50.00

SHELBY COUNTY

DESIGNED BY: D. McCorkle
DRAWN BY: M. Garay
SUPERVISED BY: D. McCorkle
CHECKED BY: F. Hoffman

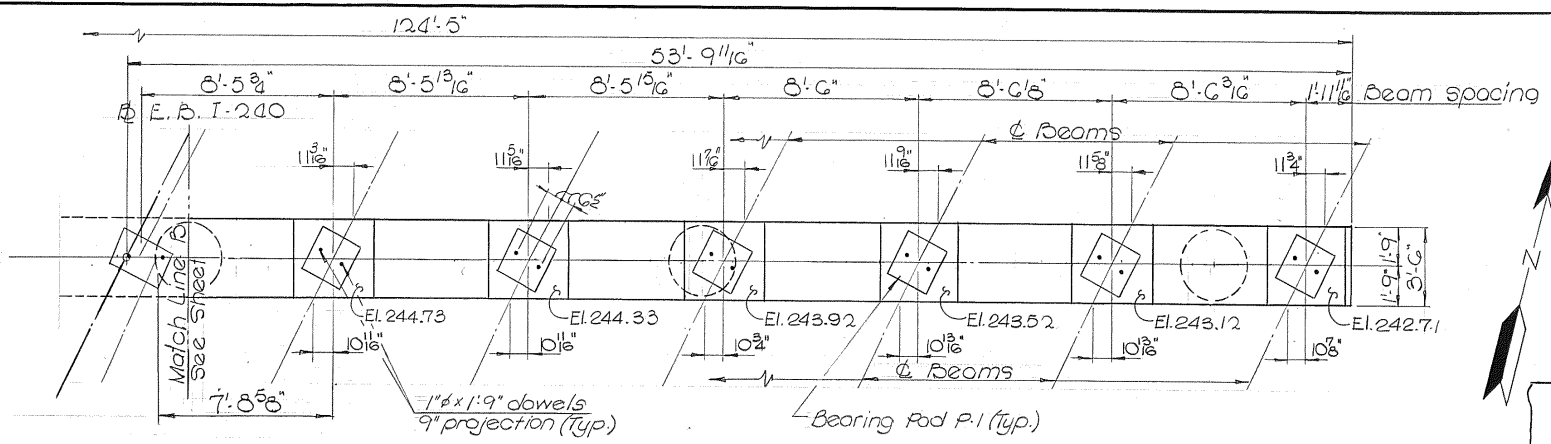
DATE: Sept 1975
DATE: "
DATE: "
DATE: "

ENGINEER OF STRUCTURES
DIRECTOR OF HIGHWAYS

M-44-102

SCANNED

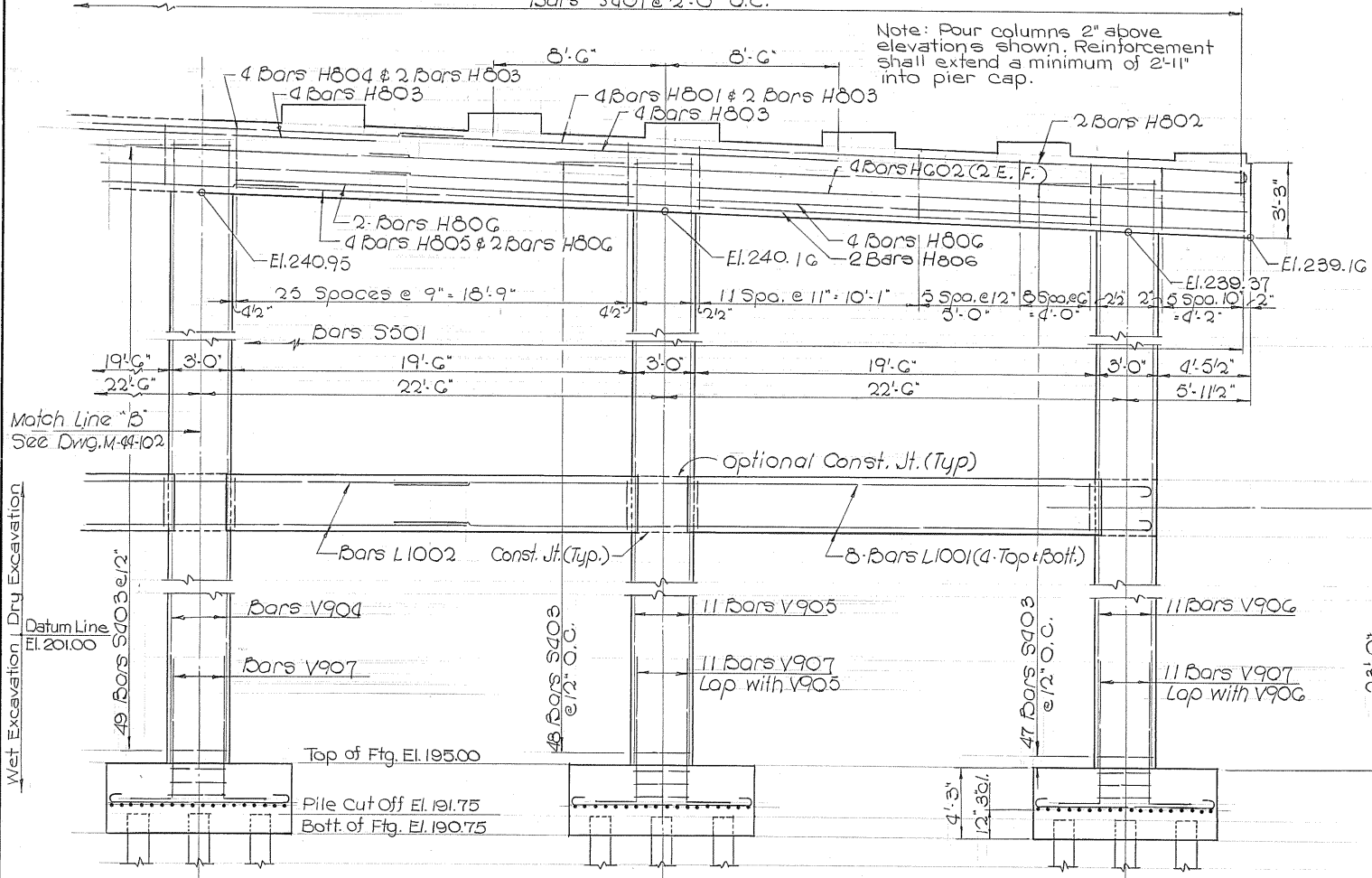
MICROFILMED



PLAN

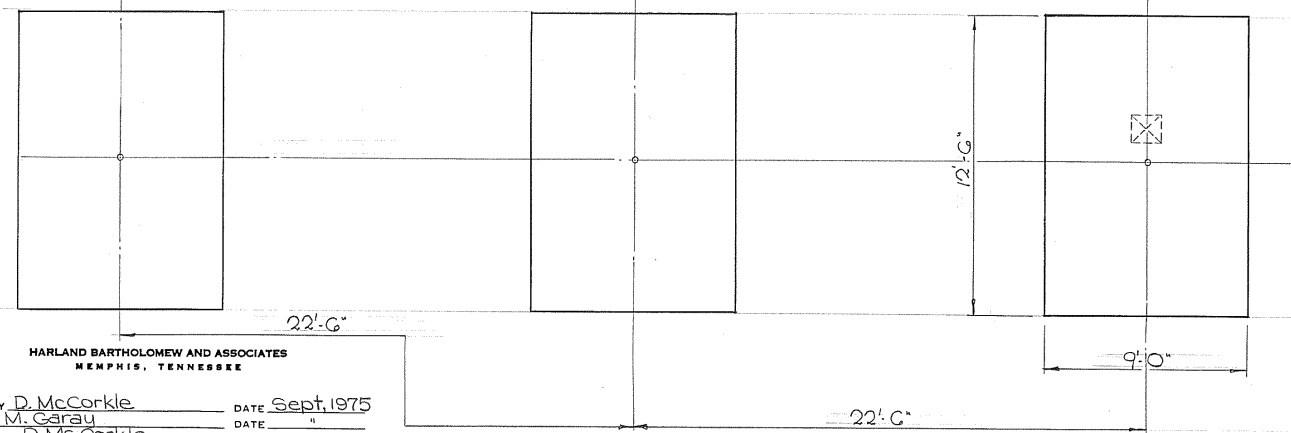
Bars S501 @ 2'-0" O.C.

Note: Pour columns 2" above elevation shown. Reinforcement shall extend a minimum of 2'-11" into pier cap.



ELEVATION

Scale: 1/4" = 1'-0"

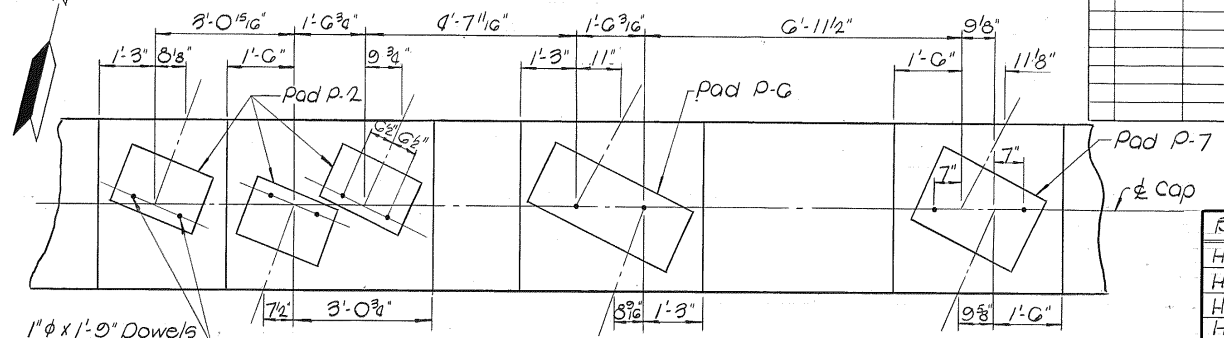


FOOTING PLAN

Scale: 1/4" = 1'-0"

NOTES

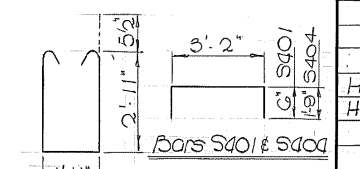
- Notes 1 thru 4 on Dwg. M-44-95 shall apply to this dwg.
- See Dwg. M-44-102 for all sections
- Minimum Bar Laps shall be as follows:
H800 = 2'-6" H801 = 1'-11"
L1000 = 3'-3" V900 = 5'-0"



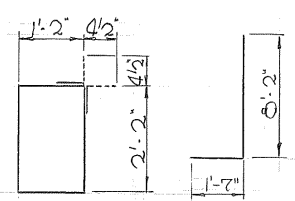
RISER BLOCK DETAIL

Scale: 1/2" = 1'-0"

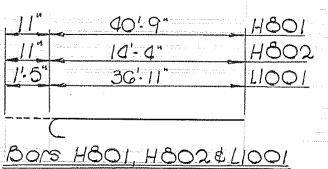
Note: See Dwg. M-44-129 for details of bearing pads.



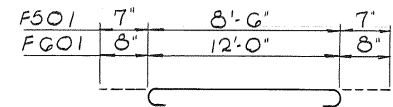
Bars S501



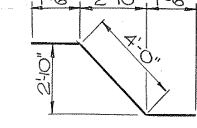
Bars S502



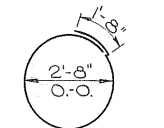
Bars S504



Bars F501 & F502



Bars S506



Bars S508

BILL OF STEEL

Bar	No.	Length	Shape
H801	8	41'-8"	C
H802	4	15'-3"	C
H803	24	17'-0"	C
H804	4	47'-6"	C
H805	8	49'-4"	C
H806	24	19'-6"	C
H807	4	30'-6"	C
H808	8	40'-6"	C
H809	4	46'-11"	C
V901	11	51'-4"	C
V902	11	50'-6"	C
V903	11	49'-9"	C
V904	11	49'-0"	C
V905	11	48'-2"	C
V906	11	47'-4"	C
V907	66	8'-9"	J
L1001	16	38'-4"	C
L1002	8	48'-8"	C
S501	258	8'-8"	U
S502	6	7'-0"	U
S503	63	4'-2"	U
S504	100	7'-5"	U
S505	295	10'-0"	O
S506	51	6'-8"	U
F501	108	13'-4"	C
F502	114	9'-8"	C

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

PIER H
STA. 242 + 50.00

SHELBY COUNTY

Item	Description	Unit	Quantity
604.03.01	Class A Concrete	Cu. Yd.	247.1
604.03.02	Steel Bar Reinforcement	Lbs.	32400
204.02.01	Dry Excavation	Cu. Yd.	125
204.03.01	Wet Excavation	Cu. Yd.	425
606.09.01	Test Piles (Precast Conc. Size 1)	Lin. Ft.	30
606.09.03	Precast Conc. Piles (Size 1)	Lin. Ft.	1050

CORRECT
ENGINEER OF STRUCTURES

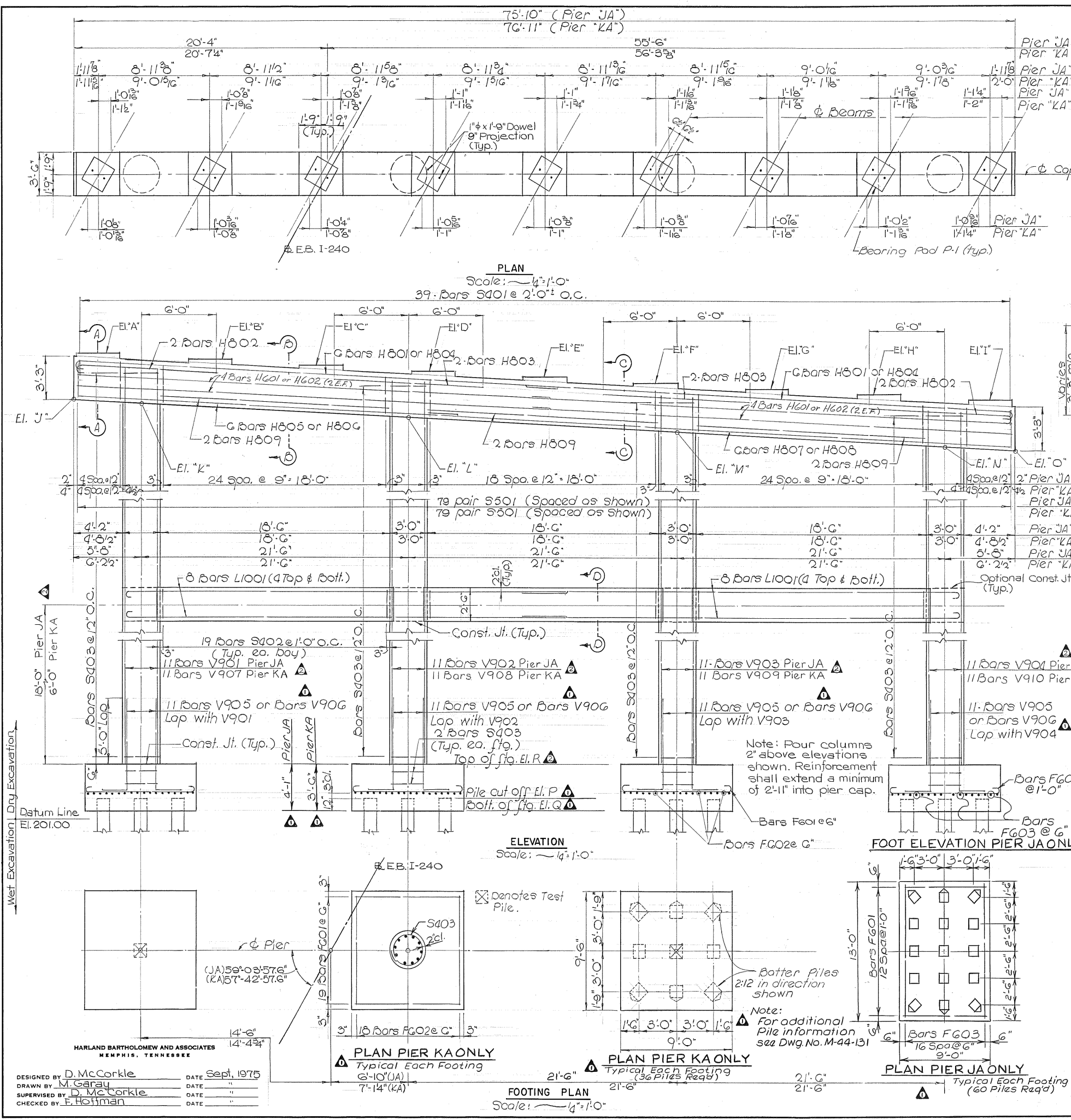
APPROVED
DIRECTOR OF HIGHWAYS

M-44-103

DESIGNED BY: D. McCorkle
DRAWN BY: M. Caray
SUPERVISED BY: D. McCorkle
CHECKED BY: F. Hoffman
DATE: Sept. 1975
DATE: "
DATE: "
DATE: "

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

MICROFILMED



NOTES:
1. Notes #1 thru #4 on Dwg. M-44-95 shall apply to this sheet.
2. Minimum Bar Laps shall be as follows:
H800 = 2'-6" V800 = 5'-0"
L1000 = 3'-3" H600 = 1'-11"

CONST. NO.		PROJECT NO.	YEAR	SHEET NO.
79007-3127-44		EACI-240-1(133)9	1975	40
REVISIONS				
NO.	DATE	BY	BRIEF DESCRIPTION	
1	8-24-76	C.E.H.	Revised Footing Pier JA & related changes.	
2	9-7-76	C.E.H.	Revised Footing Elev. Conc. Rein. Steel & Excavation Qts. Pier KA	

BILL OF STEEL				
Bar	No. Req'd.	Length	Shape	
H801	12	39'-10"	C	
H802	4	12'-11"	C	
H803	4	12'-0"	C	
H804	12	40'-8"	C	
H805	6	31'-0"	C	
H806	6	31'-6"	C	
H807	6	47'-0"	C	
H808	6	47'-6"	C	
H809	6	18'-6"	C	
H601	8	38'-9"	C	
H602	8	39'-3"	C	
V901	11	40'-6"	C	
V902	11	39'-6"	C	
V903	11	38'-5"	C	
V904	11	37'-5"	C	
V905	44	8'-10"	J	
V906	44	9'-5"	J	
V907	11	28'-6"	C	
V908	11	27'-6"	C	
V909	11	26'-5"	C	
V910	11	25'-5"	C	
S501	158	8'-8"	M	
F603	68	13'-10"	C	
S401	39	4'-2"	C	
S402	57	7'-5"	C	
S403	166	10'-0"	C	
F602	72	10'-4"	C	
L1001	16	36'-9"	C	
F601	52	9'-10"	C	

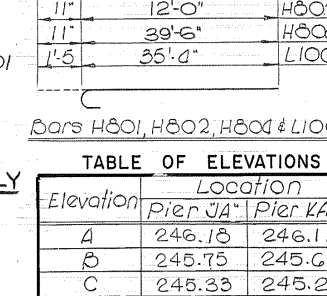
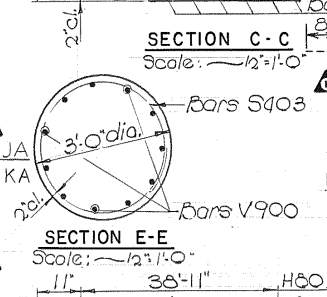
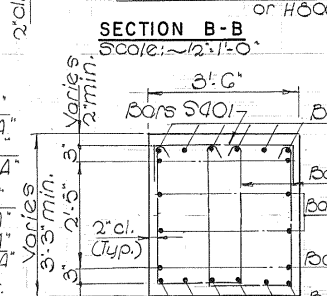
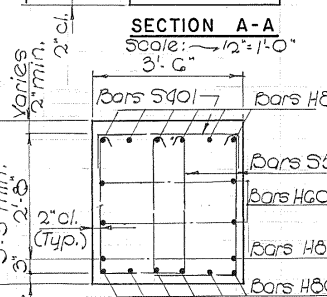


TABLE OF ELEVATIONS		
Elevation	Pier JA	Pier KA
A	246.18	246.11
B	245.75	245.67
C	245.33	245.24
D	244.90	244.81
E	244.47	244.37
F	244.05	243.94
G	243.62	243.50
H	243.19	243.06
I	242.76	242.63
J	242.33	242.20
K	241.90	241.77
L	241.47	241.34
M	241.04	240.91
N	239.61	239.48
O	239.18	239.05
P	201.92	214.50
Q	200.92	213.50
R	205.00	217.00

ESTIMATED QUANTITIES			
Item	Description	Unit	Quantities
G04-03.01	Class A Concrete	Cu.Yd.	143.8
G04-03.02	Steel Bar Reinforcement	Lbs.	18,438
204-02.01	Dry Excavation	Cu.Yd.	299
204-03.01	Wet Excavation	Cu.Yd.	400
G06-09.01	Test Piles (Precast Conc. Size 1)	Lin. Ft.	40
G06-09.03	Precast Conc. Piles (Size 1)	Lin. Ft.	1160

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 32 B
E.B. 1-240 OVER WOLF RIVER
PIERS JA AND KA
STA. 242 + 50.00

SHELBY COUNTY

CORRECT: ENGINEER OF STRUCTURES
APPROVED: DIRECTOR OF HIGHWAYS
M-44-106

DESIGNED BY D. McCorkle
DRAWN BY M. Garay
SUPERVISED BY D. McCorkle
CHECKED BY F. Hoffman
DATE Sept. 1975

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	6-16-74	ra4	Added Load Test on Pier JB
2	8-17-76	CEH	Changed Length of Test Pile Pier KB
3	9-9-76	CEH	Revised Footing E.I. Conc. & Steel, & Excavation Qts. Pier KB

NOTES:

- Notes #1 thru #4 on Dwg. M-44-95 shall apply to this dwg. also.
- Minimum Bar laps shall be as follows:
V900 = 5'-0"

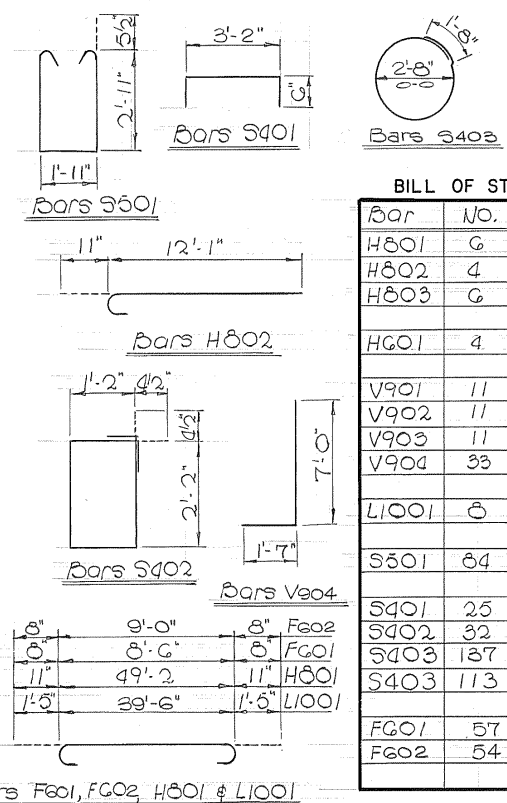
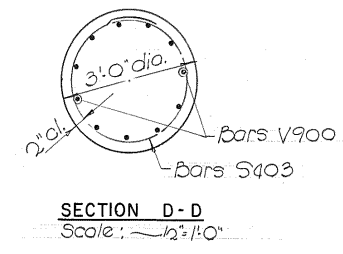
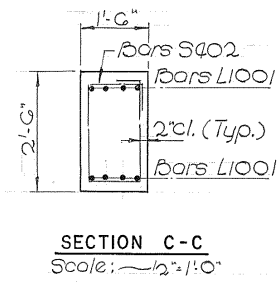
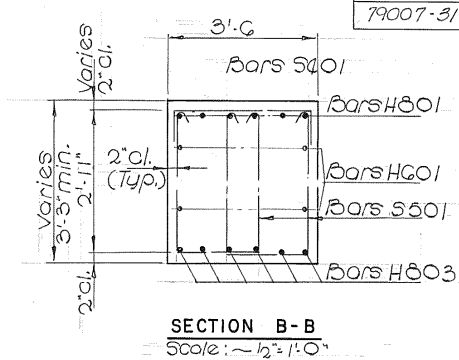
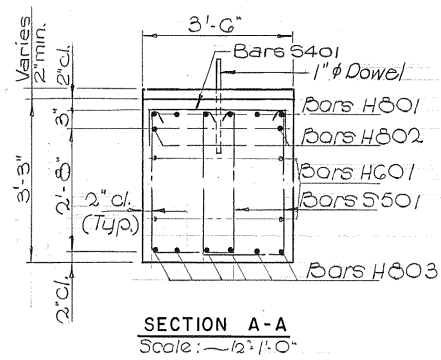
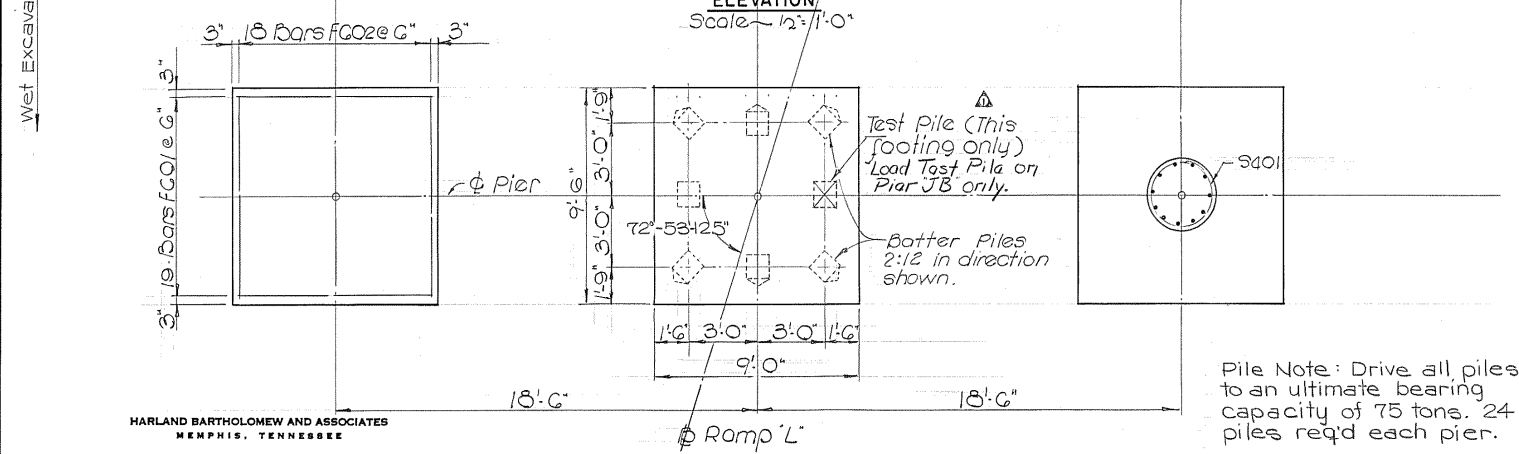
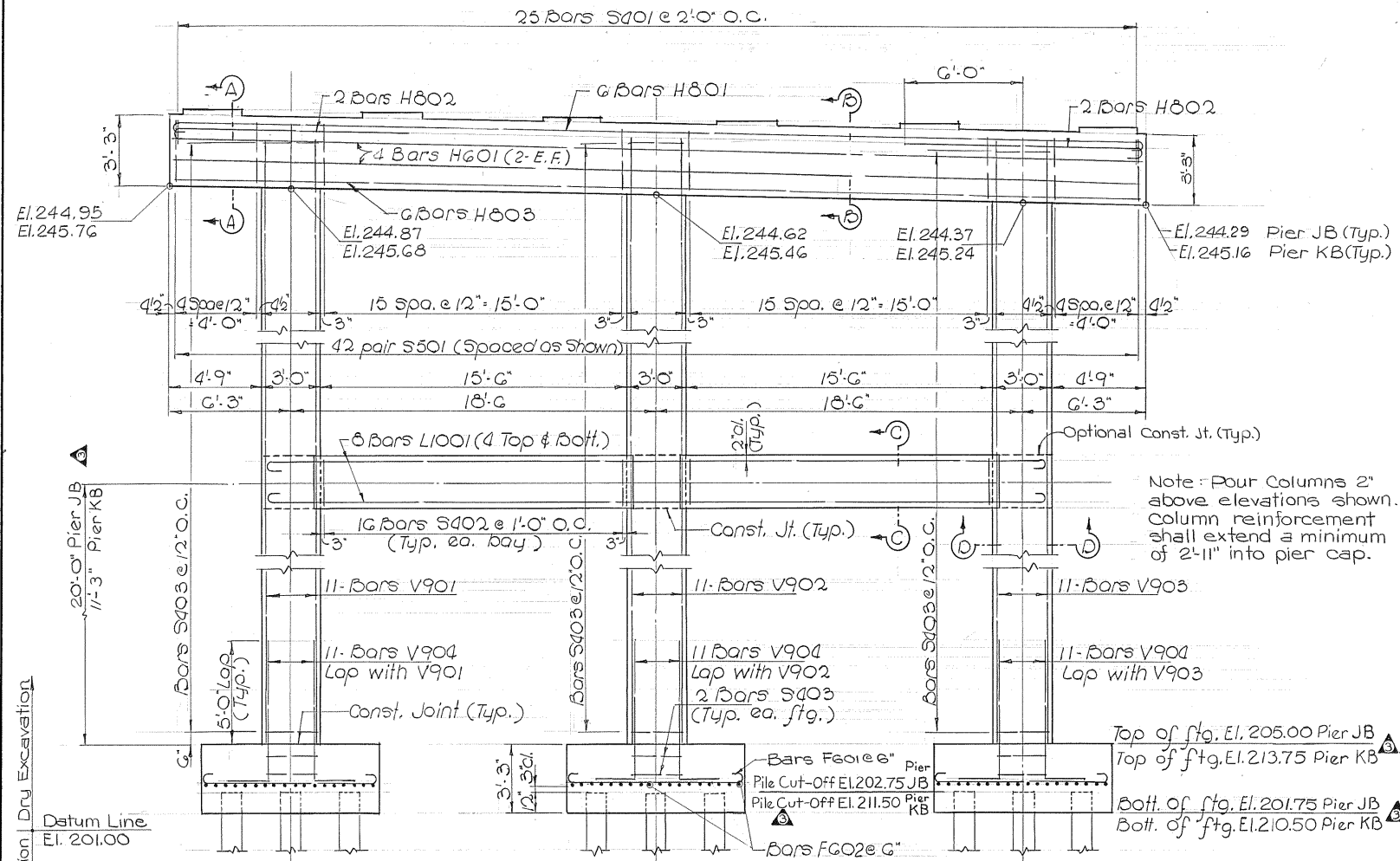
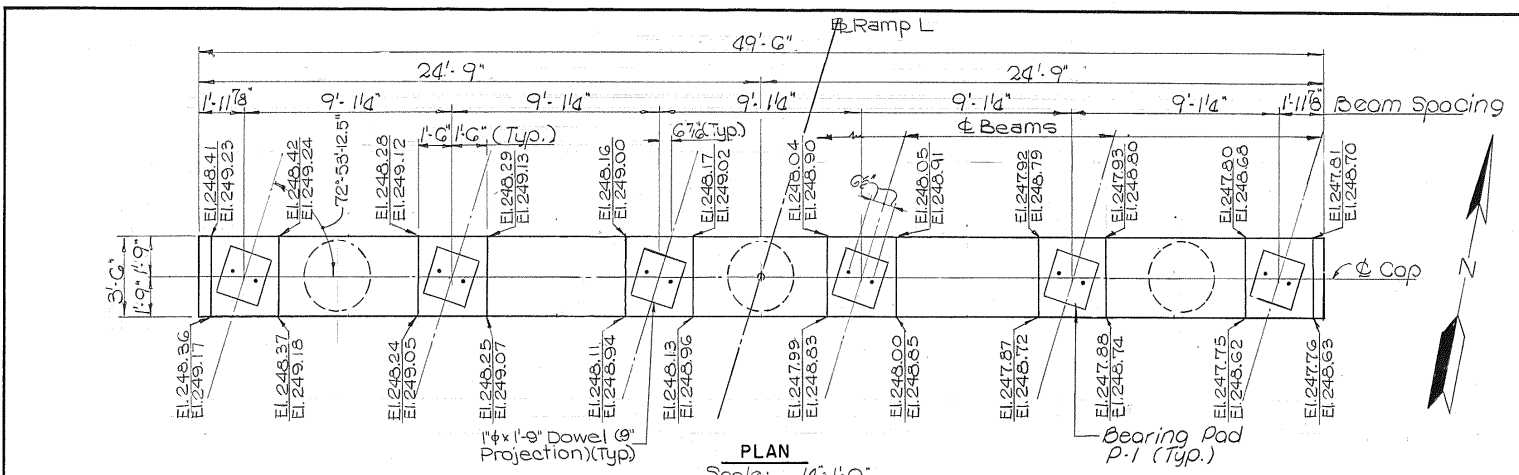


TABLE OF BAR LENGTHS

Bar	Pier JB	Pier KB
V901	42'-10"	34'-11"
V902	42'-7"	33'-7"
V903	42'-4"	34'-8"

BILL OF STEEL (ONE PIER)

Bar	No.	Length	Shape
H801	6	51'-0"	C
H802	4	13'-0"	C
H803	6	49'-2"	C
H804	4	49'-2"	C
V901	11	See Table	
V902	11	See Table	
V903	11	See Table	
V904	33	8'-7"	L
L1001	6	42'-4"	C
S501	64	8'-8"	M
S401	25	4'-2"	C
S402	32	7'-5"	C
S403	137	10'-0"	C
S404	113	10'-0"	C
F601	57	9'-10"	C
F602	54	10'-4"	C

ESTIMATED QUANTITIES

Item	Description	Unit	Pier JB	Pier KB
G04-03.01	Class A Concrete	Cu. Yd.	84.6	78.4
G04-03.02	Steel Bar Reinfor.	Lbs.	12820	11,748
204-02.01	Dry Excavation	Cu. Yd.	165	300
204-03.01	Wet Excavation	Cu. Yd.		
G06-09.01	Test Piles (Precast Conc. Size 1)	Lin. Ft.	15	30
G06-09.03	Precast Conc. Piles Size 1	Lin. Ft.	390	390
G06-09.02	Loading Tests (Precast Conc. Size 1)	Ea.	1	

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. 1-240 OVER WOLF RIVER

PIERS JB AND KB
STA. 242 + 50.00

SHELBY COUNTY

DESIGNED BY: D. McCorkle
DRAWN BY: M. Garay
SUPERVISED BY: D. McCorkle
CHECKED BY: F. Hoffman

DATE: Sept. 1975

CORRECT: _____
ENGINEER OF STRUCTURES

APPROVED: _____
DIRECTOR OF HIGHWAYS

M-44-107

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EACI-240-11(23)9	1975	42

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	6-16-76	rat	Removed Load Test on Pile

NOTE:

1. See Dwg. M-44-94 for Wing Elevation, Bar Bends not shown & Notes.

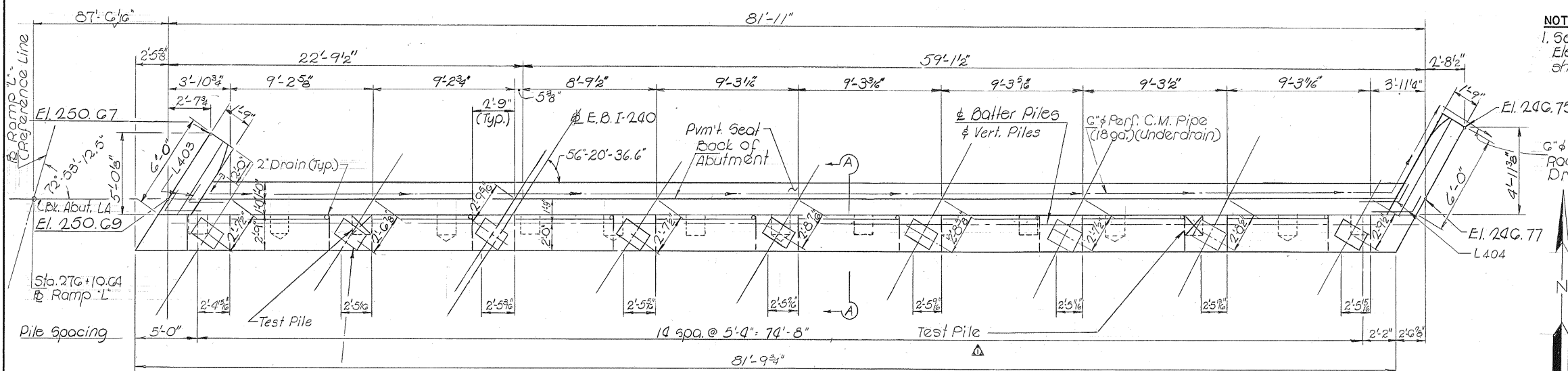
6" C.M. Pipe (18 ga.) (See Roadway plans for external Drainage)

BILL OF STEEL

Mark	No. Required	Length	Shape
H401	39	28'-1"	
H402	6	26'-11"	
H403	32	5'-8"	
H801	16	42'-4"	
V402	80	4'-10"	
V403	12	4'-2"	
V404	24	7'-7"	
V405	76	13'-4"	
V501	80	13'-4"	
C601	80	3'-3"	
C901	159	5'-4"	
L403	10	3'-0"	
L404	10	3'-0"	

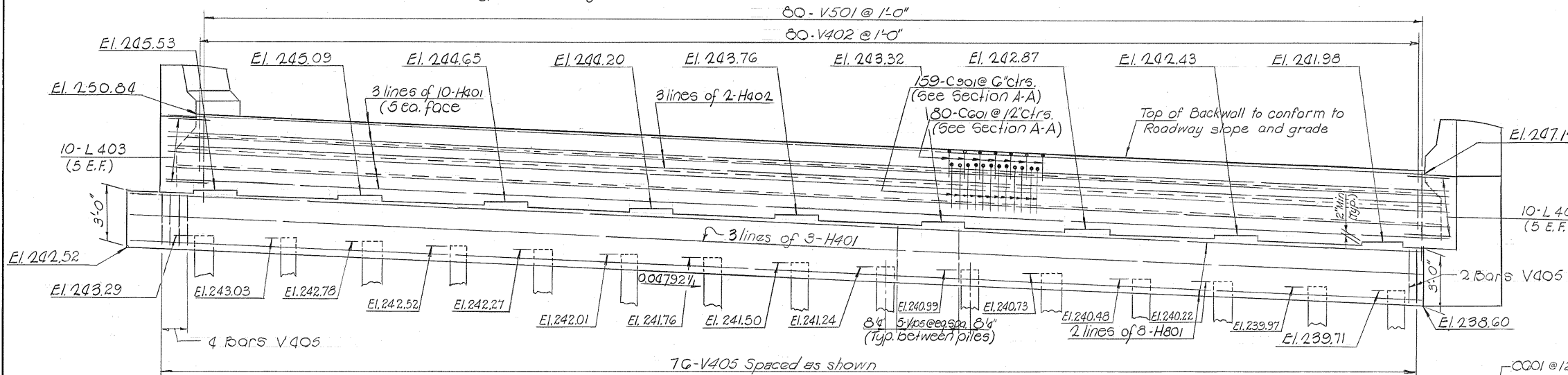
APPROACH SLAB

Item No.	Item	Unit	Quantity
A490	150	26'-10"	
A590	132	7'-6"	
A690	65	24'-0"	
A790	16	40'-2"	
A990	129	24'-0"	



Elastomeric Bearing Pad E-2
(5'8" x 16" x 1'-10") (Typ. each bearing)

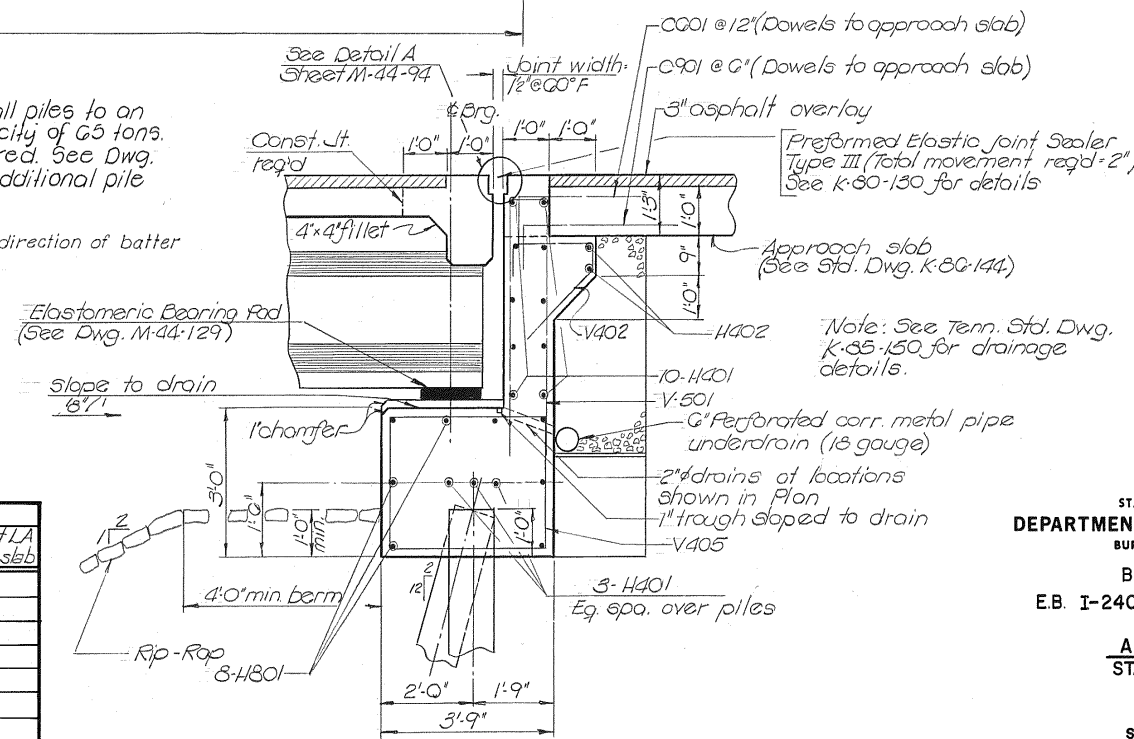
PLAN
Scale 1/4" = 1'-0"



ELEVATION
Scale 1/4" = 1'-0"

Note: Drive all piles to an ultimate capacity of 65 tons. 15 piles required. See Dwg. M-44-127 for additional pile data.

Denotes direction of batter



SECTION A-A
Scale: 1/2" = 1'-0"

SUMMARY OF ESTIMATED QUANTITIES

Item No.	Item	Unit	Quantity
204-02-01	Dry Excavation	Cu Yds.	85
604-03-01	Class A Concrete (Bridge)	Cu Yds.	54.7
604-03-02	Steel Bar Reinforcement (Bridge)	Lbs.	8260
606-09-01	Test Piles (Precast Concrete Size 1)	Lin. Ft.	90
606-09-03	Precast Concrete Piling - Size 1	Lin. Ft.	585
710-10	6" Perf. C.M.P. (18 Ga.)	Lin. Ft.	87
710-11	6" C.M.P. Underdrain (18 Ga.)	Lin. Ft.	4

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY D. McCorkle
DRAWN BY J. Millam
SUPERVISED BY D. McCorkle
CHECKED BY F. Hoffman

DATE Oct. '75
DATE
DATE
DATE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE 32B
E.B. I-240 OVER WOLF RIVER

ABUTMENT LA
STA. 242+50.00

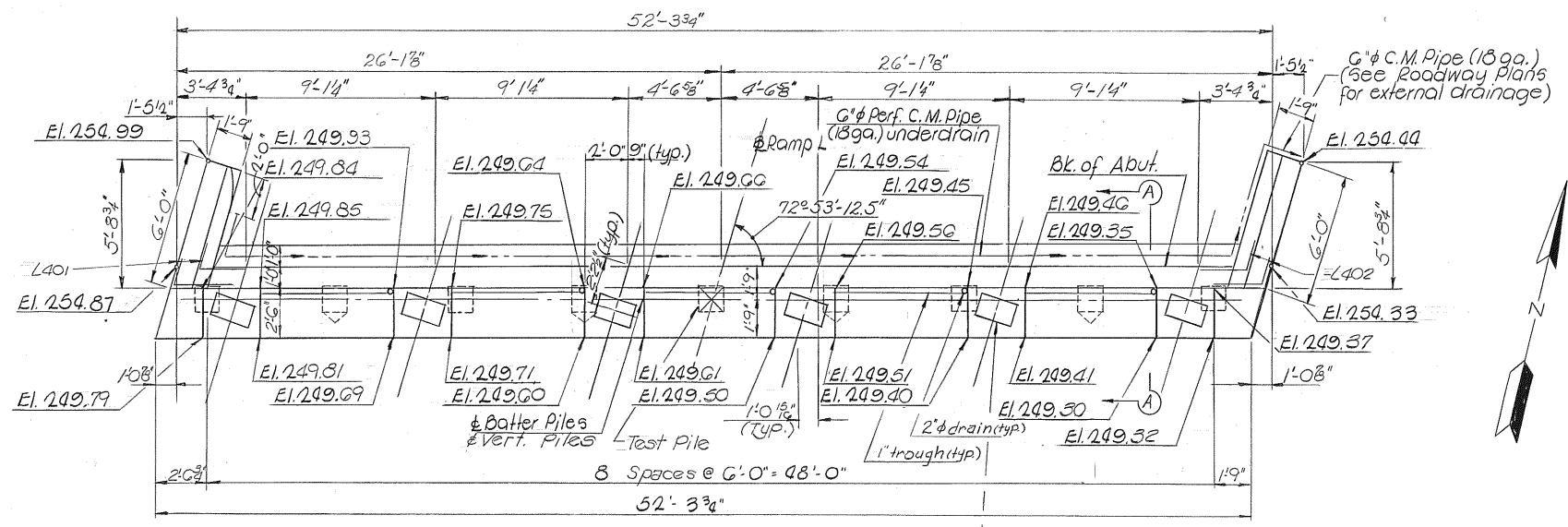
SHELBY COUNTY

CORRECT ENGINEER OF STRUCTURES

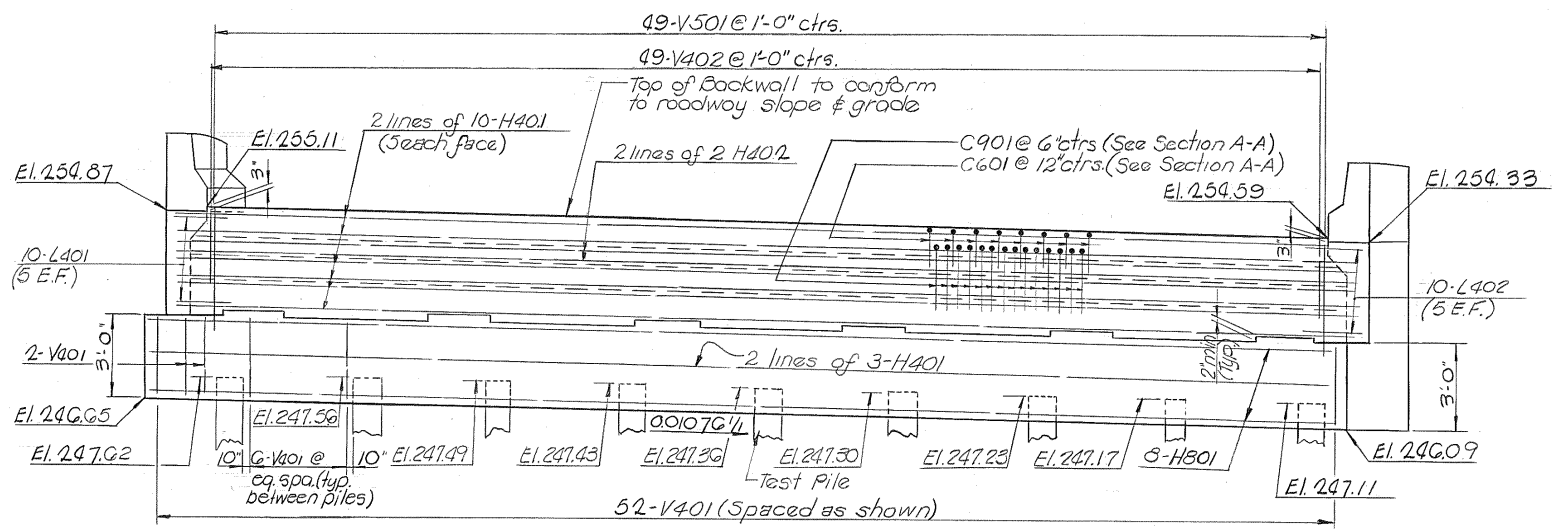
APPROVED DIRECTOR OF HIGHWAYS

M-44-108

NOT RECORDED



PLAN
Scale 1/4"=1'-0"
Elastomeric Bearing Pad E-2
(5'6" x 10' x 1'-10") (Typ. each bearing)



ELEVATION
Scale 1/4"=1'-0"
Note: Drive all piles to an ultimate capacity of 65 tons. 9 piles required. See Dwg. M-44-127 for additional pile data.
--> Denotes direction of batter.

SUMMARY OF ESTIMATED QUANTITIES

Item No.	Item	Unit	Quantities	
			Abut LB	Approach slab
204-02.01	Dry Excavation	Cu.Yds.	60	—
604-03.01	Class A Concrete (Bridge)	Cu.Yds.	36.4	49.0
604-03.02	Steel Bar Reinforcement (Bridge)	Lbs.	5,260	12,430
606-09.01	Test Piles (Precast Concrete Size 1)	Lin. ft.	50	—
606-09.03	Precast Concrete Piling - Size 1	Lin. ft.	400	60
710-10	6" Perf. C.M.P. (18 ga.) w/porous Backfill	Lin. ft.	58	—
710-11	6" C.M.P. Underdrain (18 Ga.)	Lin. ft.	4	—

- NOTES:
- Notes on Dwg M-44-94 apply to this dwg. also.
 - See Dwg. M-44-94 for Wing Elevation, Section A-A and Bar Bends.

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EACI-240-1(133)9	1975	43

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

BILL OF STEEL			
Mark	No. Required	Length	Shape
H401	26	26'-7"	—
H402	4	23'-0"	—
H403	32	5'-8"	—
H301	8	51'-9"	—
V401	52	12'-10"	□
V402	49	4'-10"	7
V403	12	4'-2"	7
V404	24	7'-2"	—
V501	49	13'-4"	□
C601	49	3'-9"	┐
C901	97	5'-4"	┐
L401	10	3'-0"	L
L402	10	3'-0"	L
APPROACH SLAB			
A490	100	24'-10"	—
A590	84	7'-6"	□
A690	47	24'-0"	—
A790	16	25'-4"	—
A990	93	24'-0"	—

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY F. Hoffman DATE 10-75
DRAWN BY J. Milam DATE 10-75
SUPERVISED BY D. McCorkle DATE 10-75
CHECKED BY D. McCorkle DATE 10-75

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE 32B
E.B. I-240 OVER WOLF RIVER
ABUTMENT LB
STA. 242+50.00
SHELBY COUNTY

CORRECT _____
ENGINEER OF STRUCTURES
APPROVED _____
DIRECTOR OF HIGHWAYS

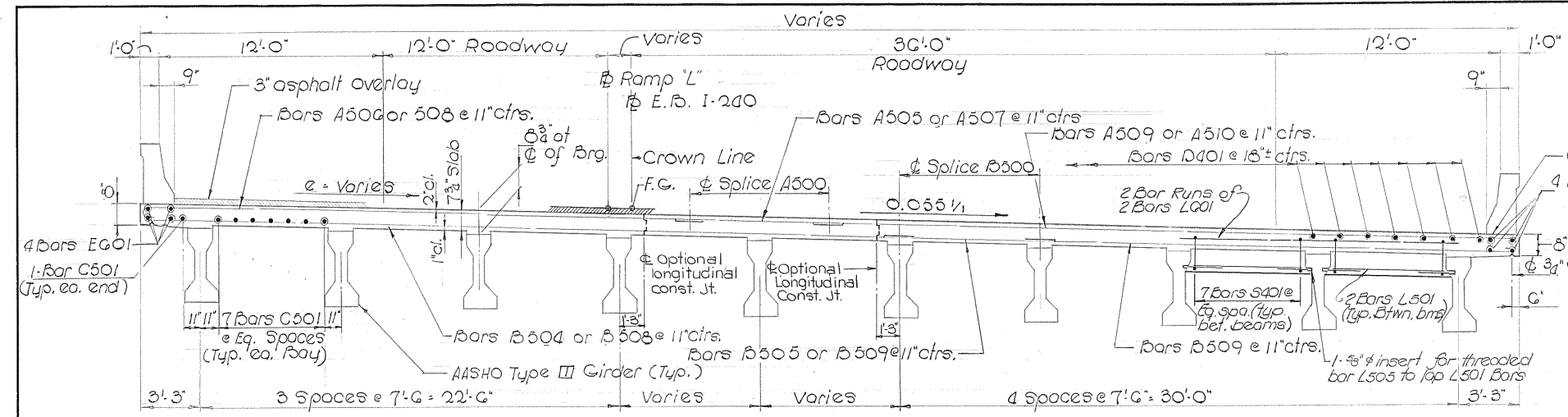
M-44-109

SCANNED

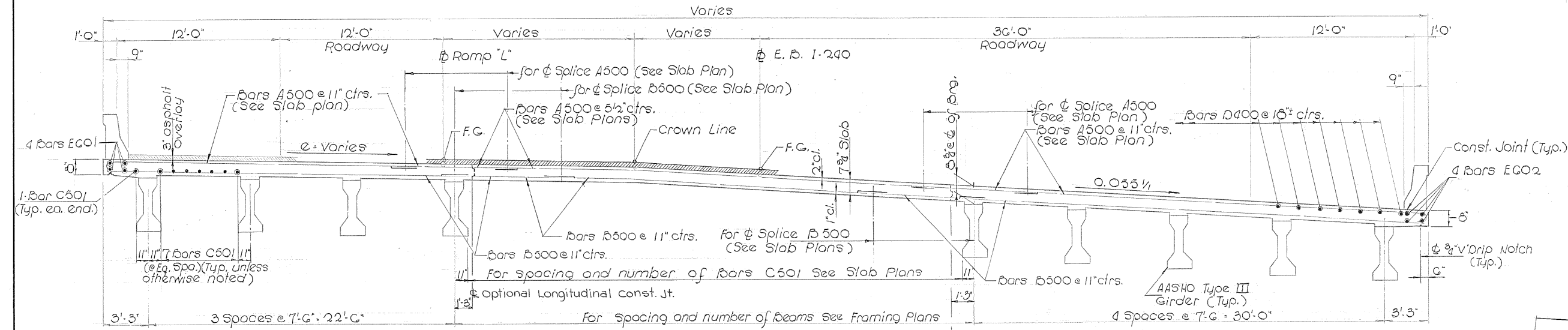
CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3/27-44	EAC1-240-1(133)A	1975	44

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

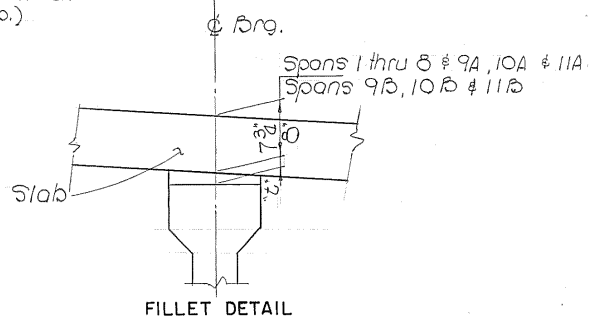
- NOTES:
- See Dwg. M-44-92 for General Notes and Design Stresses.
 - See Dwg. M-44-126 for screed Elevations.
 - See Dwg. M-44-111 for Bar Bends, Bill of Steel & Estimated Quantities
 - Notes a thru d on Dwg. M-44-112 apply to this Dwg.



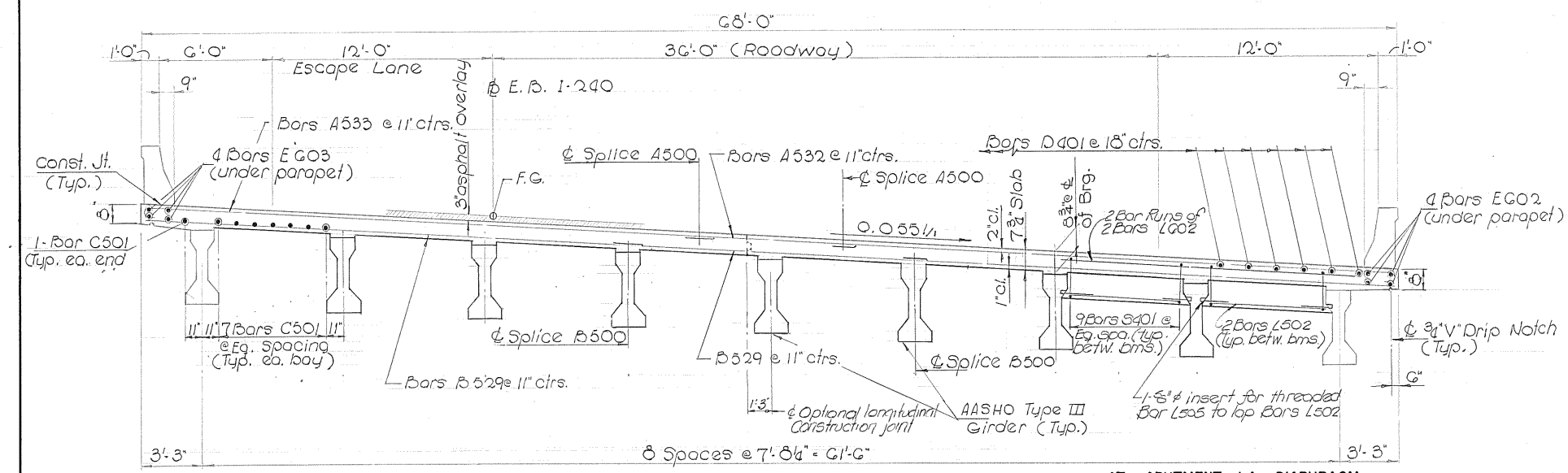
TYPICAL SECTION - SPANS 1 & 2
Scale: 1/4\"/>



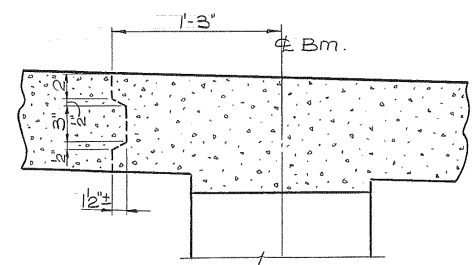
TYPICAL SECTION - SPANS 3 THRU 8
Scale: 1/4\"/>



Fillet Note: After the beams are in place, the Contractor shall field measure the top of beam elevations. These elevations subtracted from the finished surface elevations minus the slab thickness plus the algebraic dead load deflections equals the required fillet thickness "t".



TYPICAL SECTION - SPANS 9A THRU 11A
Scale: 1/4\"/>



OPTIONAL LONGITUDINAL CONSTRUCTION
JOINT IN SLAB
Scale: 1/2\"/>

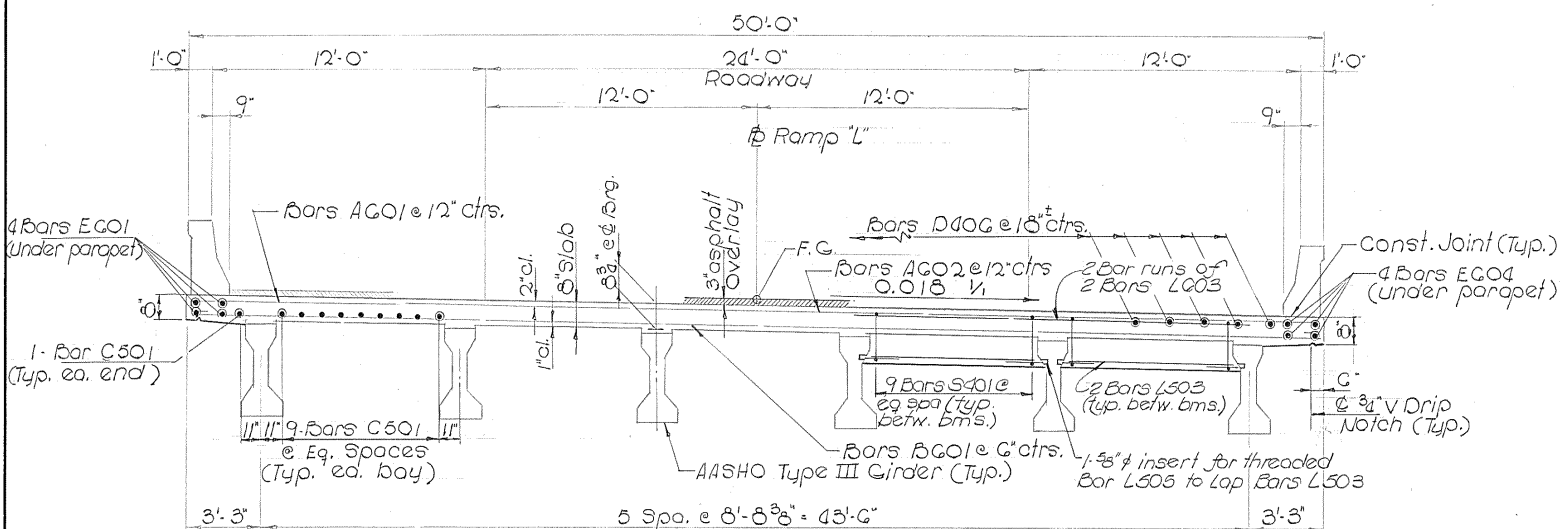
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER
TYPICAL SECTIONS - SPANS 1 THRU 11A
STA. 242 + 50.00
SHELBY COUNTY

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE
DESIGNED BY D. McCorkle DATE Sept. 1975
DRAWN BY M. Garay DATE Sept. 1975
SUPERVISED BY D. McCorkle DATE Sept. 1975
CHECKED BY F. Hoffman DATE Sept. 1975

CORRECT _____
ENGINEER OF STRUCTURES
APPROVED _____
DIRECTOR OF HIGHWAYS

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	9-20-77	JWK	Steel Quantities & Dimension

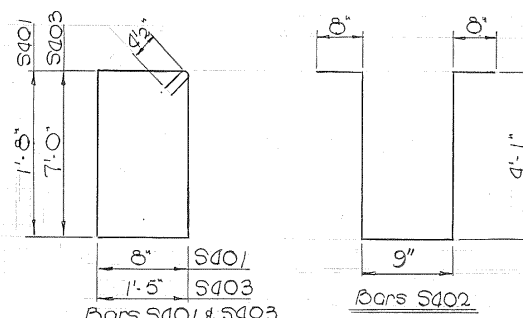
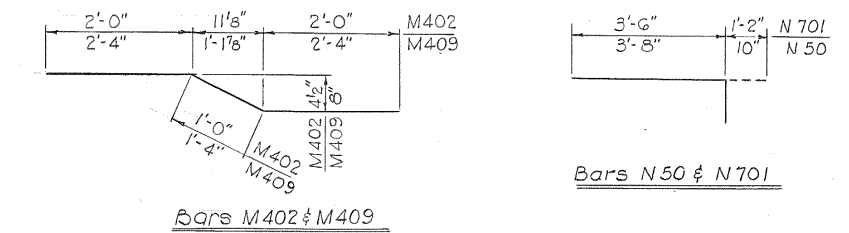
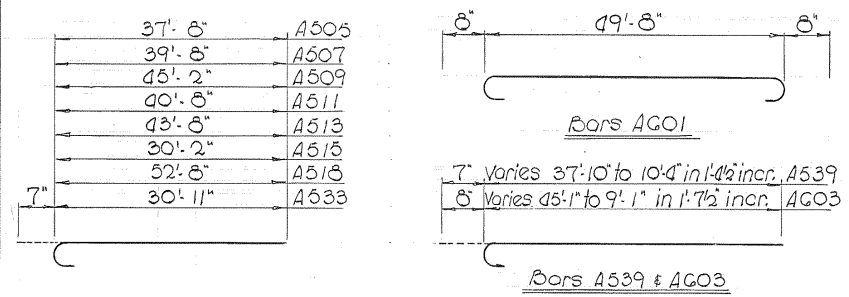
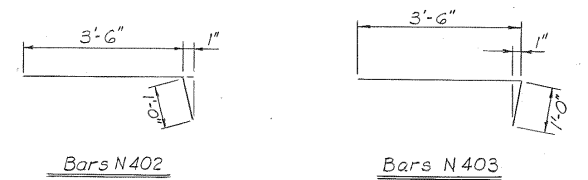
- NOTES:
- See Dwg. M-44-92 for General Notes & Design Stresses.
 - See Dwg. M-44-126 for Finished Slab Scribed Elevations.
 - Notes 3 thru 8 on Dwg. M-44-112 apply to this Dwg.



TYPICAL SECTION - SPANS 9B THRU 11B
Scale: 1/4" = 1'-0"

BILL OF STEEL											
Bar	No.	Length	Shape	Bar	No.	Length	Shape	Bar	No.	Length	Shape
AGO1	146	51'-0"	C	A537	1-Set	Varies					
AGO2	145	49'-8"		A538	1-Set	Varies					
AGO3	1-Set	Varies		A539	1-Set	Varies					
AGO4	1-Set	Varies									
AGO5	C	7'-6"									
A501	12	8'-0"									
A502	1-Set	Varies									
A503	1-Set	Varies									
A504	1-Set	Varies									
A505	89	38'-3"	C								
A506	88	30'-1"									
A507	81	40'-3"	C								
A508	81	32'-1"									
A509	340	45'-9"	C								
A510	697	37'-8"									
A511	81	41'-3"	C								
A512	82	33'-1"									
A513	77	44'-3"	C								
A514	76	36'-2"									
A515	609	30'-9"	C								
A516	251	22'-8"									
A517	82	46'-2"									
A518	81	53'-3"	C								
A519	130	25'-0"									
A520	99	29'-0"									
A521	84	33'-0"									
A522	72	37'-0"									
A523	67	41'-0"									
A524	60	45'-0"									
A525	56	33'-6"									
A526	52	37'-6"									
A527	46	41'-6"									
A528	46	45'-6"									
A529	44	34'-0"									
A530	42	37'-9"									
A531	61	43'-6"									
A532	408	38'-4"									
A533	408	31'-6"									
A534	1-Set	Varies									
A535	1-Set	Varies									
A536	1-Set	Varies									

SUPPLEMENTARY BILL OF STEEL		
Mark	No./Set	Description
AGO3	12	1 ea. 45'-9" to 10'-0" in 3'-3" incre. Δ
AGO4	12	1 ea. 46'-5" to 10'-6" in 3'-3" incre. Δ
A502	17	1 ea. 35'-8" to 8'-4" in 1'-8 1/2" incre.
A503	10	1 ea. 41'-11" to 11'-2" in 3'-5" incre.
A504	10	1 ea. 36'-3" to 5'-6" in 3'-5" incre.
A534	8	1 ea. 28'-2" to 14'-2" in 2'-0" incre.
A535	8	1 ea. 30'-0" to 23'-9" in 1'-9" incre.
A536	18	1 ea. 28'-5" to 5'-0 1/2" in 1'-4 1/2" incre.
A537	20	1 ea. 35'-1" to 8'-11 1/2" in 1'-4 1/2" incre. Δ
A538	20	1 ea. 37'-2" to 11'-0 1/2" in 1'-4 1/2" incre. Δ
A539	21	1 ea. 38'-5" to 10'-11" in 1'-4 1/2" incre.
BGO2	25	1 ea. 48'-0" to 9'-0" in 1'-7 1/2" incre.
B501	23	1 ea. 46'-10" to 9'-3" in 1'-8 1/2" incre.
B502	6	1 ea. 30'-9" to 13'-8" in 3'-5" incre.
B503	6	1 ea. 25'-0" to 7'-11" in 3'-5" incre.
B526	6	1 ea. 32'-6" to 20'-5" in 1'-9" incre.
B527	8	1 ea. 41'-0" to 23'-9" in 1'-9" incre.
B530	17	1 ea. 26'-5" to 4'-5" in 1'-4 1/2" incre.
B531	18	1 ea. 41'-2" to 17'-9 1/2" in 1'-4 1/2" incre.
B532	46	1 ea. 41'-7" to 10'-3" in 8" incre.



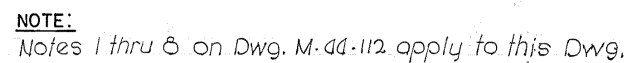
ESTIMATED QUANTITIES			
Item	Description	Unit	Quantity
G04-03.01	Class A Concrete	Cu.Yd.	2075.8
G04-03.02	Steel Bar Reinforcement	Lbs	595,759 Δ

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 32B
E.B. I-240 OVER WOLF RIVER
TYPICAL SECTION - SPANS 9B THRU 11B
STA. 242 + 50.00

SHELBY COUNTY

CORRECT ENGINEER OF STRUCTURES

APPROVED DIRECTOR OF HIGHWAYS

[illegible]

SHELBY COUNTY

DESIGNED BY D. McCortle DATE Sept. '75
DRAWN BY M. Garay DATE "
SUPERVISED BY D. McCortle DATE "
CHECKED BY F. Hoffman DATE "

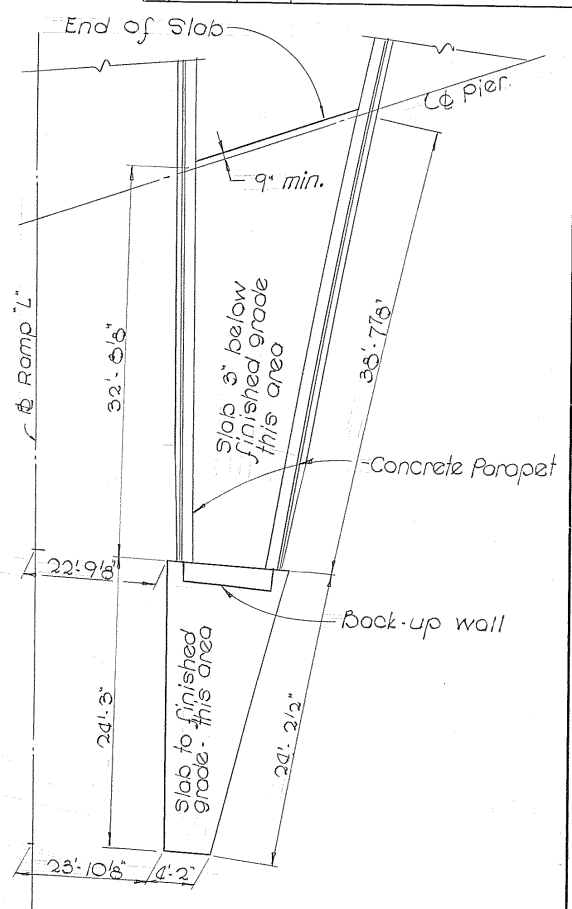
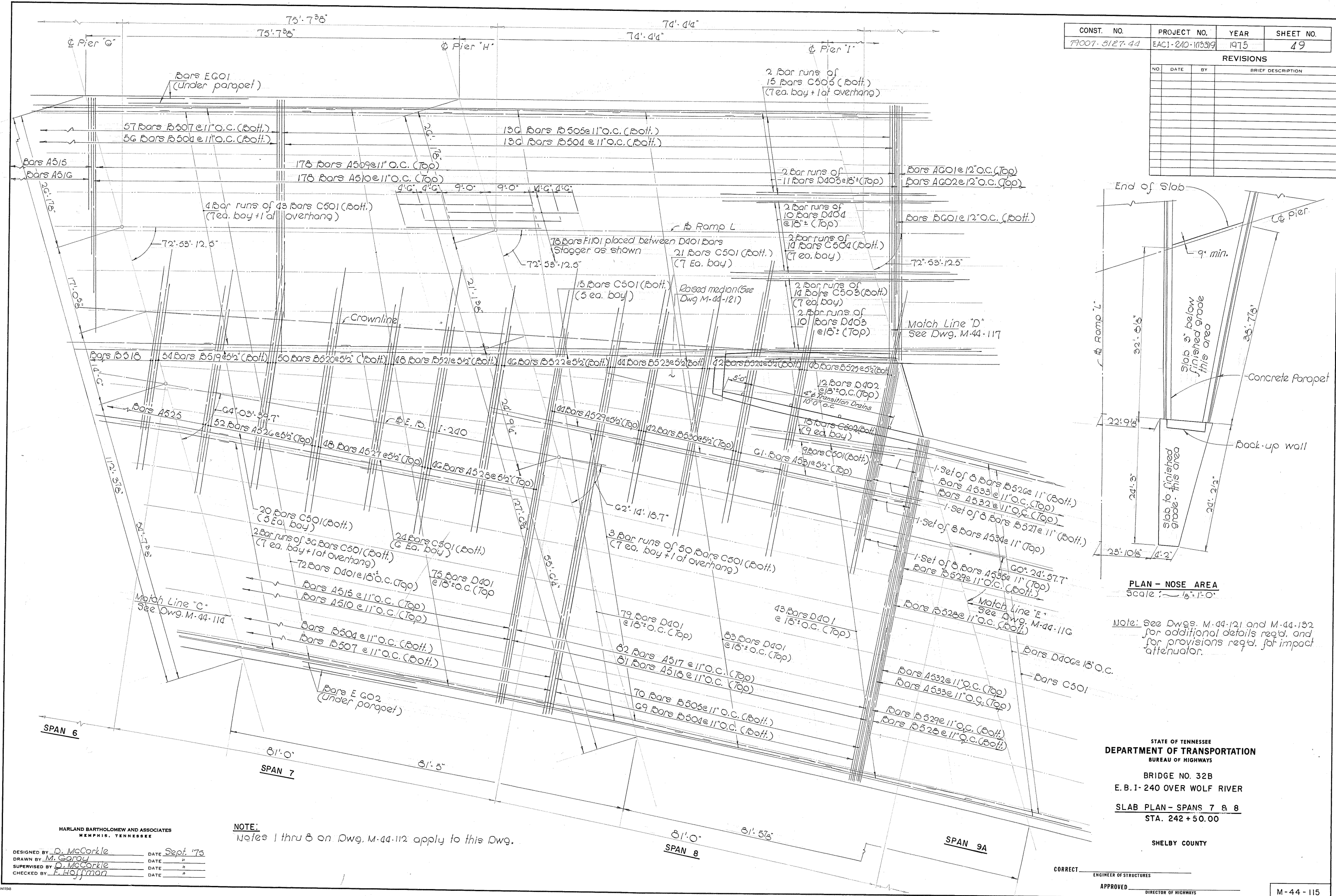
APPROVED _____
DIRECTOR OF HIGHWAYS

M-44-114

MICROFILMED

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EAC1-240-1(1133)9	1975	49

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



Note: See Dwg. M-44-121 and M-44-132 for additional details req'd. and for provisions req'd. for impact attenuator.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
 BRIDGE NO. 32B
 E. B. I-240 OVER WOLF RIVER
 SLAB PLAN - SPANS 7 & 8
 STA. 242+50.00
 SHELBY COUNTY

CORRECT _____ ENGINEER OF STRUCTURES
 APPROVED _____ DIRECTOR OF HIGHWAYS
 M-44-115

MICROFILMED

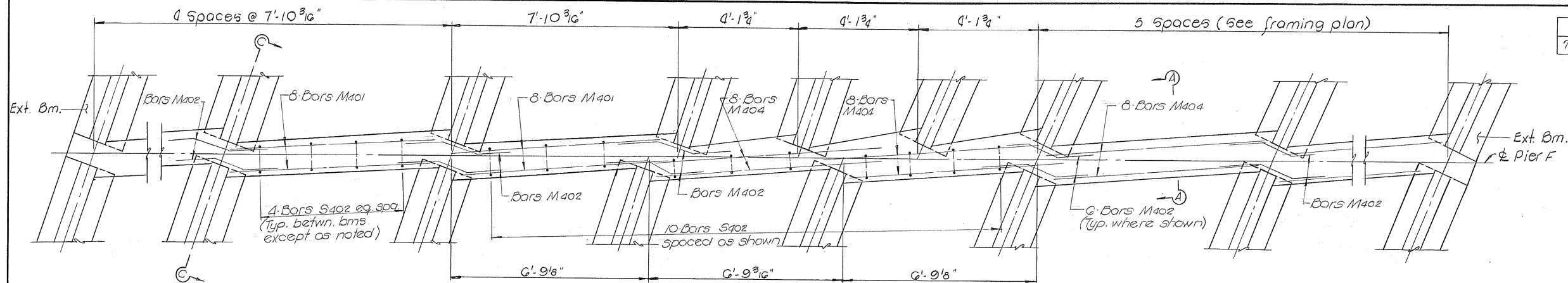
HARLAND BARTHOLOMEW AND ASSOCIATES
 MEMPHIS, TENNESSEE
 DESIGNED BY D. McCorkle
 DRAWN BY M. Garay
 SUPERVISED BY D. McCorkle
 CHECKED BY E. Hoffman
 DATE Sept. '75
 DATE " "
 DATE " "
 DATE " "

NOTE:
 Notes 1 thru 8 on Dwg. M-44-112 apply to this Dwg.

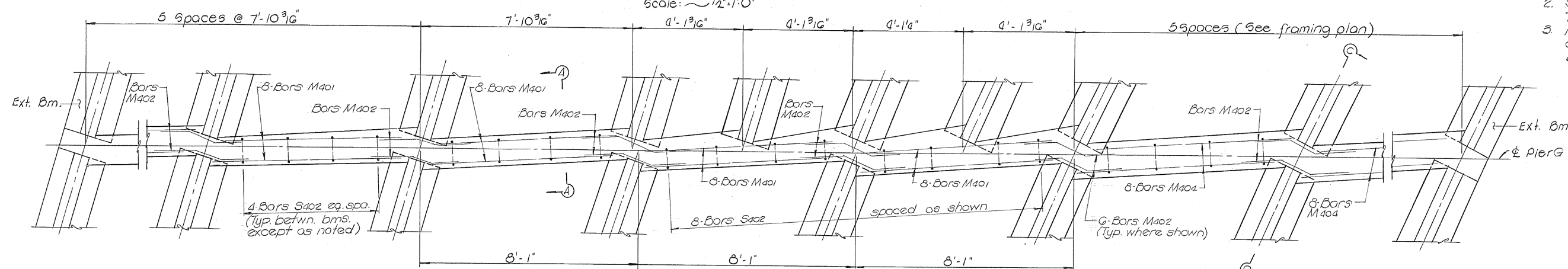
CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EACI-240-1(133)9	1975	53

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

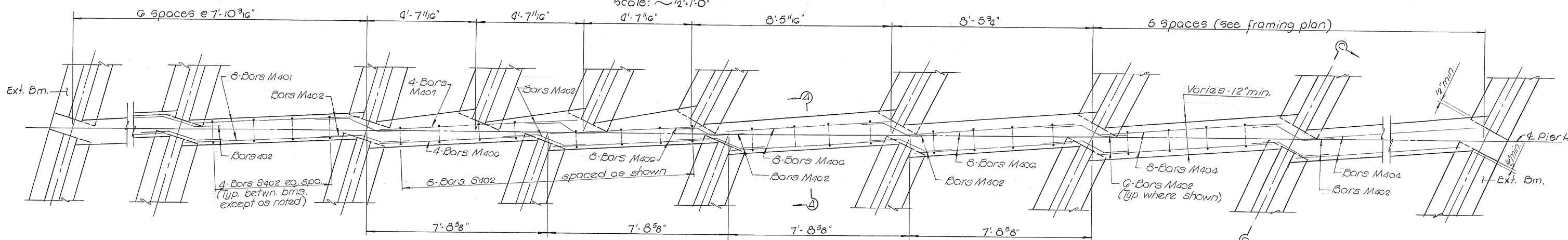
- NOTES:**
- See Dwg. M-44-111 for Bar Bends, Bill of Steel & Estimated Quantities.
 - See Dwg. M-44-110 & 111 for Typical Sections.
 - Reinforcing Steel in beams and diaphragms may be field bent to avoid conflicts.



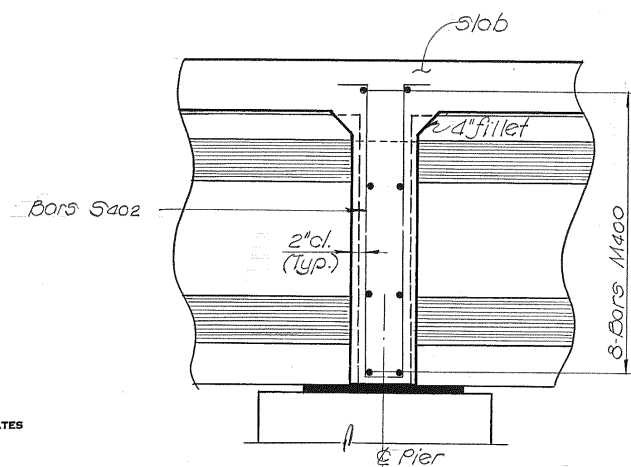
PLAN - PIER F
Scale: 1/2"=1'-0"



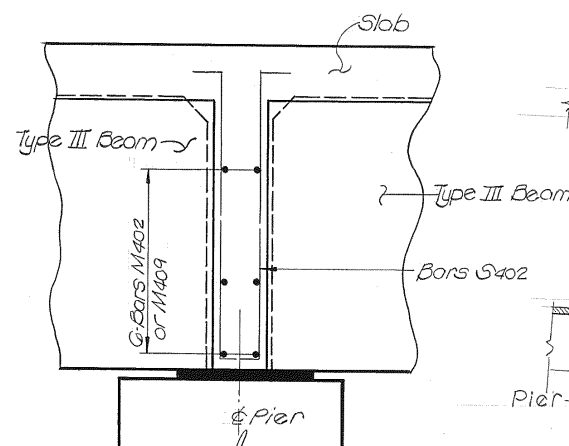
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Scale: 1/2"=1'-0"



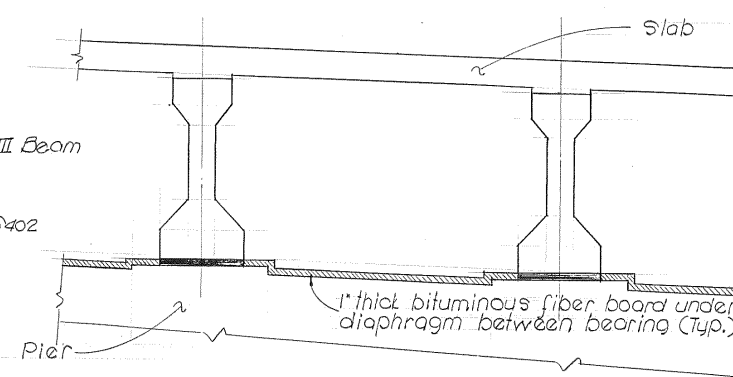
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Scale: 1/2"=1'-0"



SECTION A-A
Scale: 3/4"=1'-0"



SECTION C-C
Scale: 3/4"=1'-0"



TYPICAL PIER DETAIL
Scale: 1/2"=1'-0"

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E. B. I-240 OVER WOLF RIVER

DIAPHRAGM DETAILS - PIERS F, G & H
STA. 242 + 50.00

SHELBY COUNTY

CORRECT ENGINEER OF STRUCTURES

APPROVED DIRECTOR OF HIGHWAYS

M-44-119

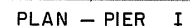
HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY: D. McCorkle DATE: 10-24-75
DRAWN BY: L. Smith DATE: 10-24-75
SUPERVISED BY: D. McCorkle DATE: 10-24-75
CHECKED BY: F. Hoffman DATE: 10-24-75

Varies - 12" min.
(except where noted)

[illegible]

1. See Dwg. M-44-111 for Bar Bends, Bill of Reinforcing and Estimated Quantities.
2. See Dwg. M-44-110 & 111 for Typical Sections.
3. Reinforcing Steel in beams and diaphragms may be field bent to avoid conflicts.
4. See Dwg. M-44-119 for Sections A-A & C-C.



Scale : $\frac{1}{2}'' = 1'-0''$



Scale : $1/2" = 1'-0"$



Scale : $12'' = 1'-0''$



Scale: $\frac{1}{2}'' = 1'-0''$

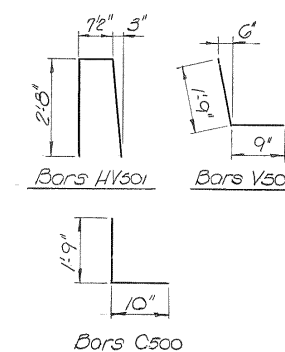
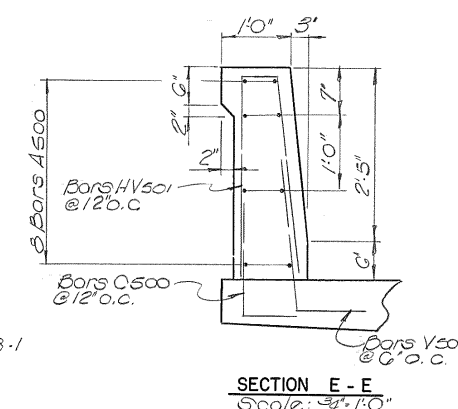
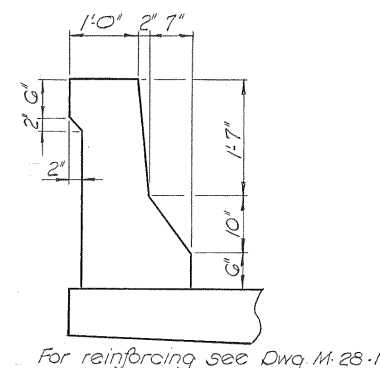
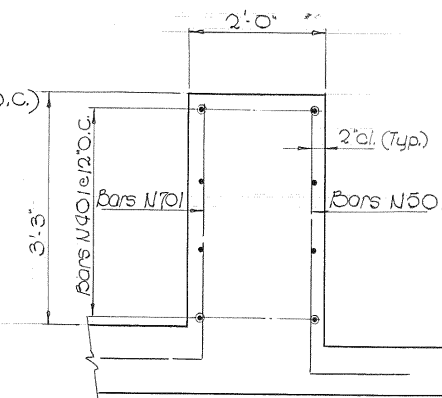
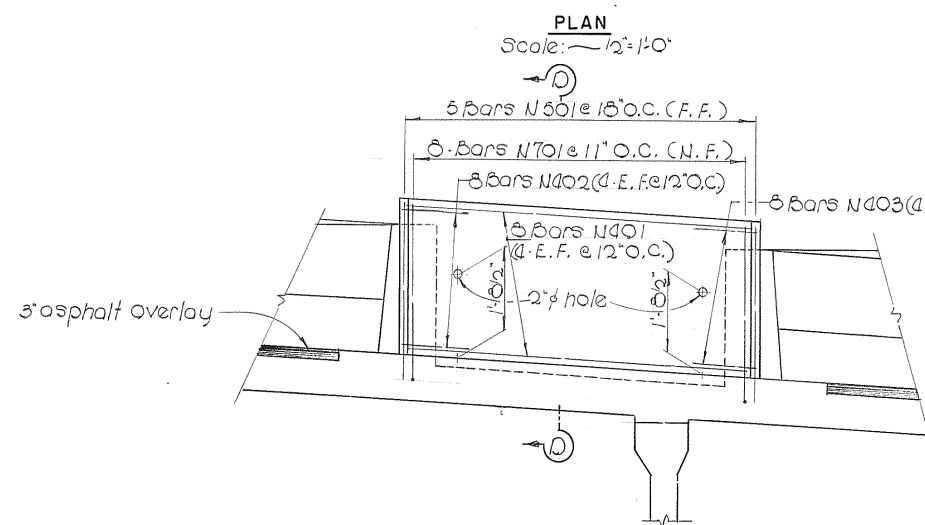
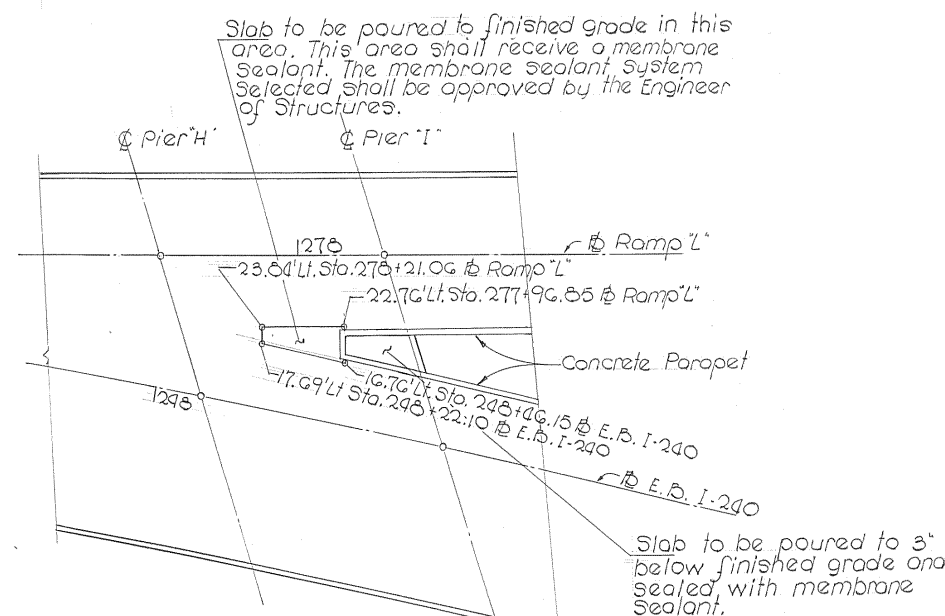
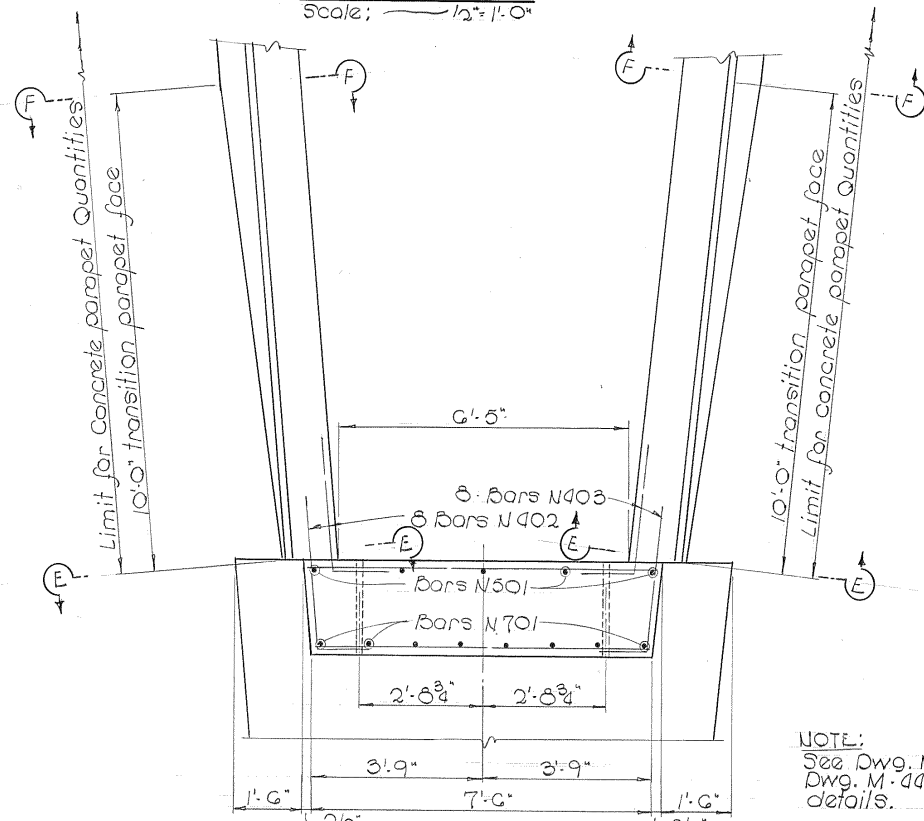
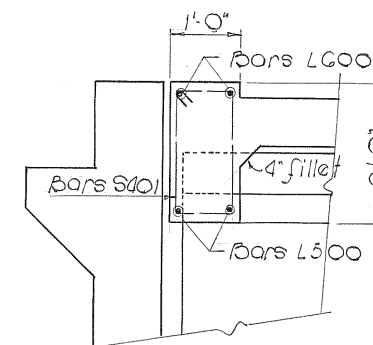
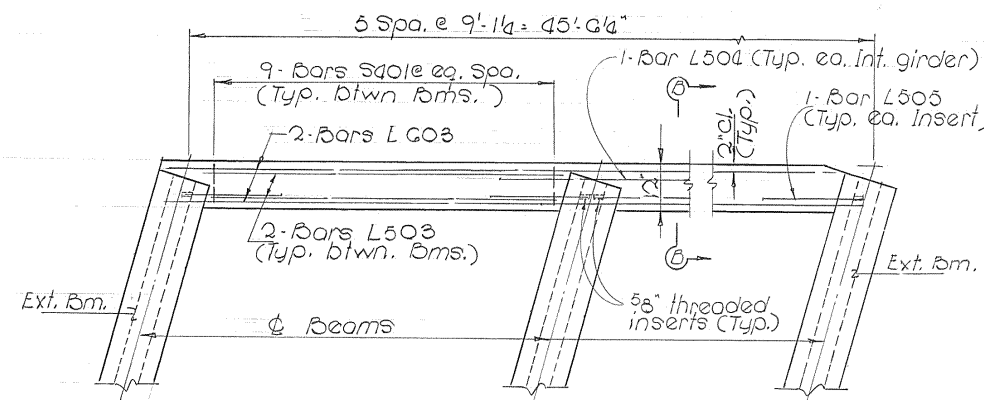
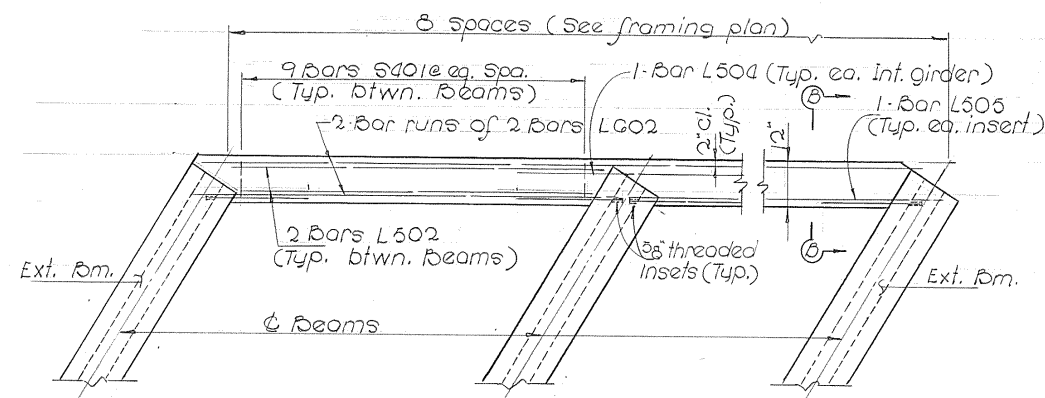

$$\text{Scale: } \sim 3'' = 1.0''$$

APPROVED _____
DIRECTOR OF HIGHWAYS

M - 44-120

DESIGNED BY D. McCorkle DATE 10-24-75
DRAWN BY L. Smith DATE 10-24-75
SUPERVISED BY D. McCorkle DATE 10-24-75
CHECKED BY F. Hoffman DATE 10-24-75

MICROFILMED



NOTES:

1. See Dwg. M-44-III for Bar Bends, Bill of Steel and Estimated Quantities.
2. See Dwg. M-44-III & M-44-III for Typical Sections.
3. Reinforcing Steel protruding from the end of beams may be field bent to permit erection.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

DIAPHRAGM DETAILS - ABUTMENTS LA & LB
STA. 242 + 50.00

SHELBY COUNTY

CORRECT ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAY

M-44-121

MICROFILMED

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

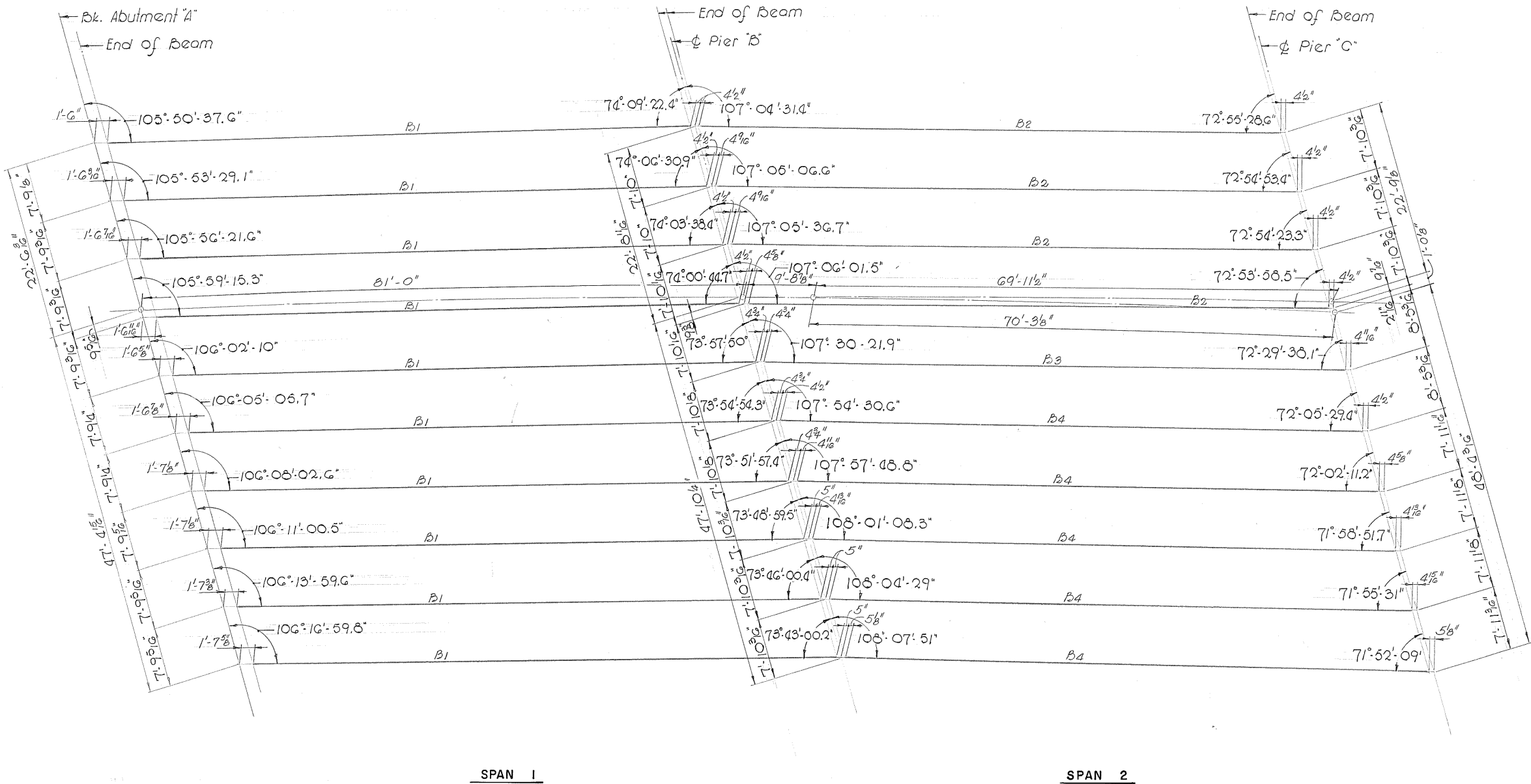
DESIGNED BY D. McCorkle DATE Oct. '75
DRAWN BY M. Garay DATE "
SUPERVISED BY D. McCorkle DATE "
CHECKED BY E. Hoffman DATE "

NOSE DETAILS

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EAC1-240-1(133)9	1975	56

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

- NOTES:
- See Dwg. M-44-128 for Beam Detail.
 - See Dwg. M-44-110 & 111 for Typical Sections.
 - See Dwg. M-44-118 thru 121 for diaphragm details.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

FRAMING PLAN - SPANS 1 & 2
STA. 242+50.00

SHELBY COUNTY

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY D. McCorkle DATE Sept. 1975
DRAWN BY M. Garay DATE Sept. 1975
SUPERVISED BY D. McCorkle DATE Sept. 1975
CHECKED BY F. Hoffman DATE Sept. 1975

CORRECT _____
ENGINEER OF STRUCTURES

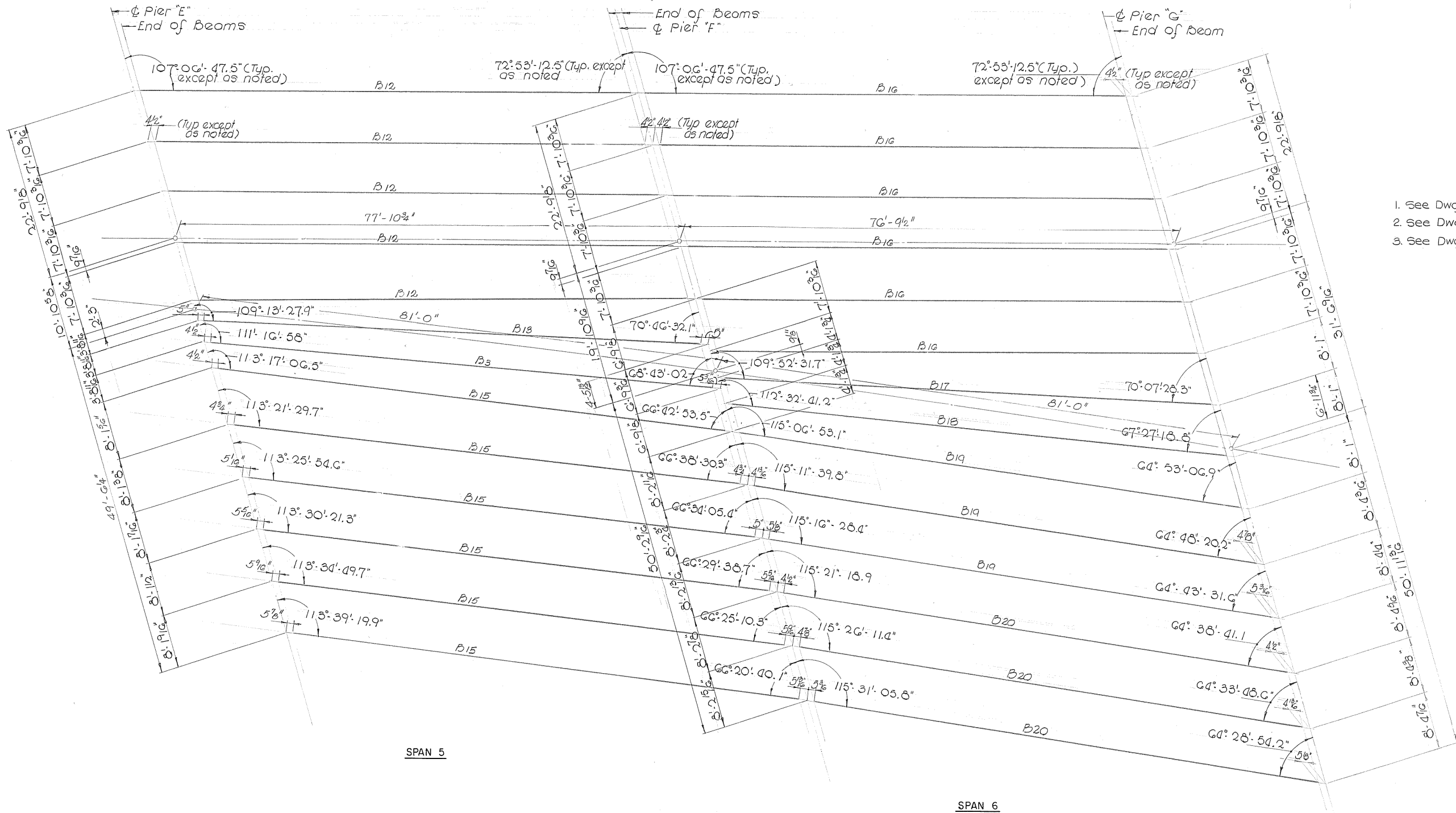
APPROVED _____
DIRECTOR OF HIGHWAYS

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3/27-44	EACI-240-111339	1975	58

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTES

1. See Dwg. M-44-128 for Beam Details.
2. See Dwgs. M-44-110 & 111 for Typ. Section.
3. See Dwgs. M-44-118 thru 121 for Diaphragm Details.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

FRAMING PLAN - SPANS 5 & 6
STA. 242 + 50.00

SHELBY COUNTY

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY: D. McCorkle DATE: Sept. 1975
DRAWN BY: M. Garay DATE: Sept. 1975
SUPERVISED BY: D. McCorkle DATE: Sept. 1975
CHECKED BY: F. Hoffman DATE: Sept. 1975

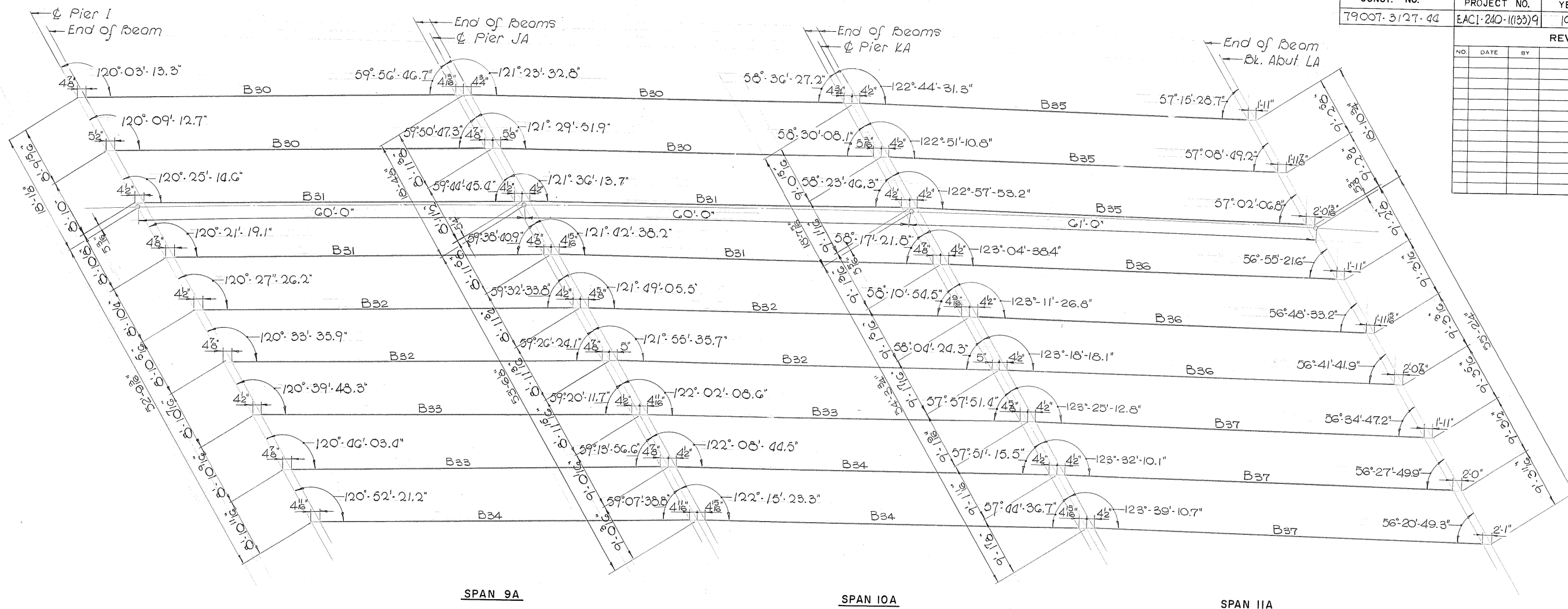
CORRECT: ENGINEER OF STRUCTURES

APPROVED: DIRECTOR OF HIGHWAYS

M-44-124

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EAC1-240-1(133)9	1975	60

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



NOTES

1. See Dwg. M-44-128 for Beam Details.
2. See Dwg. M-44-110 #111 for Typ. Section.
3. See Dwg. M-44-118 thru 121 for Diaphragm Details.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

FRAMING PLAN - SPANS 9A, 10A & 11A
STA. 242+50.00

SHELBY COUNTY

CORRECT _____
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

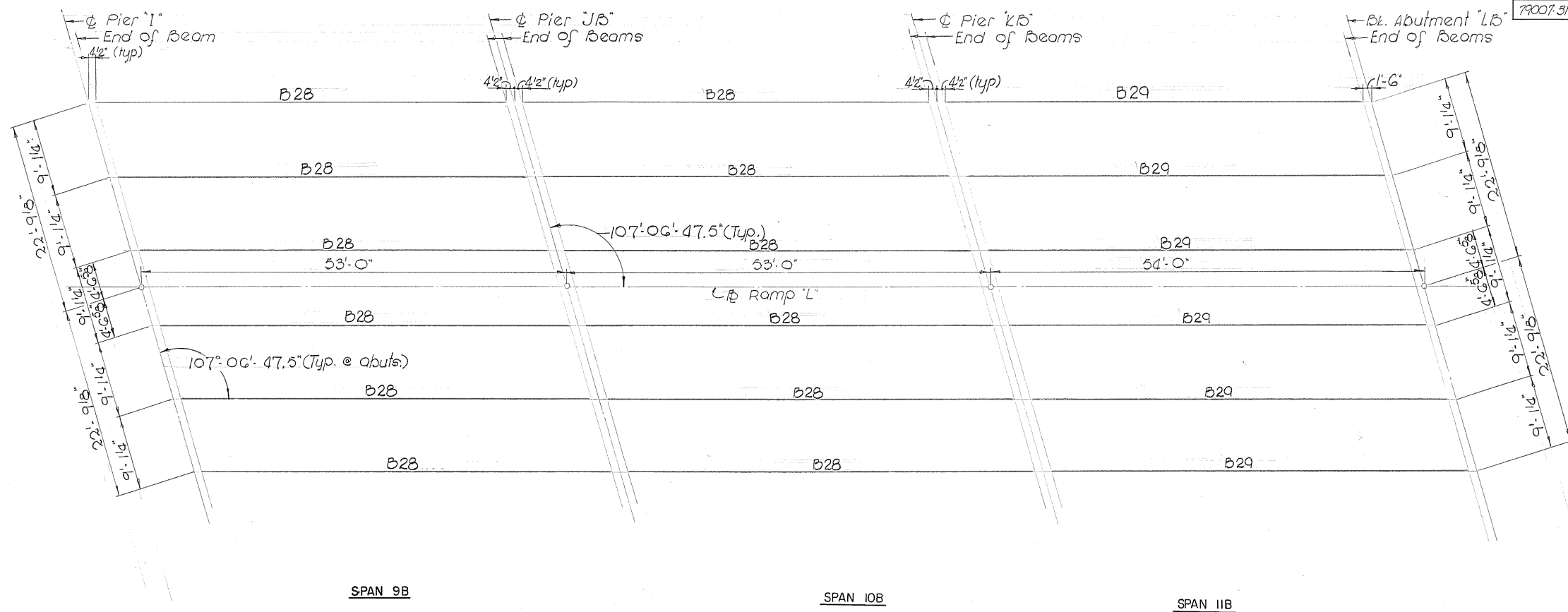
HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY D. McCorkle
DRAWN BY M. Garay
SUPERVISED BY D. McCorkle
CHECKED BY F. Hoffman

DATE Sept. 1975
DATE "
DATE "
DATE "

MICROFILMED

MICROFILMED



CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007.3/27-44	EACI-240-1(133)9	1975	61

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTES:
1. See Dwg. M-44-128 for Beam Details.
2. See Dwg. M-44-111 for Typical Section.
3. See Dwg. M-44-118 thru. M-44-121 for Diaphragm Details.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
E.B. I-240 OVER WOLF RIVER

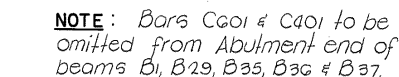
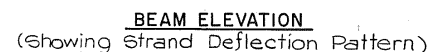
FRAMING PLAN - SPANS 9B, 10B & 11B
STA. 242+ 50.00

SHELBY COUNTY

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY F. Hoffman DATE Sept '75
DRAWN BY M. Garay DATE
SUPERVISED BY D. McCorkle DATE
CHECKED BY D. McCorkle DATE

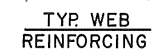
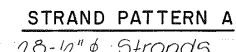
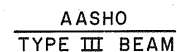
CORRECT ENGINEER OF STRUCTURES
APPROVED DIRECTOR OF HIGHWAYS



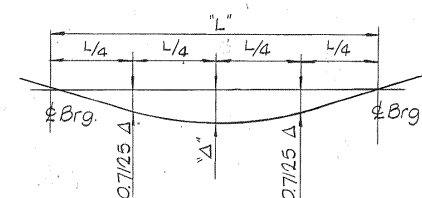
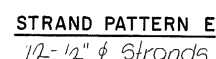
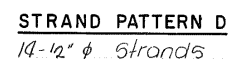
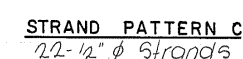
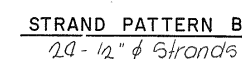
BEAM DATA										ESTIMATED QUANTITIES (PER BEAM)		
Beam	No. Rein'd	L1	Strand Pattern	"a"	"b"	"c"	"d"	L2	(Δ)	Conc. (Cu.Yds)	Reinf. Steel (lbs.)	Prestressing Steel (lin.ft.)
B1	10	79'-0 1/8"	A	30	12	10	G	40'-4	0.1770	11.4	1,249	2,228
B2	4	78'-11 1/4"	B	20	1	1	10	40'-3	0.1679	11.4	1,163	1,906
B3	2	79'-1"	B	1			1	40'-4	0.1695	11.4	1,183	1,910
B4	5	79'-3 3/8"	B					40'-5	0.1710	11.4	1,184	1,915
B5	4	78'-1 1/4"	B					39'-10	0.1600	11.2	1,180	1,887
B6	2	78'-5 1/2"	B					40'-1"	0.1633	11.3	1,182	1,895
B7	1	78'-10 1/16"	B				1	40'-3	0.1667	11.4	1,183	1,905
B8	10	79'-3 3/4"	B				10	40'-3	0.1703	11.4	1,184	1,916
B9	5	77'-2 1/4"	C				8	39'-5	0.1481	11.1	1,164	1,709
B10	1	77'-10 3/8"	C				8	39'-9	0.1532	11.2	1,166	1,724
B11	2	80'-2"	B				10	41'-8	0.1636	11.5	1,192	1,936
B12	5	77'-1 1/16"	C				8	39'-5	0.1465	11.1	1,164	1,703
B13	1	78'-0 1/16"	B				10	39'-10	0.1579	11.2	1,180	1,884
B14	2	79'-9 1/2"	B				10	40'-8	0.1666	11.5	1,185	1,927
B15	G	80'-3 3/8"	B				10	41'-0"	0.1775	11.6	1,187	1,939
B16	G	76'-0 1/2"	C				8	39'-2	0.1434	11.1	1,162	1,700
B17	1	77'-3 1/2"	C				8	39'-5	0.1472	11.1	1,164	1,711
B18	2	78'-8 3/8"	B				10	40'-2	0.1638	11.3	1,182	1,901
B19	G	80'-3 3/8"	B				10	41'-0"	0.1770	11.6	1,187	1,939
B20	7	80'-5 3/8"	B				10	41'-1"	0.1784	11.6	1,188	1,943
B21	7	74'-10 3/8"	C				8	38'-3	0.1286	10.8	1,157	1,658
B22	1	76'-0 1/16"	C				8	38'-10	0.1368	10.9	1,160	1,683
B23	7	73'-7 1/4"	C				8	37'-8	0.1199	10.6	1,153	1,630
B24	1	75'-5 3/8"	C				8	38'-9	0.1326	10.9	1,159	1,671
B25	1	77'-7"	C				8	39'-0	0.1489	11.2	1,166	1,718
B26	2	79'-5 1/2"	B	1	1	1	10	40'-7	0.1742	11.4	1,185	1,919
B27	1	80'-8"	B	20	12	10	10	41'-2	0.1749	11.6	1,188	1,948
B28	12	52'-3"	E	10	7	7	10	27'-0"	0.0329	7.5	650	633
B29	G	52'-1 1/2"	E	10	7	7	10	26'-11"	0.0326	7.5	605	632
B30	4	59'-0 1/8"	D	8	12	G	12	30'-3	0.0469	8.5	912	834
B31	4	59'-2 1/8"	D	1	1	1	1	30'-3	0.0475	8.5	913	836
B32	4	59'-4 1/8"	D					30'-7	0.0479	8.5	914	838
B33	3	59'-6"	D					30'-7	0.0483	8.6	914	840
B34	3	59'-7 3/8"	D					30'-8	0.0487	8.6	914	841
B35	3	58'-6 3/8"	D					30'-1"	0.0452	8.4	866	827
B36	3	58'-9 3/8"	D	1	1	1	1	30'-2	0.0460	8.5	866	830
B37	3	59'-0 1/4"	D	8	12	G	12	30'-4	0.0468	8.5	867	833

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY F. Hoffman DATE 10-75
DRAWN BY T. Robison DATE 8-74
SUPERVISED BY D. M^s Corkle DATE 10-75
CHECKED BY D. M^s Corkle DATE 10-75

[illegible]

1. The top of all beams are to be rough floated. At approximately the time of initial set, the top of the beams will also be scrubbed transversely with a coarse wire brush to remove all laitance and produce a rough surface.
2. Mild reinforcing shall be ASTM. A615 Grade 60.
3. All prestressing strands to be '2' & high strength type 270K 7 wire uncoated stress-relieved prestressing strands.
4. An initial force of 28,910 lbs. shall be applied to each strand in all beams.
5. All beams are AASHTO- PCI Standard Type III.
6. After the beam is removed from the prestressing bed, the bars C601 & C401 shall be bent horizontally a sufficient amount so as to permit bars "C" of adjoining beam to mesh when in the erected position.
7. The prestressing strands shall be cut flush at abutment ends of beams and a protective coating placed on the ends of beams. At the pier ends of beams, the prestressing strands shall be left projecting 3" from the ends of the beams. There shall be no protective coating placed on the ends of beams or on the projecting strands.
8. The concrete for this construction shall be of such properties as to attain a compressive strength of not less than 5000 P.S.I. at the age of 28 days and stress transfer shall not be made to the bridge member until the test specimens indicate that the concrete has reached a compressive strength of at least 4000 P.S.I.
9. The cost of furnishing and placing all plain Elastomeric Pads, dowel bars, inserts and rubber bonding cement shall be included in the bid price for the beams.
10. All beams to be supported during slab construction to prevent rotation.
11. Elastomeric bearing pads shall conform to section 25 of AASHTO Standard Specifications for Highway Bridges (1973) plus Interim (1974).
12. Plain bearings may be made of 70 durometer hardness Elastomers. Laminated bearings shall be made of 50 durometer hardness Elastomers.
13. See Dwg. M-44-129 for Bearing Details, Bar Bends and Bill of Steel.



NOTE: This curve is for dead load of the slab and all dead loads that are applied after the slab is in place, and should be corrected to compensate for the effects due to vertical curve.
See Beam Deflection Diagram.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32 B
W. B. I-240 OVER WOLF RIVER

PRESTRESSED BEAM DETAILS

STA. 242 + 50.00

SHELBY COUNTY

CORRECT

ENGINEER OF STRUCTURES

APPROVED

DIRECTOR OF HIGHWAY

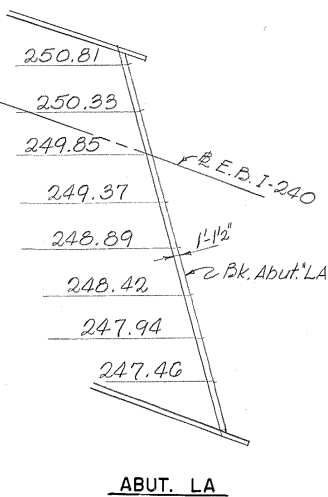
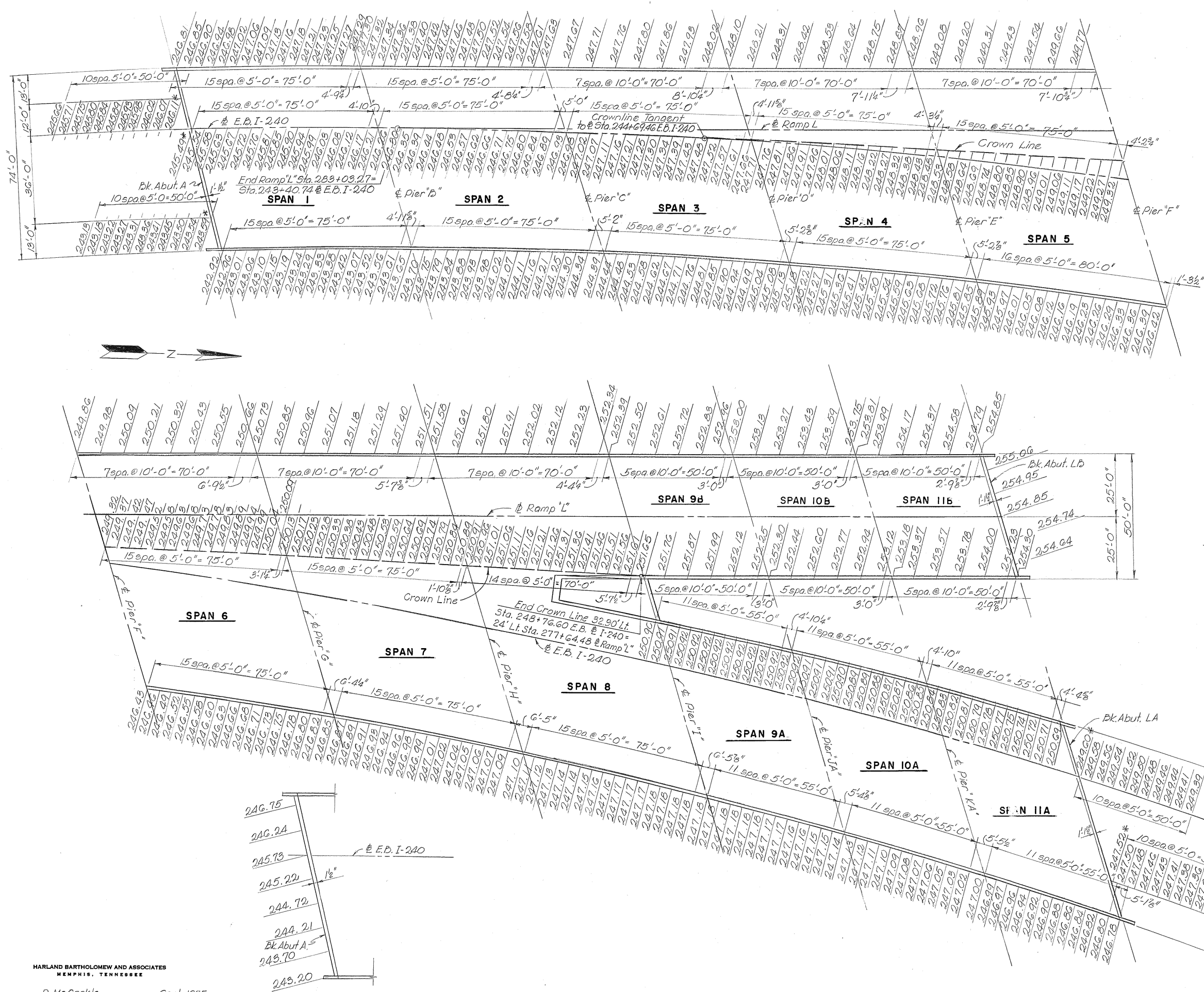
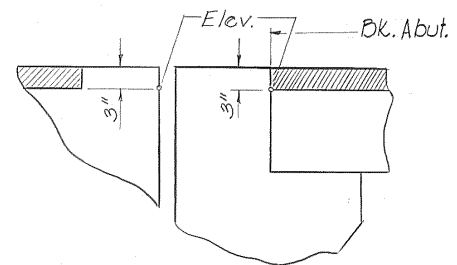
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MICROFILMED

CONST. NO.	PROJECT NO.	YEAR	SHEET NO.
79007-3127-44	EACI-240-1(133)9	1975	64

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

- NOTES:
- Elevations shown are top of concrete (3" below finished grade) and do not include asphalt overlay.
 - Elevations at Bk. Abutments given at finished grade.
 - Elevations at Abutments and marked thus (*) are given to points as shown below.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE NO. 32B
E.R. I-240 OVER WOLF RIVER

SCREED ELEVATIONS
STA. 247+50.00

SHELBY COUNTY

HARLAND BARTHOLOMEW AND ASSOCIATES
MEMPHIS, TENNESSEE

DESIGNED BY D. McCorkle
DRAWN BY J. Starr
SUPERVISED BY D. McCorkle
CHECKED BY F. Hoffman

DATE Sept. 1975
DATE Sept. 1975
DATE Sept. 1975
DATE Oct. 1975

CORRECT

ENGINEER OF STRUCTURES

APPROVED

DIRECTOR OF HIGHWAYS

M-130